

INFORMATION REPORT

Title: Housing Accelerator Fund and Plan Review Update
Date: 2025-10-08
Prepared by: Lindsay Slade
Contributors: Devin Lake



SUMMARY

Housing Accelerator Fund - Draft Planning Documents

The Town has been working on the Housing Accelerator Fund file since February 2024. Work-to-date can be found here: wolfvilleblooms.ca/hafimplementation

Staff and Consultants at Fathom Studio and Happy Cities have created an [Implementation Framework](#) to guide decision making – the final chapter dealing with the lands of Acadia University is included as an attachment in this package.

The bulk of this package includes a Draft Municipal Planning Strategy and Land Use By-law that attempt to implement the Town's HAF commitments – articulated in our Action Plan:

1. Plan Review:

- Allow more than 1 dwelling per lot in the R-1 zone - (see MPS part 8, LUB part 10)
- Increased building height - (See MPS parts 8,9, LUB parts 4.16, 9, 10, 11, 12, 13)
- Appropriate approval processes (see LUB Part 8, table 8.1 and 8.2,
- Simplified zoning categories – (see zoning map LUB Schedule A)
- Reduce or eliminate parking requirements (see MPS Part 5.2, LUB Part 6)

2. Student Housing (preview today) - see attachment

- Identify opportunity sites on campus and provide development support.
- Update LUB to allow 4 storey mixed use building built to full footprint of Subway parking lot
- LUB updates to support a 5-6 storey mixed use building on President's Field
- LUB updates to support taller buildings on appropriate sites through campus, and
- LUB updates to allow a mixed of institutional, commercial and residential at all sites.

3. Non-Market Housing (being worked into MPS - part 3.3,

- Establish policy to guide a non-market housing grant program,
- Enable waiving of development fees for non-market housing,
- Create town-owned land bank to enable disposal of parcels to eligible non-market housing providers,
- Maintain the Non-market Housing Strategy
- Set target of 5% of all new dwelling units to be non-market housing,
- Prioritize and expedite non-market housing development applications,

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- Consider buildings higher than 4 storeys in the Downtown for substantial non-market housing proposals,

The documents under review also include changes to areas that were not commitments under the HAF agreement but are timely to update when we are opening our planning documents. These include:

- **Strategic Directions/Community Priorities** (Part 2 of the MPS) – minor edits to Part 2 of the MPS to align with the new Council Strategic Plan
- **Mobility Chapter** of the MPS (Part 5) Workshop forthcoming with consultant and Council.
 - To have our policy documents align with the multi-year Active Transportation work
- **Development Constraints** - update based on Provincial approach changing and the Dyke topping project now being committed
- **Recreation** section (Forthcoming)
- **Design Guidelines** – attached
- **Urban Forest**

What to expect at the Oct 9th meeting:

No motions will be asked of the committee at this meeting. This is an opportunity to receive a presentation and the draft planning documents, and then a full month to review the documents before we meet again and recommendations are made.

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1) CAO COMMENTS

This report is for information purposes only so CAO may not have any comments

2) REFERENCES AND ATTACHMENTS

- Draft Municipal Planning Strategy – Attached
- Draft Land Use Bylaw - Attached
- [Wolfville Blooms – Package 3](#)
- Acadia Chapter – Attached
- Design Guidelines - Attached

3) DISCUSSION

Reviewing the DRAFT Documents

These documents are approximately 90% complete and provide the Committee and Council the opportunity to see the details and proposed changes to meet our HAF commitments.

Additional changes to the documents will be forthcoming as Staff and Consultants gather more feedback through the Open Houses and take direction from Town Council through the forthcoming process.

During the October 9th PAC meeting, the documents will be introduced and the Committee and Council will have a preliminary discussion. PAC will then have more time to review and make a recommendation to Council on November 13th.

Advice on reviewing

While reviewing these documents, bear in mind that some revision to the details is forthcoming. As PAC members and Councillors, you are not expected to be able to interpret the documents in their entirety. Take note of generalized questions or comments you have as you review and Staff and consultants will provide you with responses.

HAF Contract Commitments and Progress

These draft documents address the minimum requirements of our HAF Action Plan. To remain in the HAF program, retain issued funds and continue receiving funds, there is little room for further amendments to these action plan items.

- **Building height** – increase height in the Downtown
 - Building heights up to 4 storeys are enabled in the Downtown (DT), Residential Medium Density (RM) and Mixed Use (RM) zones.

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- Up to 6 storeys are enabled in the Residential High Density (RH),
 - Up to 5 storeys are enabled in the Downtown (DT) zone for substantial non-market housing developments.
 - Up to 8 storeys in the Institutional University (I-2) zone.
 - Building height has not increased in the Residential Low Density (RL) and Residential Low Density – Restricted (RL-R) zones.
- **Amend the R-1 zone to allow more than 1 unit**
 - Draft documents allow for 1 dwelling unit plus an accessory dwelling unit OR a secondary suite – to a total of 2 dwellings.
- **Non-market housing (MPS part 3.3)**
 - Draft documents contain policies related to non-market housing (grant program, land bank, density bonusing, other support)
- **Student housing (see attached)**
 - Acadia Chapter identifies opportunity sites for new student housing on university owned lands.
- **Reduce parking requirements (LUB Part 6, MPS Part 5.2)**
 - Parking requirements have been eliminated or reduced in the draft documents.

Timeline and next steps

- October 9 - PAC and Council meeting to receive the draft Planning Documents
- October 29, 30 - Open houses at Wolfville Legion
- COW:
- November 13th – PAC receives What We Heard summary from Open House, makes recommendation to Council on the draft Planning Documents.
- December 16th – Council gives First Reading to draft Planning Documents
- January/February – Legislated process of public hearing notices, public hearing, and second reading occur.

4) FINANCIAL IMPLICATIONS

HAF budget contribution details

The Town has entered into a \$1,827,600 agreement with the Federal Government's Housing Accelerator Fund. The fund is issued every year, for 4 years. To date, the Town has received 2 installments of this fund to a total of \$913,800. Two more installments will be issued as the Town works to implement the HAF Action Plan, by our agreed-upon date of February 2026.

Failure to adopt the Action Plan may result in the requirement to return funds received to-date, and forfeit any further HAF funds.

These funds are available to use for a variety of purposes that support housing and community infrastructure, staff capacity, and more.

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5) REFERENCES TO COUNCIL STRATEGIC PLAN AND TOWN REPORTS

Reference the appropriate strategic direction(s) from the 2021-2025 Strategic Plan:

- Economic Prosperity
- Social Equity
- Community Wellness

6) COMMUNICATION REQUIREMENTS

See next steps. Open houses will be held on October 29 and 30th. An advertisement has been placed in the Grapevine newspaper, brochures are being distributed across town with support of Councillors, advertisements are also being shared on the Town's social media pages. The legislated process to advertise for public hearing will occur following first reading.

7) FUTURE COUNCIL INVOLVEMENT

Council will receive a recommendation from PAC at the November 13th 2025 Council meeting.

5

Action 4: Acadia University Housing

5.1 Acadia

University campuses throughout Atlantic Canada act as incubators and catalysts for the development of community and as a guarantor of our quality of life. The strength of these institutions is directly correlated to the resilience of our communities as we weather the uncertainty in the changing climate, new international geopolitical forces, and Nova Scotia's role in nation building projects that will define the next quarter century of prosperity in Canada.

The relationship between Acadia and the Town of Wolfville is well established, with policy expressly adopted to formalize the relationship and ensure that the Town is supportive of Acadia's mission, the Housing Accelerator Fund offers an opportunity for the Town to detail how opportunity sites around the Acadia campus can be activated to provide housing and serve the mission of the University.

University Opportunity

In this moment, while addressing the housing crisis it is important to note the changing landscape for Canadian institutions. It is imperative that the Town of Wolfville works to understand not only how to nurture a successful Town & Gown relationship through support, but moreover what can be done to drive the success of Acadia University in new models of partnership.

Creating a true “two-way street” where the Town is not only removing barriers for the University, but also actively seeking and creating new partnerships is a path that will increase the likelihood of success for the University, which will in turn drive the success of Wolfville.

5.2 Sites

Opportunities exist at critical Acadia University owned properties. Municipal actions through HAF can support Acadia with new, exciting development support. This builds on work with the Town & Gown committee and the work of Council and Staff in preparing HAF – Package 3.

The model for these opportunities is almost exclusively private partnerships and land sales. The efforts to monetize I-2 zoned lands and other University owned lands will ensure that new assets can be developed without new cash outlay from Acadia, and without necessarily partnering with the Provincial government.

Value added features that could be part of these redevelopments include;

- Campus Retail Store (clothing, merchandise, etc.),
- Events Acadia Space,

- Conference / Events Venue Space,
- Infrastructure to Support Media Access to Fields,
- Potential to offer spaces “rent free” to Acadia,
- Potential to hold units for Acadia's use (visiting scholars), and
- Potential to influence private build through agreements of purchase and sale.

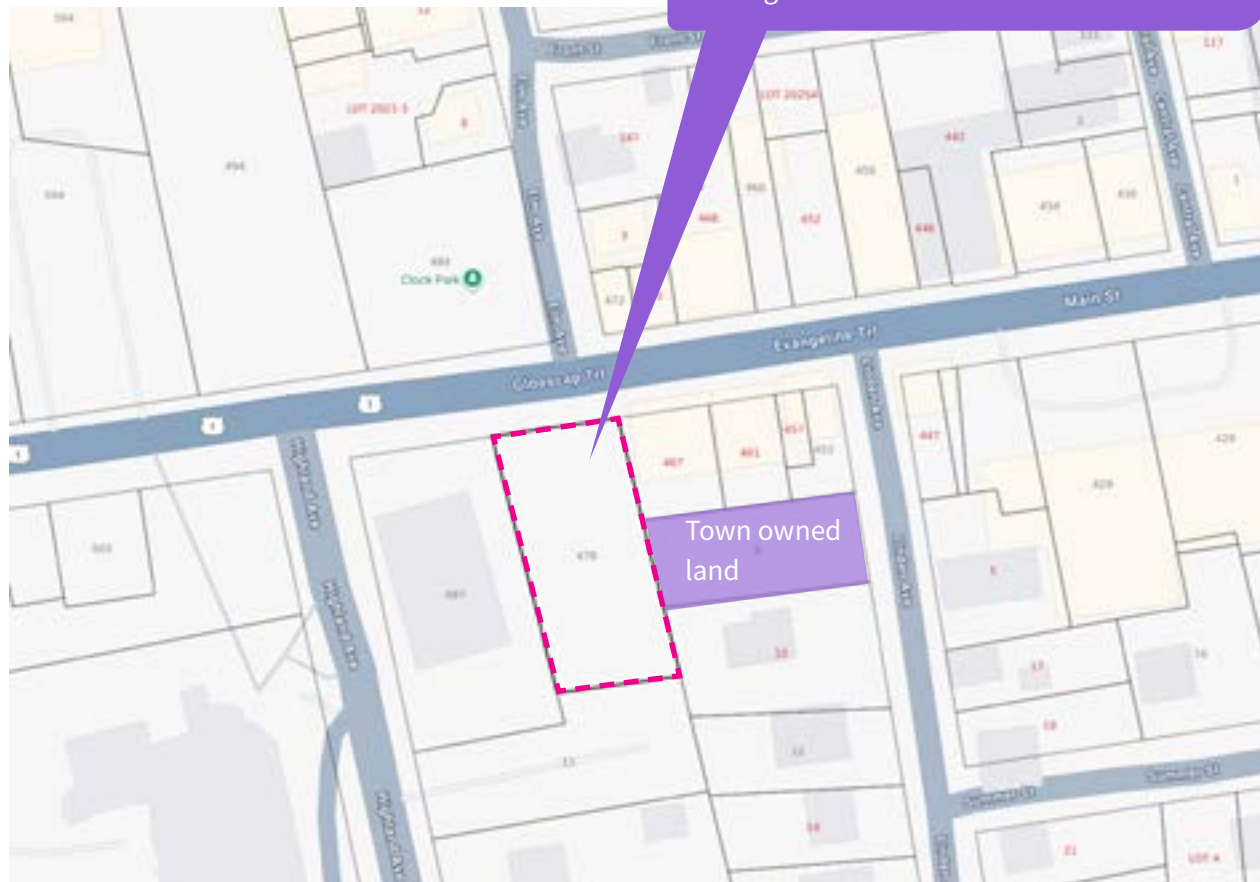
Necessary Actions from the Town include;

- LUB updates to support 4 storey mixed use building built to full footprint at Subway Parking Lot,
- LUB updates to support a 5-6 storey mixed use building at President's Field,
- LUB updates to support taller buildings on appropriate sites through campus, and
- LUB updates to allow a mix of institutional, commercial, and residential at all sites.

479 Main Street - Subway Parking Lot

This site, colloquially known as the Subway Parking Lot, represents a development opportunity in the Downtown that can mirror the success of recent residential mixed-use projects and provide for new commercial offerings and an institutional “outpost” to see Acadia well represented on Wolfville’s commercial mainstreet.

The lot provides Acadia with an opportunity for a visible face to the university on Wolfville’s commercial main street at the highly visible terminus to Elm Street. The lot is also accessible via the Town’s undeveloped parking lot to the east of the property. This could provide access to an underground parking garage and a grade-level parking garage tucked behind a commercial frontage on Main Street. Part of this at-grade parking would be buried into the hill at the back of the lot adjoining the Church land.



Subway Lot

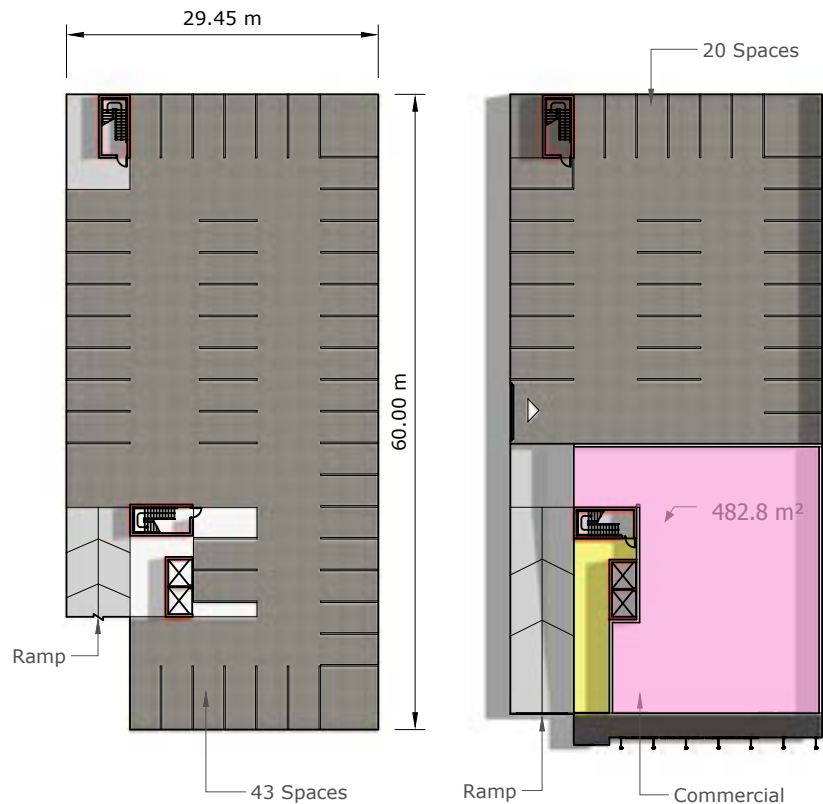
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Lot Area: 20, 451 sf

Existing Use: Parking Lot for the Town, 33 parking spots

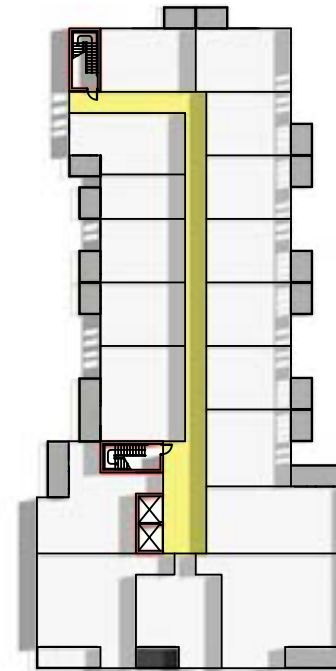
Zoning: C1

Town owned land

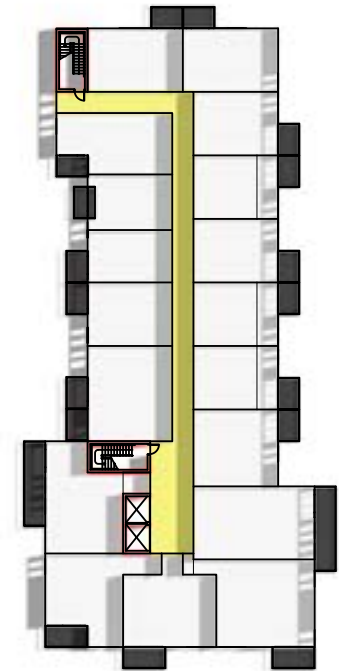


P1 Floor
scale: 1:500

Ground Floor Commercial
scale: 1:500



Floors 2-4
scale: 1:500



Floors 5-6
scale: 1:500

Floor Plans

The site is wide enough to support parking below grade and at-grade behind the commercial storefront. One floor of underground and at-grade parking provides about 63 spaces. The floor plan provides access to the at-grade parking (20 spaces) via the Town-owned lot to the east. Underground parking is accessed via a parking garage entry onto Main Street (43 spaces). The at-grade commercial frontage provides about 5,000 sf of floor area which could be reduced or enlarged with the loss or gain of parking in the rear. A residential lobby on Main Street provides access to the upper 3-storeys of residential. The residential layout allows for about 54 units with a 60% mix of 1-bedroom units (500-650 sf) and a 40% mix of 2-bedroom units (850-950 sf). More or less units could be achieved depending on the desired unit mix. We assume this would be a premium location for the university with a higher mix of family sized units. Assuming 4-storeys of height, the density of this development would be about 115 units per acre.

	Parking	Commercial (m2)	Units
P1	43		
Floor 1	20	482	
Floor 2			18
Floor 3			18
Floor 4			18
	63	482	54

Avg Unit Size 52 m2
 Site Area 0.47 acres
 Density 115 UPA
 Parking Ratio 1.16666667

Fig.33 — View From Elm Ave.



Elevations

The site has over 100' of frontage on Main Street so it would be important to break down the building massing into a series of more vertical articulations so the facade is not too imposing on Main Street.

The 4th storey steps back 2.5m from the front facade so that from the sidewalk, the building only appears as a 3-storey massing consistent with the current scale of Main Street. This 4-th storey

has been imagined as a traditional mansard roof which blends a more modern building to a more traditional historic setting.

The 5,000 sf commercial groundfloor could easily accommodate a range of commercial uses like a university book store, a merchandise and apparel store, a recruiting hub, a university cafe or some other use that would be sympathetic to Main

Street activity and the University's needs.

The building could also house an accessible green roof along the Main Street frontage to provide amenity space for residents.

Fig.34 — View From Elm Ave.



Design Details

The building has also been set back a few metres from the front property line to provide additional room for a pedestrian plaza on Main, and similar to the Baptist Church next door, provides a bit more breathing room for the church spire to the west which is so visible on Main Street today.

The proposed building uses glazing in the first floor to connect the interior uses to the public spaces along Main Street. The balconies on the

second and third levels look out over Main and provide more opportunity for eyes on the street and a connection between the building and the busy pedestrian environment.



Fig.35 — View From Main Street





Fig.36 — View From Clock Park



President's Field

President's Field provides an excellent opportunity for redevelopment, potentially with a private partner, that could provide new student housing and a wide variety of services and offerings that integrate with the athletics and cultural facilities in the 500 block of Main Street.

The site is generally unused and sits close to the Harvest Moon Trail, the Wolfville Market, and is at the eastern fringe of downtown. There is an easy opportunity to expand the parking lot owned by Acadia at the market and potentially an opportunity for underground parking in this location.

A multi-faceted development opportunity here includes sports, arts, and cultural tourism supported by short-term and seasonal mixed housing accommodations and conference services. This new complex could serve as the home base for Events Acadia and would bring desperately needed services and accommodations to the region. At the same time, there may be an opportunity for an expanded events centre adjacent to the football field, and the rooftop of this building could be used for

Presidents Field Lot

PID: 55279418

Lot Area: 2.5 acres

Existing Use: Welcome Centre and Sports Field

Zoning: I-2

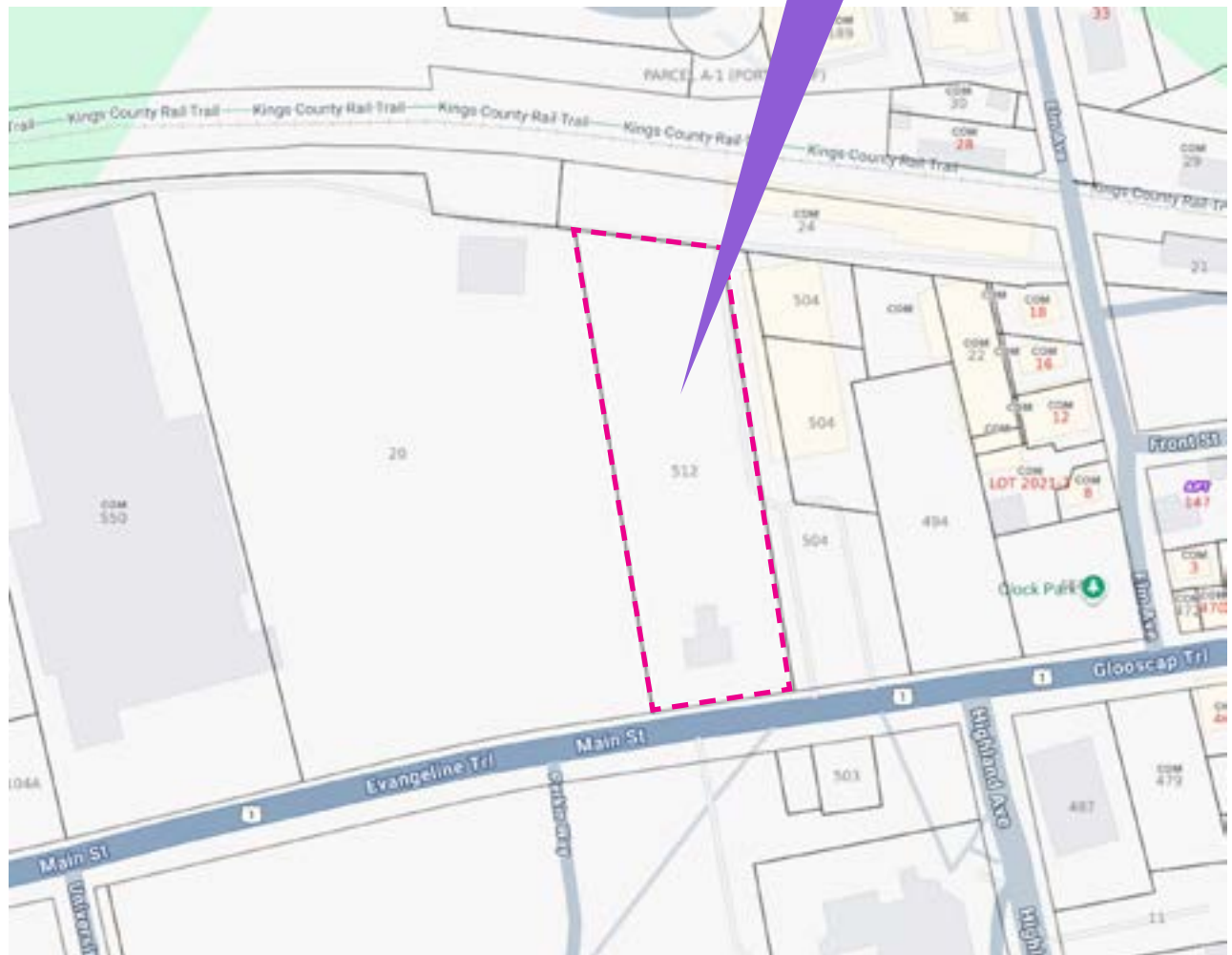


Fig.38 — Presidents Field Location Plan

Fig.39 — Presidents Field Site Plan



Fig.40 — Presidents Field looking east

media services for large scale sports events like the CFL games that sometimes call Acadia home.

The lack of resources to develop this site limits the university to a model whereby private partners would enter a lease agreement for the land that provided them the opportunity to build and operate the site with anchor tenants from the University and a partnership with Events Acadia to ensure that conference spaces, media collaborations for large events, and short term stays complement a robust offering of private housing options.

This site could accommodate a 6-storey development with the groundfloor and other sections portions of the building fully associated with events and private oprations and the upper storeys dedicated to residential or short stay units

The plan shows a potential development with a central stair up to the podium of the 2nd floor. The outdoor stairs could also serve as bleachers for the field, while the second floor podium could be event spaces, recreational spaces or even public garden spaces.

	Parking	Non-Residential (m2)	Concept Residential (m2)	Units
P1	80			
Floor 1		3420		
Floor 2		2880		
Floor 3			2700	42
Floor 4			2800	43
Floor 5			2800	43
Floor 6			2800	43
	80	6300	11100	171

Public Connections

In addition to the opportunity provided by the development of the building, there is a considerable oppourtunity to connect from Main Street through the Presidents Field Site, past the the Wolfville Farmer’s Market, and onto Dykeland Street. This would better connect existing Downtown Commerical to the Western end of Main Street and potentially create a new axis of public amenities surrounding Alumni House and the new development that will help alleviate pressure on the existing commercial streets as the Town and university grow.



Fig.41 — Presidents Field looking east



Fig.42 — Presidents Field looking south





Fig.43 — Presidents Field looking north- east



Fig.44 — Presidents Field Pedestrian Connection





Highway 101 Lands

These lands are located off Highland Avenue, totalling 4.3 acres directly adjacent to Highway 101 (no access to the highway) and frontage only on Highland Avenue. There exists a plan to develop a new 104 space childcare centre, developed by Acadia in partnership with the Province of Nova Scotia, and the intention of the university is to sell or lease the remaining lands.

The childcare centre will be developed in such a way that the project enhances access and parking for the Mosque on the adjacent property.

Updated Planning Documents are proposed to support development up to 8 storeys on this site, but design opportunity here is mediated by the adjacency to conservation lands to the West and a significant watercourse through the site. This property is not part of the university's 2003 Campus Master Plan (CPM).

Site Context

The site is bounded to the south by Highway 101, to the west by Acadia's Woodland Trails, to the east by Highland Avenue, and to the south by a collection of residential and community development.

To respect watercourses through the site and provide deference to the Woodland Trails, a



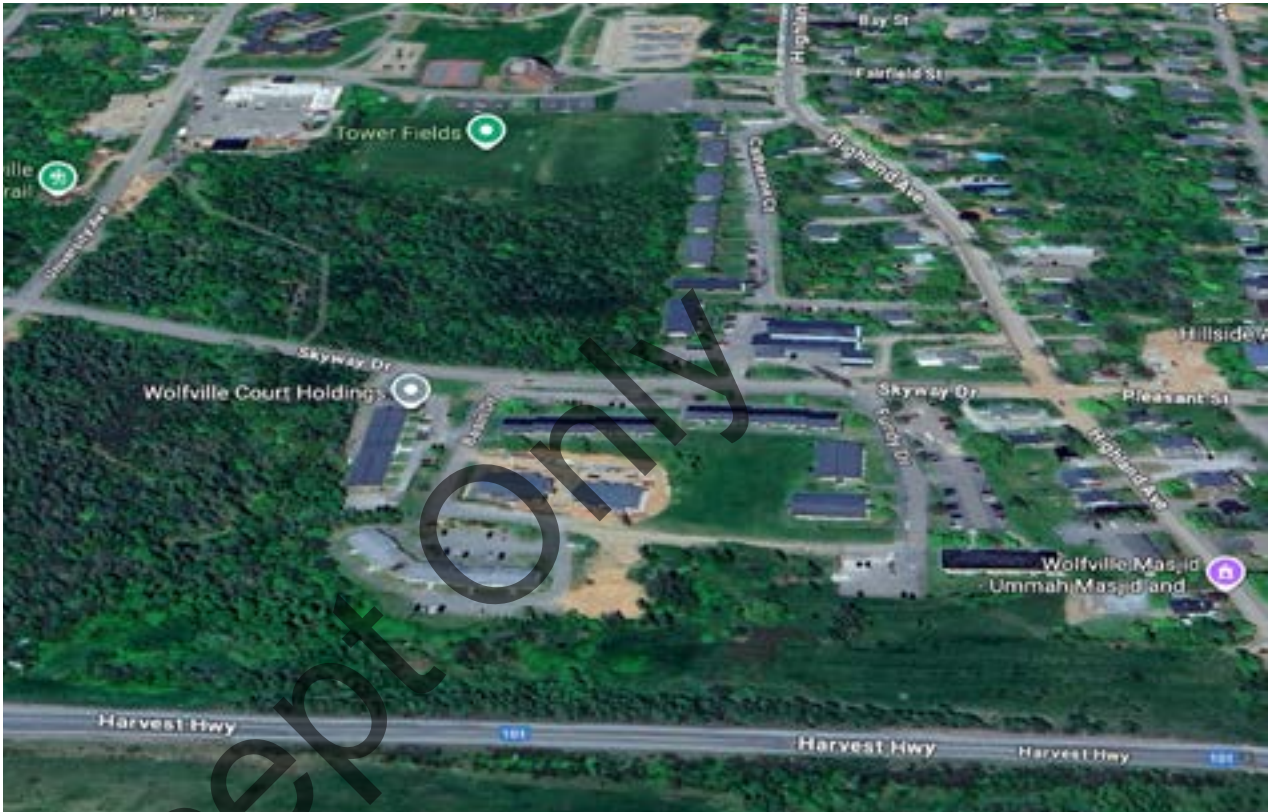
single building is proposed on this site toward the centre of the property.

We have scaled and massed a 6 atorey building using quick modelling software that shows viability of the form and the ideal layout of the structure.

Highway 101 Pro Forma

The pro forma assumes 10% of the unit mix is studio uses (<500sf), 70% is one bedroom (600 sf), and 20% is 2-bedroom (850-900 sf). Assuming one 6-storey building with 20 units / floor, this layout provides 120 units and 116 parking spaces on the surface parking surrounding the building (shared with the daycare). The massing of this opportunity is preliminary, it considers major constraints but limited architectural detail.

	Parking	Residential (m2)	Units
Surface Parking	116		
Floor 1		1479	20
Floor 2		1479	20
Floor 3		1479	20
Floor 4		1479	20
Floor 5		1479	20
Floor 6		1479	20
	116	8874	120



Total Project Pro Forma

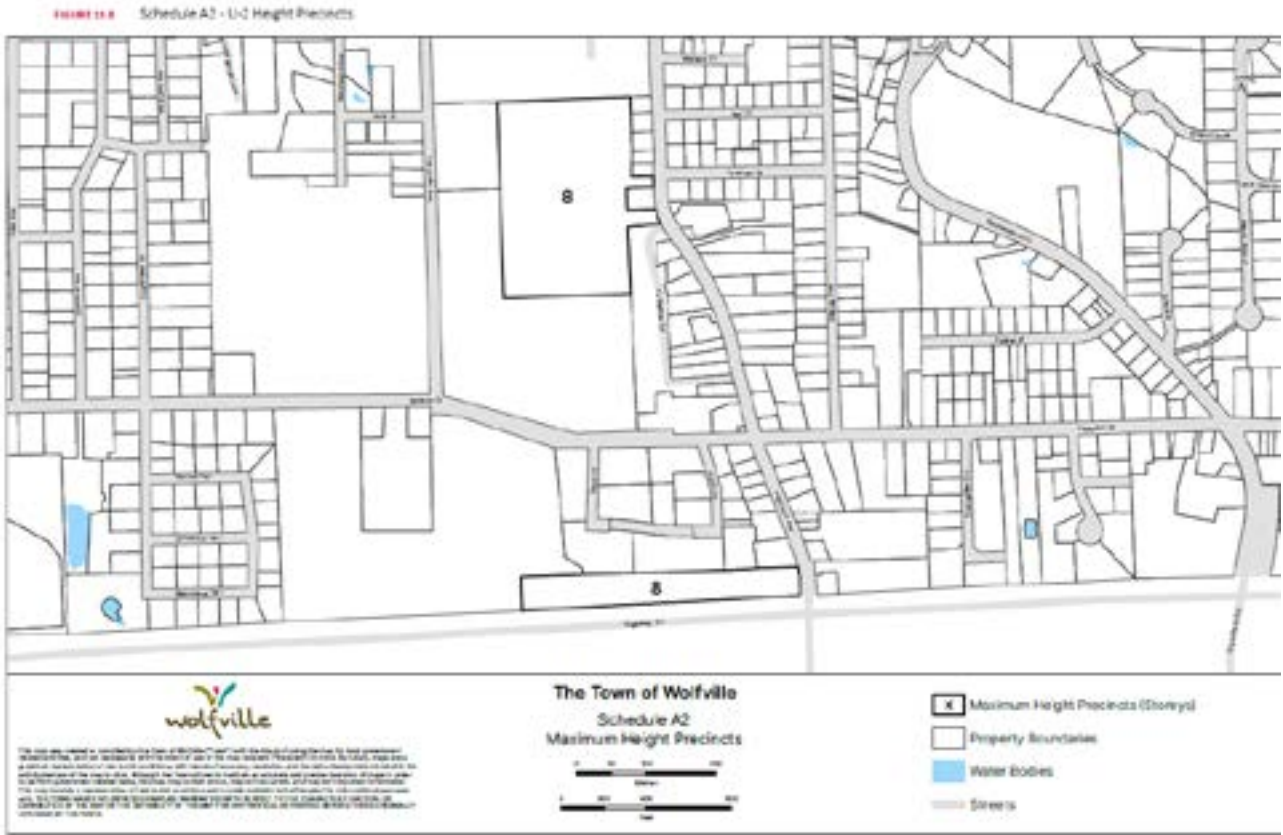
The combined proforma for all three sites shows the overall potential for Acadia which includes up to 345 units, 259 parking spaces, 482 m² of commercial space and 6,300 m² of other non-residential space. Each of these sites can support partnership relationships with private partners and all require collaboration and cooperation with the Town.

RECOMMENDED MPS & LUB CHANGES

Updated MPS policies and Land Use By-Law regulations include provisions for a wider variety of uses in the I-2 zone, 6-storey development rights and the ability to consider larger buildings (up to 8-storey) in the area area the existing Tower residence and by highway 101 – regulated with a schedule showing these areas in the Land Use By-law.

The Acadia University sections of the Town’s Municipal Planning Strategy and Land Use By-law are attached to this chapter to show the updates and support a strong Town and Gown relationship by providing additional opportunities for the University to work with over time.

	Parking	Commercial (m2)	Other Non-Residential (m2)	Residential Units	Floors
Subway Site	63	482	0	54	4
Presidents Field Site	80	0	6300	171	6
Highway 101 Site	116	0	0	120	6
Total	259	482	6300	345	



REVENUE GENERATION & ADDITIONAL AMENITIES

Establishing partnerships between the University and private industry is an effective and increasingly common method for universities to broaden the reach of their ideas, to underline the value of their work in the local, provincial, and national economies, and to provide valuable experiential learning opportunities for their students. One of the primary factors determining the feasibility of these partnerships is the availability of appropriate sites and/or buildings.

Other types of partnerships require sites within a reasonable distance of the University. “Incubator” space for faculty pursuing private-sector applications of research methods also fall into this category.

The opportunities outlined at the Subway Parking Lot, and at President’s Field both offer exceptional opportunity for partnerships due to the opportunity to create private market housing and to provide space back to Acadia on the ground floors that will create a low-cost footprint for Acadia in new areas.

The revenue that may be generated is to be determined by the private development opportunity and the scale of the needs of Acadia for long term use in these new developments. The Town can support Acadia by allowing maximum building envelopes to be developed on these crucial opportunity sites.

CONCLUSIONS

The recommended changes to the Land Use Bylaw and the Campus Master Plan are the critical first steps to unlocking the potential of these key sites.

This strategic alignment will enable public-private partnerships that deliver essential housing, commercial spaces, and university facilities. By acting now, the Town and University can build a stronger community, drive regional development, and solidify their leadership for the next quarter-century.

PART 10

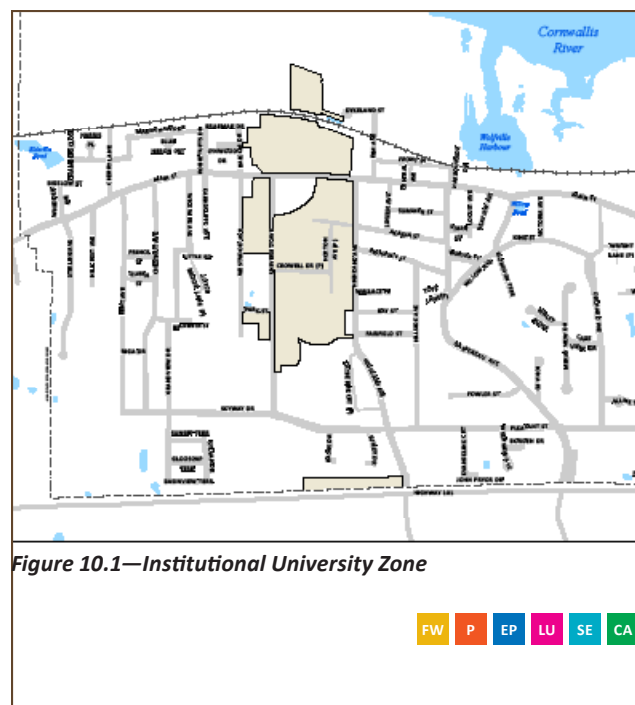
ACADIA UNIVERSITY INSTITUTIONAL

10.0 Town and Gown

Acadia has offered exemplary learning opportunities in a historic, small town environment since it's creation in 1838. The University owns more than two hundred acres of land that stretches from the Dykelands, north of the former rail line, southward to Highway 101 and from Westwood Avenue to Highland Avenue. One hundred and twenty acres of this land holding is developed.

The presence of the University exerts a substantial influence on the economic, cultural and social fabric of the community by providing employment and identity to the region. The identity, ongoing vitality and economic prosperity of both the town and the University are intrinsically linked through shared uses, places and populations. The future of the Town of Wolfville is reliant on a shared understanding of the needs and capacity of both the University and the Town.

New development on Acadia campus could offer a variety of benefits, including new income streams for the university, and new shops and services for students and residents. Additional housing would also better meet the campus's housing needs while helping to address the Town's overall housing shortage. This plan therefore seeks to encourage investment in new buildings on Acadia campus.



Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

IT SHALL BE THE POLICY OF COUNCIL:

1. To maintain a 'Partnership Agreement' with Acadia University in order to address areas of mutual concern and benefit, including Land Use Planning.
2. To support the following objectives with respect to Acadia University:
 - a. to recognize Acadia University as a distinct community of interest that is dispersed throughout the Town;
 - b. to foster co-operation between the Town and the University in terms of such matters as the provision of parking, student housing, servicing, conservation of

heritage buildings and areas, access and development proposals;

- c. to support the growth and redevelopment of the University and to encourage its long-term vitality within the Town;
 - d. to encourage, in particular, the construction of new student housing on Acadia-owned lands;
 - e. to minimize any adverse effects the University may have as a major activity centre on adjacent and surrounding neighbourhoods;
 - f. to recognize the various University areas as having different land use mixes, land use characteristics, locational factors, and different surrounding uses; and
 - g. to encourage Acadia University to protect and conserve their various heritage properties whether designated or not.
3. To designate as University those lands of Acadia University and currently being used for University purposes and generally bounded by Westwood Ave, Highland Ave, The Dykelands and Highway 101 as shown on Schedule 1 - Future Land Use Map.
 4. To establish an Institutional University (I-2) zone in the Land Use Bylaw. This zone is intended to include existing University uses.
 5. To recognize the role the University campus plays in the provision of Wolfville's public open spaces and to zone open space portions of the campus Parks and Open Space – University (P-2) as outlined on Schedule A of the Land Use ByLaw.
 6. To use Site Plan Approval in the Institutional University Designation for the consideration of certain uses that require additional regulatory considerations, as outlined in the Land Use ByLaw.
 7. To use Development Agreements in the Institutional University Designation for the consideration of certain

uses that require unique and site-specific considerations as outlined in the Land Use Bylaw.

8. To establish appropriate regulation for the uses permitted (as-of-right) or enabled for consideration in the Institutional University (I-2) zone of the Land Use Bylaw.
9. To permit, in the Land Use Bylaw, building heights in the Institutional University (I-2) zone congruent with the type of use and scale of present buildings located in the core of the University campus.
10. To restrict the setbacks of new buildings in the Institutional University (I-2) zone as outlined in the Land Use Bylaw, on those properties which abut residential zones or have frontage on Main Street to ensure greater architectural compatibility.
11. To establish abutting yard requirements in the Land Use Bylaw, which may include but is not limited to fencing, screening, parking lot location and standards where any yard in the Institutional University zone abuts a residential zone.
12. To recognize that the University development on the Dykelands area is intended to be used for accessory and utilitarian uses that support the University and not to accommodate principal University buildings or residences, as outlined in the Land Use Bylaw.
13. To recognize that complementary land uses to those permitted on Institutional University (I-2) land area those which allow the University to function as a community and shall be enabled through the Land Use Bylaw. These uses may include, but not be limited to:
 - a. classrooms, laboratories and similar educational and research facilities
 - b. residential buildings for the primary use of housing students and faculty
 - c. places of assembly
 - d. foodservice facilities for the primary use of students



- e. sport facilities
 - f. University offices
 - g. bookstores for the primary use of students
 - h. student services buildings
 - i. convention facilities as a secondary use of any other facilities
 - j. public and community gardens
14. To recognize that housing accommodation for post-secondary school students has specific land use characteristics and impacts on surrounding areas that require regulation through zoning and other means.
 15. To recognize that the Town shares a responsibility, along with Acadia University, and private landowners for ensuring that students have access to accommodation that is affordable, safe, accessible and sanitary.
 16. To support Acadia University to create an update to the most current Acadia University Campus Masterplan (2003) which establishes the location for:
 - a. Recreational and Active Sports Parks;
 - b. Open Spaces;
 - c. New residential uses;
 - d. Access road onto the public road network and Active Transportation opportunities; and
 - e. Community Buildings.
 17. That if campus areas currently designated University become available for non-University development, to undertake the completion of a secondary plan for redevelopment that includes analyses of transportation, infrastructure, and financial implications to be reviewed by the Town.
 18. To recognize the role of the University in Core Area management and consider this during development applications as outlined in the Land Use Bylaw.



PART 19 Institutional University (I-2) Zone

19.1 INTENT

The I-2 zone is intended to permit a range of university uses such as classrooms, places of assembly, sports facilities, residential buildings for the use of students, etc. Council may consider unique and site-specific developments by development agreement.

19.2 PERMITTED USES

As-of-right, Site Plan and Development Agreement uses are shown in Table 19.1

19.3 RE-ZONING

Rezoning to any other zone shall be considered subject to **policy 11.4.3** of the MPS.

19.4 HEIGHT

Up to 6-storeys are permitted by site plan approval. Up to 8 Storeys may be considered in the area in Schedule 2.



University uses



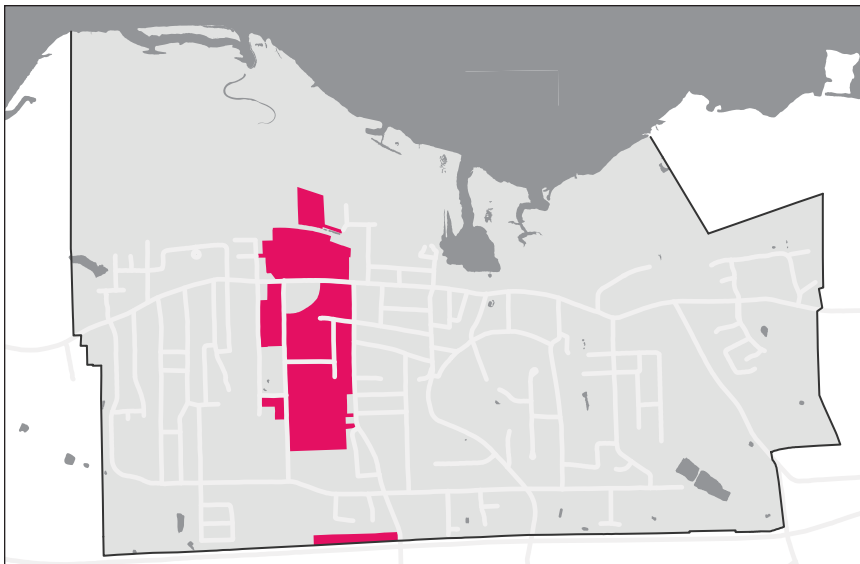
Sports uses



Places of
assembly



Classrooms



Other Provisions Apply

Part 4:

General Requirements for
All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 19:

General Requirements for
Institutional Zones

FIGURE 19.7 Schedule A2 - U-2 Height Precincts



19.5 BUILT FORM STANDARDS

See Figure 21.1 for all built form standards in the I-2 Zone.

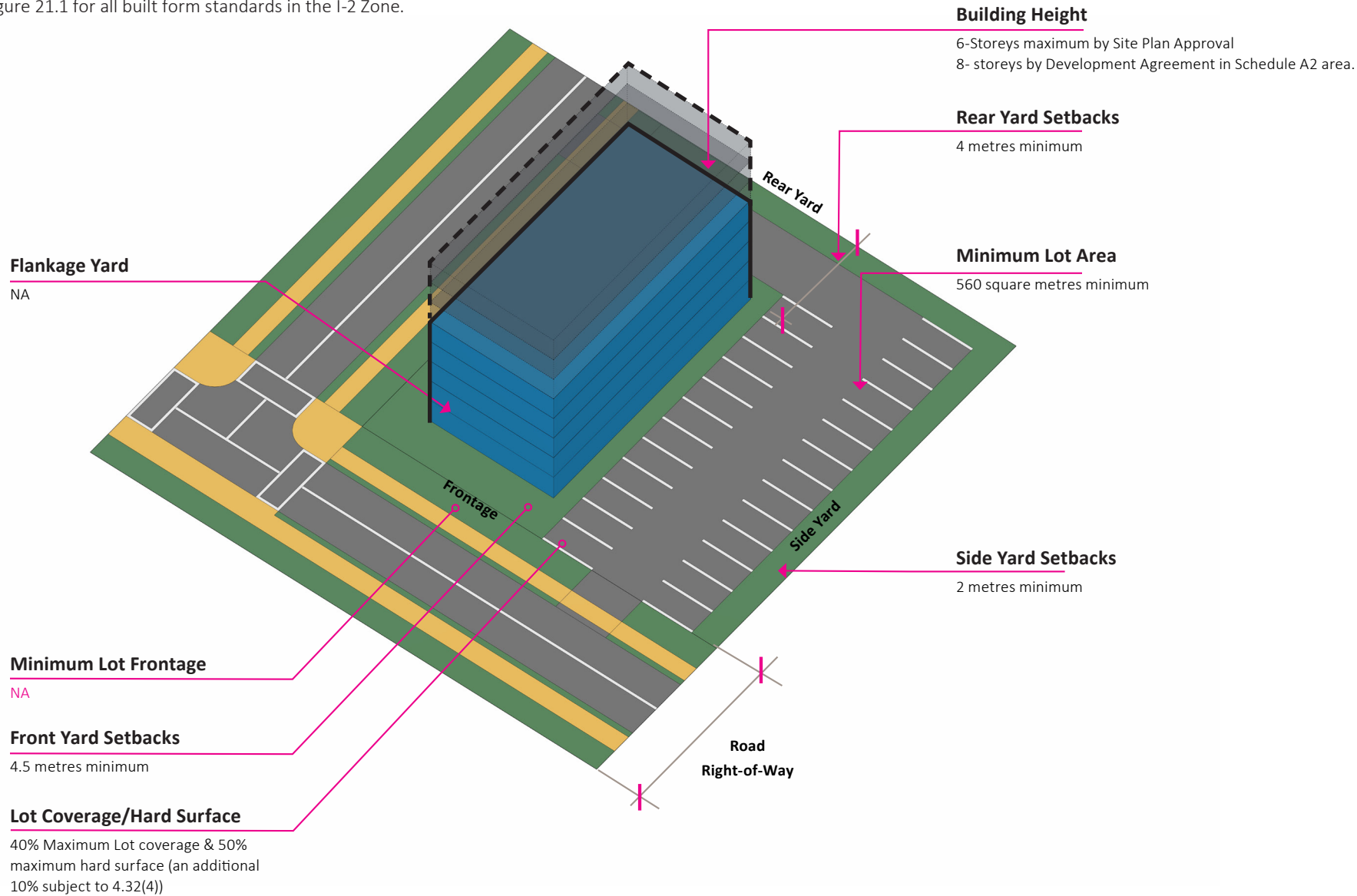


FIGURE 19.1 I-2 ZONE Built Form Standards

19.6 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the I-2 Zone.



FIGURE 19.2 I-2 Institutional Example

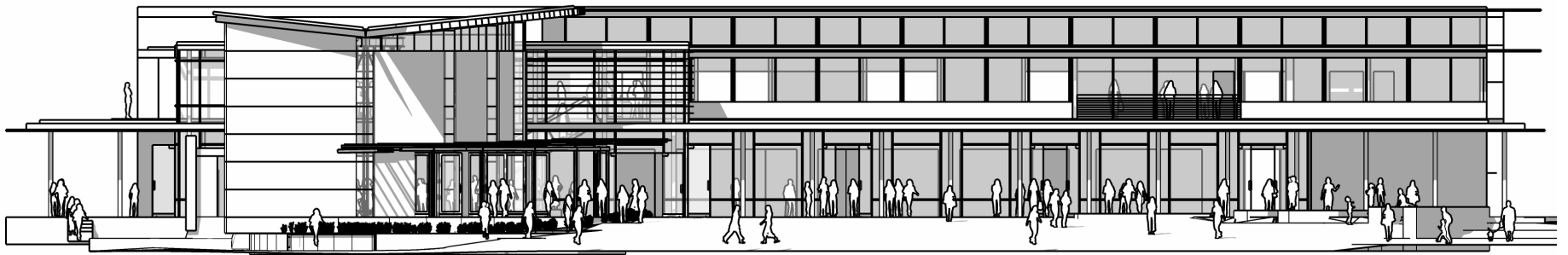


FIGURE 19.3 I-2 Institutional Example

PART 21 Parks And Open Space - University (P-2) Zone

21.1 INTENT

The P-2 zone is intended to permit a range of park and open space uses on green space areas owned by the University such as community gardens, sports fields, etc.

21.2 PERMITTED USES

As-of-right, Site Plan and Development Agreement uses are shown in Table 19.1

21.3 RE-ZONING

Rezoning to P-2 shall be considered subject to **policy 11.4.3** of the MPS.



Gardens & parks



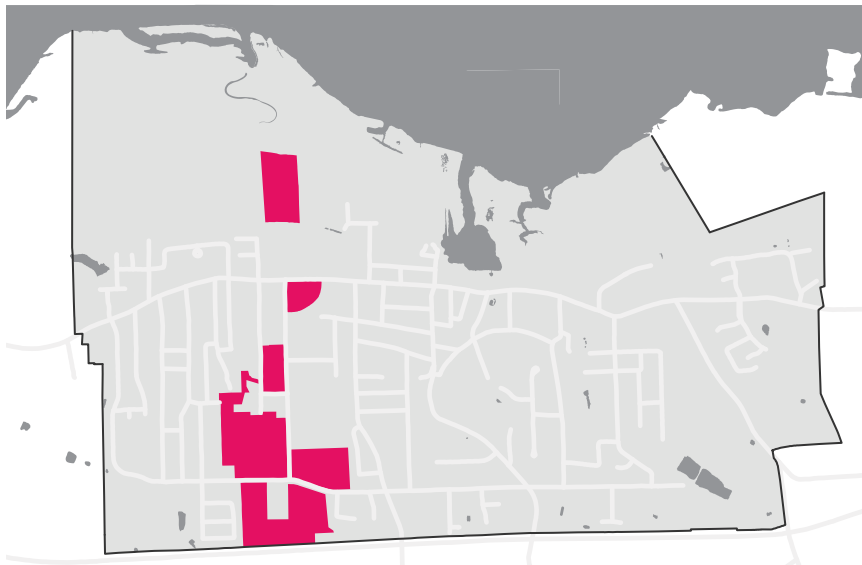
Sports fields



Trails



Natural areas



Other Provisions Apply

Part 4:

General Requirements for All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 19:

General Requirements for Institutional Zones

21.4 BUILT FORM STANDARDS

See Figure 23.1 for all built form standards in the P-2 Zone

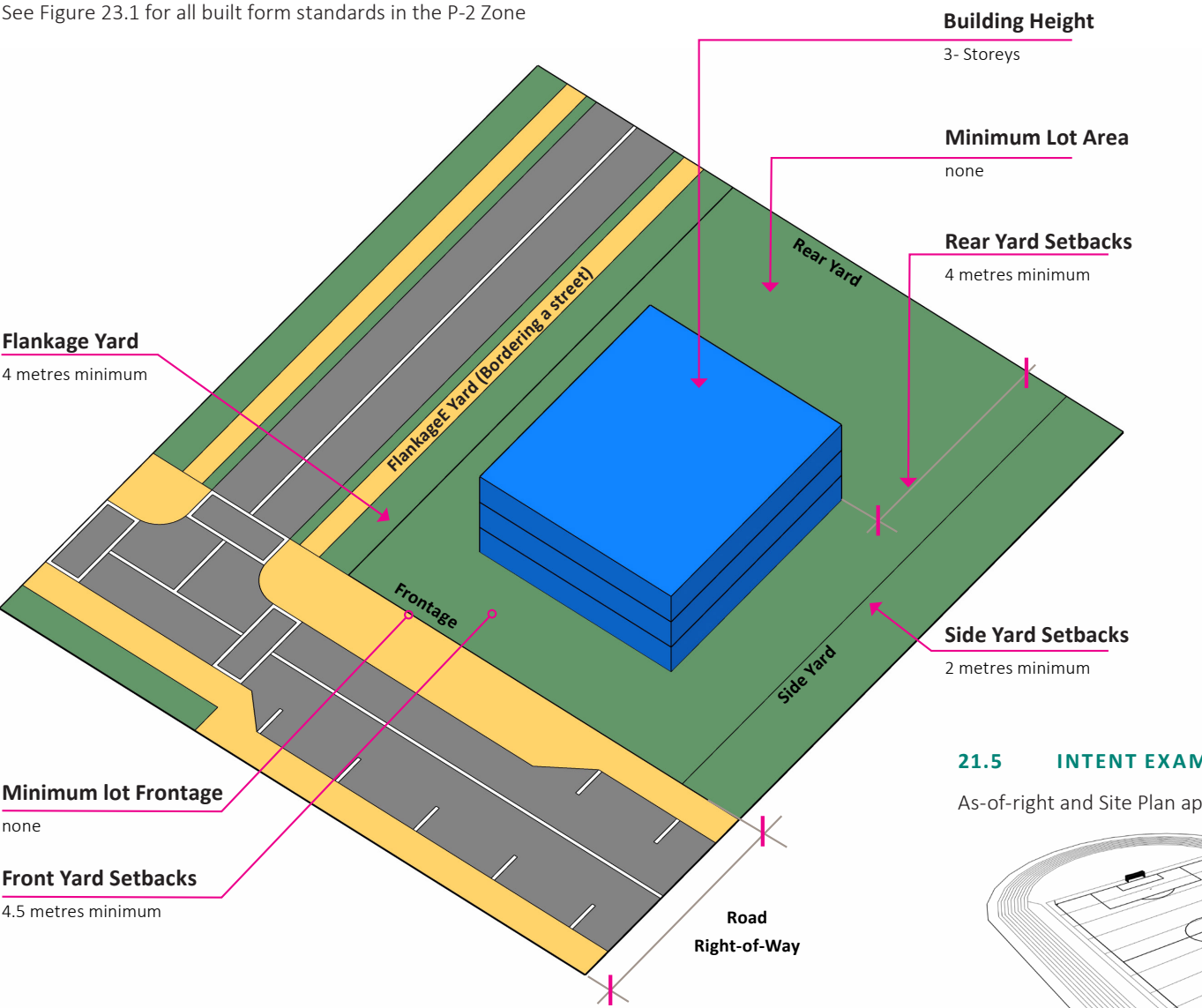
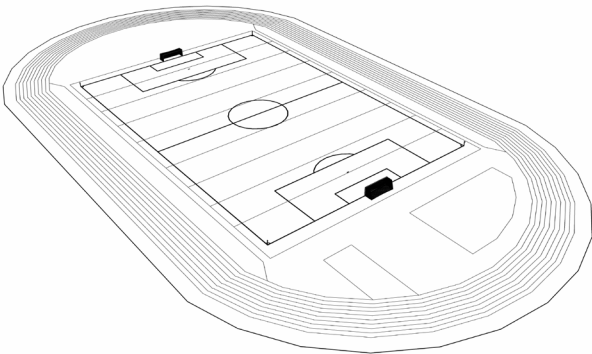


FIGURE 21.1 P-2 ZONE Built Form Standards

21.5 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the P-2 Zone





DRAFT

Town *of* Wolfville MUNICIPAL PLANNING STRATEGY

APPROVED BY COUNCIL JUNE 30, 2020 EFFECTIVE SEPTEMBER 3, 2020

AS AMENDED JUNE 20, 2023

DRAFT UPDATES FOR REVIEW OCTOBER 9, 2025



PART 1

INTRODUCTION

Disclaimer

This part of the MPS is administrative in nature and may be updated as we move toward adoption of the Planning Documents.

1.1 Purpose

This Municipal Planning Strategy (MPS) has been prepared in accordance with the provisions of the Municipal Government Act Chapter 18 of the Statutes of Nova Scotia, 1998, as amended. Application of this MPS and the associated Land Use Bylaw, Design Guidelines and Subdivision Bylaw relate to land within the bounds of the incorporated Town of Wolfville, as shown in Figure 1.1.

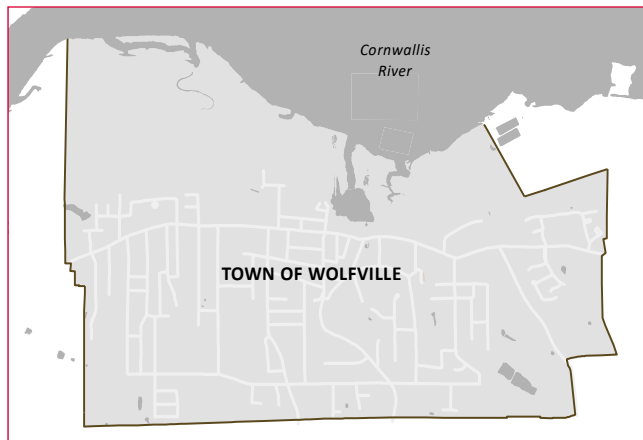


Figure 1.1—Bounds of the Town of Wolfville

The MPS is Council’s principal guide for decisions on land use and development matters within the Town and is critical to the municipality’s long-term strategic direction. The MPS serves as a guide for residents and interested citizens, in particular for those engaged in residential or commercial development, to gain a more thorough understanding of the Town’s growth management approach.

The Land Use Bylaw, Design Guidelines and Subdivision Bylaw are the companion documents to the MPS and are the means by which Town Council will carry out the intent of the MPS, as set out in the Municipal Government Act.

This Municipal Planning Strategy aims to:

1. **Guide Town decisions:** By guiding growth and change in accordance with the community’s priorities, needs and vision for the future.
2. **Provide clarity for managed growth:** By providing clarity on where, and what type of growth should occur in the Town and how it can be supported. It is intended to provide direction for businesses, land owners, developers, organizations, community groups and residents regarding future development.
3. **Influence the Town’s built form:** By informing public realm improvements, street enhancements, park and open space plans, and other municipal land and design decisions. It also provides clear Downtown built form parameters detailed in the Land Use Bylaw and Design Guidelines.
4. **Improve development decision making:** This plan introduces different tools and provides clear parameters. Processes should respect all involved in moving the Town toward its stated community priorities.

The Municipal Government Act (S. 213) outlines the purpose of a Municipal Planning Strategy as follows:

- Address problems and opportunities concerning the development of land and the effects of development;
- Provide a framework for the environmental, social and economic development within a municipality;
- Are consistent with the intent of the statements of provincial interest; and
- Specify programs and actions necessary for implementing the municipality planning strategy.



Figure 1.2—Factors affecting the MPS and its evolution moving forward

To be effective, the Town’s Municipal Planning Strategy informs the Town’s 10 year Capital Investment Plan; is influenced by Asset Management realities; informs Committee and Administrative (Staff) workflows; aligns with other Town Strategies and plans; and is considered regularly for alignment with Council’s Strategic Plan and priorities.

To ensure it remains relevant, this Municipal Planning Strategy is meant to be a living document. Amendments to this plan may be necessary to respond to complex challenges or take advantage of emerging opportunities.

IT SHALL BE THE POLICY OF COUNCIL THAT:

1. This Municipal Planning Strategy (MPS) supersedes the MPS approved by the Minister of Municipal Affairs on June 20, 2023 (including amendments up to the adoption of this plan).
2. This Municipal Planning Strategy shall serve as the Towns Integrated Community Sustainability Plan (ICSP).
3. Council shall have regard to the policies contained within the MPS and no development shall be permitted which is contrary to the overall intent.

1.2 Review Process and Plan Structure

1.2.1 REVIEW PROCESS

In 2025, the Town of Wolfville began a major update to its Municipal Planning Documents. This review is driven by the federal Housing Accelerator Fund agreement, signed in 2024, and the ongoing housing crisis. It also ensures the Town's plans reflect current community needs, growth trends, and priorities. The update focuses on enabling more housing development, simplifying planning processes, and providing clear direction to help the Town meet its housing targets.

- Promote economic vitality
- Embrace the Town's culture and heritage
- Foster a dynamic and livable Downtown
- Inspire high quality design, and
- Encourage social and environmental sustainability
- Expedite housing approvals to alleviate housing crisis
- Reduce barriers to affordability and homeownership
- Support growth of the non-market housing sector

An extensive community engagement programme was undertaken and a large number of background reports and studies were produced or reviewed in developing this Municipal Planning Strategy.

The importance of quality process, analysis and community engagement are at the forefront of policy development in Wolfville.

This plan aims to improve land use policy and process to meet the goals of the review and align with the shared future and community priorities outlined in Part 2.

1.2.2 PLAN STRUCTURE

This Plan consists of policies and maps which are implemented through the Land Use Bylaw, Design Guidelines and Subdivision By-law (implementing documents). While this Plan is organized into distinct parts, for any part to be properly understood it must be read in the context of the whole document, implementing documents, and mapping.

Although not exclusively a land use document, the Town is striving to effectively deal with the three primary aspects of land use through this MPS and implementing documents:

1. **the Arrangement of the Town** — the design of the parcel, block, and transportation (streets, trails, etc) network. These are the 'bones' of the Town and without good ones, it is difficult to realize a vision, adapt to changing conditions, and create places that lend to a high quality of life.
2. **the Use of land** — the purpose for which land is utilized (e.g. residential, commercial, mixed use, agriculture, etc). This is the root of the traditional land use zoning approach. The Town will continue to use this approach but be open to new ideas (e.g. form based) and other tools (e.g. secondary planning strategies) to better manage community expectations.



3. **the Form of land improvements**—the design of improvements on a property (e.g. attached or detached buildings, setbacks, height, etc) are often the details that make the difference between a successful development and something that seems ‘out of place.’ The Town will continue to use design guidelines, which will form a part of this plan’s implementation documents.

Mapping and other graphical communication tools such as design guidelines are essential to illustrate and contextualize land use policy. The Maps included as Schedules or in the body of this Municipal Planning Strategy or Land Use By-law include: a Future Land Use Map, Parks, Open Space and Mobility Network, Streets, Land Use Zoning, the Development Constraints Area Overlay, and Design Guidelines Overlay. These maps are a legal part of these planning documents and are required to properly interpret policy.

ACTIONING OUR COMMUNITY PRIORITIES

To provide clear linkages to the Town’s stated Community Priorities, a policy key / legend has been prepared. Policy in the document has been linked back to themes related to the Town’s stated community priorities. Also included are indications where future work or partnerships will be essential. The policy key is included here for reference.

POLICY KEY

EP	Economic Prosperity	LU	Land Use and Design
SE	Social Equity	CA	Climate Action
P	Partnership	FW	Future Work

1.3 Community Past & Present

1.3.1 WOLFVILLE'S PAST

Where Wolfville stands today, where the Cornwallis (Jijuktu'kwejk) River meets the Minas Basin, was originally inhabited by aboriginal tribes leading a migratory and nomadic existence driven by available sustenance. The Mi'kmaq became the dominant culture in A.D. 1400 and continued to live nomadic lifestyles in the region. By 1650, an estimated 3000 Mi'kmaq peoples resided in the maritime provinces, yet by 1841 only an estimated 1166 remained and were being located on reserves (Mud Creek, J. Davison, 1985).

In 1650, the first Europeans (French Acadians) began to settle in this region at Port Royal and began moving up the Bay of Fundy into the Minas Basin area. From this time until their expulsion by the British in 1755, the Acadians developed dykes and the aboiteau wooden sluice system that transformed brackish floodplains into rich farmland, giving Wolfville and Grand Pre (now a UNESCO Cultural Landscape) their distinctive agricultural fields and dykelands.

After the expulsion of Acadians, New England Settlers arrived to inhabit the vacant land. These settlers founded and developed the community of Mud Creek, now known as the Town of Wolfville, around shipbuilding, various forms of agriculture and supporting industry and commerce. Acadia University (founded 1838) first opened as Horton Academy and has developed into a world class educational institution (Mud Creek, J. Davison, 1985). Mud Creek was renamed Wolfville in 1830, and recognized as "the world's smallest port".

After confederation in 1867, The Windsor and Annapolis Railway (later the Dominion Railway) connected Wolfville through the Annapolis Valley, making Wolfville Harbour a seaport that shipped Annapolis Valley apples across the world.

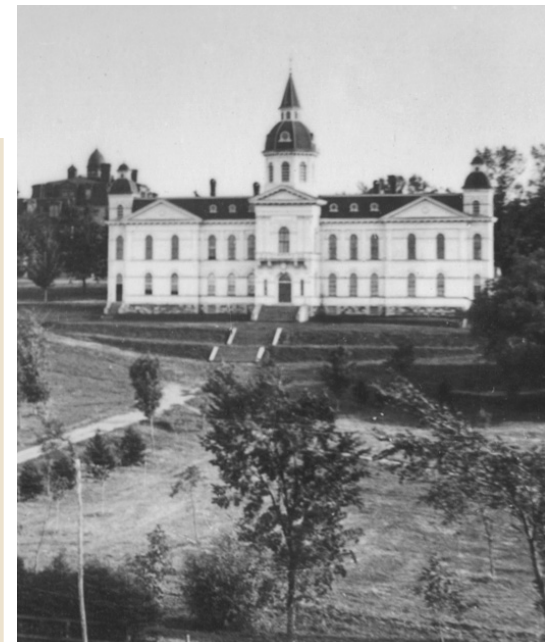
IT SHALL BE THE POLICY OF COUNCIL:

FW P

1. To acknowledge that we are in Mi'kma'ki, the ancestral and unceded territory of the Mi'kmaq People. This territory is covered by the "Treaties of Peace and Friendship" which Mi'kmaq and Wolastoqiyik (Maliseet) People first signed with the British Crown in 1725. The treaties did not deal with surrender of lands and resources but established the rules for what was to be an ongoing relationship between nations.
2. To update the Community Profile and Housing Needs Assessment every 5 years or as needed.
3. To have regard for the Community Profile and Housing Needs Assessment when considering land use planning.



Watercolour painting depicting traditional Mi'kmaq settlement c.1791 (Nova Scotia Museum)



1.3.2 WOLFVILLE PRESENT

Wolfville now represents a cultural centre within the County of Kings offering historic and contemporary built forms, community and international events, and strong ties with Acadia University. The active, educated, residents have worked to proclaim Wolfville a nuclear free zone in 1985, declared Canada's first fair trade town in 2007, and designated Wolfville as the third Cittaslow community in Canada in 2016. The Town has been, and continues to be, an aspirational community and an important educational and cultural centre in the region.

To ensure the Town of Wolfville's sustainable future requires an equitable balance of many factors in decision making. Understanding opportunities and challenges is critical, as is recognizing possible disruptions that may directly influence, or change, our community. It is important to be able to adapt to these changes while maintaining stability, innovation and the ability to pursue a unique path so Council can take actions that reflect the values of the community. Council endorses a sustainable and socially equitable community development approach to these factors and the implementation of this approach is enabled through the policies of our Municipal Planning Strategy.

1.3.3 OPPORTUNITIES AND CHALLENGES

CLIMATE CHANGE AND THE ENVIRONMENT

Wolfville encompasses an array of environmental features that define the Annapolis Valley. Wetlands, watercourses, agricultural fields, and undisturbed natural areas surround the town and flow through its downtown. The Town is surrounded by a greenbelt of forested and agricultural lands and bordered by the Minas Basin and Bay of Fundy to the north. The waters are home to whales, unique ecosystems, and the bordering cliffs and foreshore contain ancient fossils. The dykelands, an iconic piece of Nova Scotia's heritage, were constructed to capture nutrient-rich sediment in low-lying coastal areas, and the rich salt marshes created ideal agricultural conditions provided in part by water carried by steep southern slopes that border the commercial areas.

Wolfville's unique environmental features continue to play an important agricultural role for the Annapolis Valley, and should be protected and maintained as they contribute to the ecosystem services and resources required for economic sustainability and resilience.

Opportunities and Challenges:

Environmental conservation and protection

Ecosystem services

Climate Change adaptation and mitigation efforts.



DEMOGRAPHICS

Wolfville is home to approximately 5,247 residents, as outlined by Statistics Canada's 2021 Census. Between the 2016 and 2021 Census, Wolfville saw record population growth of 20.5% overall, or 4.1% per year. This population increase was driven primarily by the COVID-19 pandemic, which resulted in a higher number of people relocating to Nova Scotia and to rural and small towns where there were fewer COVID cases, more open spaces and lower housing prices. For comparison, the Town grew by only 2% annually between 2011-2016. The Town's Community Profile projects population growth to slow in the coming years and suggests a 5.9% population growth from 2024-2034.

There is a mismatch between available housing types in Wolfville and what residents need. Roughly 40% of houses have 3 or more bedrooms, but only 20% of households have 3 or more people. A large portion of the permanent population in Wolfville are retirees, a population that has their own set of residential and social needs such as age-friendly infrastructure and accessible transit. In contrast, approximately half of Wolfville's residents are university age, a trend constant in all census years.

Each fall Wolfville becomes a seasonal home to 1500 additional residents living in Acadia University residences. Wolfville also receives an estimated 1000 commuting students each day, based on data retrieved from Acadia University. Total enrolment for 2023-2024 was approximately 4,325 students.

Each summer Wolfville welcomes an estimated 191,000 visitors. 25,000 of those visitors stay for an average of three nights, as identified in Nova Scotia Tourism survey-data.

Opportunities and Challenges:

Variety of housing options

Age-friendly infrastructure

Demand for housing of all types

Opportunities for cross-cultural connections

Balancing environmental economics



ECONOMICS

The dykelands from the Bay of Fundy and lands flowing from the Gaspereau Valley have presented ideal agricultural opportunities, which have shaped the local economy for centuries. Educational Services is the largest sector in Wolfville largely thanks to Acadia University, representing 19.6% of the total labour force in the Town according to Census 2021. 13.9% of the labour force is based in accommodation and food services, and 13.3% is centered in Health care and social assistance sector. Retail trade makes up 11.9% of total labour force. Education and local medical service providers are major contributors to the local economy, servicing local and regional needs while attracting international residents and students. Tourism, including retail and accommodations providers, are another major contributor to the local economy.

Despite these areas of growth, unemployment in Wolfville continues to climb. The unique needs and socioeconomic mix of students, retirees, tourism workers, agriculturalists, and professionals requires a concerted effort to balance a variety of economic opportunities with a growing need for housing and employment options for those under or unemployed.

Opportunities and Challenges:

- Affordable housing options
- Urban agriculture
- Renewable energy sources
- Seasonal demand for tourism services
- Commercial and Retail opportunities

GLOBAL INFLUENCES

The Town of Wolfville has enjoyed the benefits of a high quality of life, a diverse economy, and a wide range of temporary and permanent residents of all ages and ethnicities due in part to regional, national and global influences. These global trends can dramatically shift due to circumstances outside of our control, such as global politics, climate change, national population changes, immigration, new technologies, regional policies and even other unknown factors. In recent years, trends in Nova Scotia continue to indicate growing populations of retirees in relation to younger populations, ever-increasing impacts of climate change and erratic weather patterns, the growing costs and impacts of fossil fuels, and a long-term need to attract younger Canadian and International residents. These trends are not unique to the Town of Wolfville, yet impact Wolfville uniquely and should be considered as we consider how they impact the quality of life for Wolfville residents today, in the near future, and in the lives of future generations.

Opportunities and Challenges:

- Climate change
- Canadian Migrations
- Technology
- Fossil Fuels
- Generational and Social Equity



1.4 Provincial and Regional Coordination

1.4.1 PROVINCIAL COORDINATION

A Municipal Planning Strategy provides the policy framework for land use and development control. The Municipal Government Act (MGA) provides Council with the power to make statements of policy with respect to a broad range of activities including future development, land use, public lands, transportation, municipal services, municipal development, coordination of public programs, and any other matters related to the physical, social or economic development of the town.

Alongside the MGA, the Province provides Statements of Provincial Interest to ensure municipalities are complying with issues of province-wide significance. Municipalities must take the statements into account when creating or reviewing land use planning policies and regulations. The Statements of Provincial Interest address the areas of Drinking water supplies; Flood Risk Areas; Agricultural Land; Infrastructure; Housing and The Nova Centre. Each Statement has a set of provisions that dictate how municipal planning documents must respond to the goal of the Statement. The Nova Centre Statement is excluded as it is specific to a development in the Halifax Regional Municipality.

IT SHALL BE THE POLICY OF COUNCIL:



- 1) To create Municipal Planning Strategy Policy that complies with the Statements of Provincial Interest:



STATEMENT 1: DRINKING WATER SUPPLIES

GOAL: To protect the quality of drinking water within municipal water supply watersheds.



STATEMENT 2: FLOOD RISK AREAS

GOAL: To protect public safety and property and to reduce the requirement for flood control works and flood damage restoration in Flood Risk Areas by restricting exposure to flood risk.



STATEMENT 3: AGRICULTURAL LAND

GOAL: To protect agricultural land for the development of a viable and sustainable agriculture and food industry.



STATEMENT 4: INFRASTRUCTURE

GOAL: To make efficient use of municipal water supply and municipal wastewater disposal systems.



STATEMENT 5: HOUSING

GOAL: To provide housing opportunities to meet the needs of all Nova Scotians.

1.4.2 REGIONAL COORDINATION

The core message of the One Nova Scotia report (“The Ivory Report”) by the Nova Scotia Commission on Building Our New Economy is that Nova Scotia, particularly rural Nova Scotia, faces a crisis of economic and demographic viability. This is not inevitable, but leveraging the province’s assets to change its economic outlook will demand a shared vision and a commitment to economic growth and renewal.

The County of Kings has its own Municipal Planning Strategy (MPS) and Land Use Bylaw that provide a regional perspective beyond the Town of Wolfville. This regional context is important to consider as Wolfville evaluates its options for future growth.

The Kings 2050 initiative was developed as part of the Kings County MPS update. It was developed in collaboration with the Towns of Berwick, Kentville, Wolfville and the Municipality of the County of Kings, and provides a joint vision for the future of the region. The “Kings 2050 Vision” document is the main deliverable of the second phase of the Kings 2050 process and attempted to draft a set of Statements of Regional Interest, intended to be the ‘foundation for a regional approach to land use planning’. Four broad issues are addressed through four Statements of Regional Interest.

While the MGA does not currently outline the requirement for a Municipal Planning Strategy to demonstrate consistency with Statements of Regional Interest, a best practice approach would still have regard to their intent.

IT SHALL BE THE POLICY OF COUNCIL:

FW P

1. To have regard for the Statements of Regional Interest as outlined in the Kings 2050 Regional Approach Report.
2. To be open to collaborating with the County of Kings and other regional partners on regional land use planning, climate change mitigation and adaptation, environmental protection, recreation, transit, economic development and other matters.
3. To include in the Town’s Public Participation Program Policy provisions to ensure engagement with abutting municipalities.

STATEMENTS OF REGIONAL INTEREST:

Four broad issues are addressed through four Statements of Regional Interest. These include:



Settlement

To ensure an effective, efficient, equitable and focused pattern of development that will support planned residential growth in response to the needs of the public.



Economic Development

To encourage the development of the Kings regional economy for the benefit of all communities and residents.



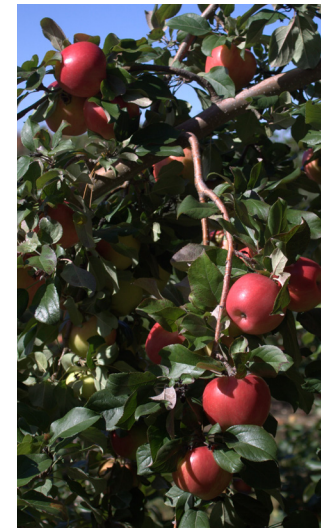
Municipal Facilities

To ensure the efficient provision of cultural, recreation and other facilities required to serve and enhance the lives of all residents in the Kings region.



Environmental Protection

To ensure the protection and enhancement of valued features of the natural environment in the Kings Region.





PART 2

OUR SHARED FUTURE AND COMMUNITY PRIORITIES

Disclaimer

Small amendments have been made to this part of the MPS to reflect the current Council's strategic planning.

Further updates will be made to better align this Part of the document with Council's direction as we move toward adoption.

2.1 Sustainability and Resilience

The Town of Wolfville is a leader in the pursuit of improved community well-being and is regarded as one of the most progressive small towns in the country.

The Town of Wolfville embraces sustainability as a lens to create an integrated and effective approach to addressing current and future challenges. The figure to the right illustrates the ‘nested circle’ sustainability framework and supports Council’s belief that social and economic activity occur within ecological limits. This concept of sustainability contrasts with the ‘three legs’ (or other approaches) based on the premise that the social, ecological and economic aspects are equally important. The nested circle framework recognizes that social and economic activity occurs within ecological limits.

The Town of Wolfville is also committed to being a resilient community where ‘disruptions’ can be managed effectively. It will be important to adapt to change while maintaining stability into the future. Community stability allows for innovation and the ability to pursue a unique path where Council can take actions that reflect the values of the community.

Council and Staff will be bold to forge a path that is in the best interests of both current and future generations of our Town and region.

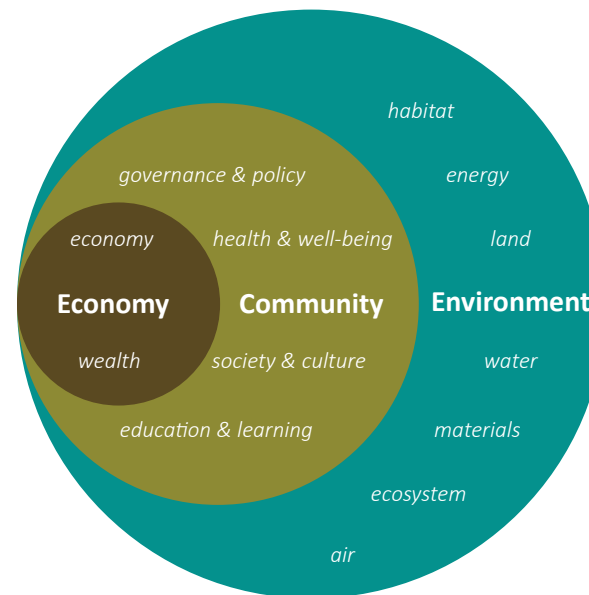
IT SHALL BE THE POLICY OF COUNCIL:

FW P

To institutionalize and promote the “Nested Circle” Sustainability Framework in both municipal operations and the overall community.

Core Concept:

“Nested Circle” Sustainability Framework



2.2 Our Shared Future

Wolfville is an aspirational community. “Our shared future” is a declaration of the Town’s desired outcome during the lifetime of this plan. It is what we want to be, what we’re currently doing and our reason for existing. In the context of a Municipal Planning Strategy, the “shared future” is the Town’s desired outcome from a growth management perspective.

Our Shared Future is...

An inclusive and vibrant community where natural beauty, heritage, innovation and well-being bloom.



2.3 Community Priorities

Community priorities are specific statements of what the Town intends to achieve during the life of this plan. They are the WHAT and the HOW — split into 4 priorities with objectives and anticipated results associated with each. The priorities fall into 4 categories that together provide a road map to achieving ‘Our Shared Future.’ The community priorities are guided by the Town’s commitment to sustainable community development, resilience, community consultation, and evidence-based decision making.

The policies of this Plan shall be consistent with and supportive of these priorities. Council acknowledges that priorities and/or specific objectives must be monitored and may change over time. Council is committed to monitoring the effectiveness of these policies and making changes as appropriate — this is a living document. It is important to acknowledge that some of these priorities will and do compete with each other. The challenge for Council will be finding an appropriate balance, consistent with our overarching sustainability aspirations.

ACTIONING OUR COMMUNITY PRIORITIES

To provide clear linkages to the Town’s stated Community Priorities (See Part 2), a policy key / legend has been prepared. Policy in the document has been linked back to themes related to the Town’s stated community priorities. Also included are indications where future work or partnerships will be essential. The policy key is included here for reference.

POLICY KEY

EP	Economic Prosperity	LU	Land Use and Design
SE	Social Equity	CA	Climate Action
P	Partnership	FW	Future Work

2.3.1 LAND USE AND DESIGN

The Town believes that creating and preserving places where people want to be—to live, work, play, and visit—is critical for the long-term prosperity of the Town. Land Use and Design decisions are important to the quality of life the Town can offer and plays a critical role in resident's overall health. Our unique sense of place captures the imagination of long-time residents and visitors alike. Looking forward, Council will strive to encourage innovative design and housing options while honouring existing character and heritage. The Land Use and Design objectives guide policies of this Municipal Planning Strategy and are identified with a symbol throughout this MPS.

CORE CONCEPT: HEALTHY COMMUNITIES

The design of our communities influence how physically active we are, how we travel through our communities, how socially connected we are, the kinds of foods we have access to, how exposed to the natural environment we are, and ultimately, how we experience health and wellness.

CORE CONCEPTS LU

Complete Communities

Meet people's needs for daily living throughout an entire lifetime by providing convenient access to an appropriate mix of jobs, local services, a full range of housing, and community infrastructure, schools, recreation, open space, and mobility options.

Human Scale

To see the Town as the people who use and experience it at eye level and to optimize the pedestrian experience by taking the possibilities and limitations dictated by the human body into account.

Pedestrian First

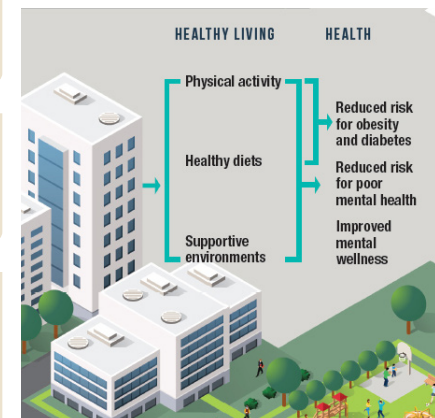
Providing safe, comfortable, direct and attractive walking and rolling routes separated from motor vehicle traffic for pedestrians to enjoy the surrounding amenities.

Neighbourhood Character

A subjective measure that combines public and private realms and the cumulative impact of all contributors (property, public place, infrastructure) that create a unique character.

New Urbanism

Approach based on the principles of walkable streets, amenities in close proximity, accessible and well-designed public spaces that create sustainable, human-scaled places where people can live healthy and happy lives.



Source: The Chief Public Health Officer's Report on the State of Public Health in Canada 2017 – Designing Healthy Living.

OBJECTIVE 	OBJECTIVE 	OBJECTIVE 	OBJECTIVE 	OBJECTIVE 
To create complete neighbourhoods to maximize our infrastructure and promote community health.	To ensure a full range of housing options for Wolfville residents of all ages and incomes.	To respect and strengthen existing neighbourhood character, while encouraging quality, inclusive urban design.	To preserve and enhance our architectural and cultural heritage assets.	To support public art and cultural programming.
ANTICIPATED RESULTS • Create and incentivize neighbourhood commercial to create convenient access to services for all residents. • Focus on walkability improvements. • Strive to integrate Asset Management and Planning.	ANTICIPATED RESULTS • Increase housing options based on community needs. • Encourage affordable housing options (including non-market). • Set non-market housing targets and monitor over time.	ANTICIPATED RESULTS • Improve processes and requirements for infill development. • Create tools for improved social inclusion of all residents. • Policies to support innovation.	ANTICIPATED RESULTS • Identify and register heritage properties. • Better establish roles for the Town's Heritage Advisory Committee and Historical Society.	ANTICIPATED RESULTS • Institutionalize public art appreciation. • Work with and support action by community groups, non-profits and other partners.

2.3.2 ECONOMIC PROSPERITY

To grow its economy, Wolfville is focused on enhancing its position as an educational and cultural hub, a destination for tourism, niche/boutique retail, and professional services while further developing offerings around food, libations (wine, cider, beer, etc) to leverage the Town's CittaSlow designation and brand proposition. The Economic Prosperity objectives guide policies of this Municipal Planning Strategy and are identified with a symbol.

EP

CORE CONCEPTS

EP

Strategic/Managed Growth

Using municipal assets such as land, infrastructure and water resources efficiently; building more compact, walkable communities with adequate infrastructure; encouraging infill development Downtown; encouraging missing-middle and gentle density in residential neighbourhoods; and planning neighbourhoods near amenities such as schools, jobs and recreation to encourage sustainable community development.

Fiscal Sustainability

To achieve a fiscal stance that allows the Town to service public debt, without the need to undertake policy adjustments that are implausible from an economic or political standpoint, given financing costs and conditions we face.

CittaSlow

The Town of Wolfville was officially designated as a Cittaslow community in April 2016. To be designated, a community must fulfill over 50 criteria addressing environmental protection and healthy lifestyles, support for local products, agriculture and artisans, community engagement, social justice, celebration of and respect for local culture, heritage and traditions, and the thoughtful development and use of technology for sustainability and community well-being.

OBJECTIVE EP	OBJECTIVE EP	OBJECTIVE EP	OBJECTIVE EP	OBJECTIVE EP
To encourage growth and manage land use with a goal of balanced economic and environmental sustainability.	To ensure a vibrant local economy by supporting economic development, creative entrepreneurship, and home based business endeavours that further the Town's position as a regional cultural, tourism, and educational centre.	To enhance and strengthen the Downtown as the focal point of commercial and community activity.	To encourage a range of local and regional partnerships that enhance economic development within the Town.	To support what works and encourage new opportunities and investments.
ANTICIPATED RESULTS - Expand neighbourhood commercial opportunities. - Focus on infill development and gentle density, - Focus investments in public services that are reflective of community need.	ANTICIPATED RESULTS - Strive for top quality facilities and services, including an enhanced public library. - Further our brand as a hub of tourism, food and libations through additional opportunities along Main Street and in the Downtown.	ANTICIPATED RESULTS - Design Guidelines and visioning specific to the Downtown. - Focus on infill and walkability.	ANTICIPATED RESULTS Strengthen partnerships with Acadia, the County of Kings, other Towns/ Municipalities, the Wolfville Business Development Corporation, Devour, Deep Roots and other partners and events.	ANTICIPATED RESULTS - Better development approvals process (e.g. fewer development agreements, more site plan approvals). - Focus on walkability and the Downtown.

2.3.3 CLIMATE ACTION

The Town intends to lead the way in community well-being and environmental health by finding a balance between responding to a demand for short-term benefits and creating opportunities for creating more sustainable neighbourhoods that will continue to support the community in the future. We anticipate encouraging sustainable practices and initiatives that respond to climate change implications and resource preservation. The Climate Action objectives guide policies of this Municipal Planning Strategy and are identified with a symbol throughout this MPS. 

CORE CONCEPTS

Adaptation

Strategies and Actions that manage the impacts of climate change, including climate variability and extremes.

Mitigation

Strategies and Actions that reduce the emissions that cause climate change.

Ecological Footprint

The impact of human activities measured in terms of the area of biologically productive land and water required to produce the goods consumed and to assimilate the waste generated.

Energy Poverty

Individuals who are unable to afford the energy/fuel needed to maintain their life (a household that spends more than 10% of their income on home energy).

Clean Energy

Energy sources including solar, wind, water, geothermal, and bioenergy.

Active Transportation

Any form of human-powered transportation through many modes such as walking and cycling that encourage less vehicle transportation.

OBJECTIVE CA	OBJECTIVE CA	OBJECTIVE CA	OBJECTIVE CA
To control land use in a way that preserves, enhances and protects the natural environment to ensure open space opportunities for all residents.	To promote clean energy and reduce greenhouse gases (Climate Mitigation) by maximizing energy efficiency through conservation, local renewable energy opportunities, partnerships, and the use of sustainable infrastructure and green building design.	To undertake climate adaptation and mitigation measures to create a community resilient to disruptors.	To create a leading sustainable transportation network that supports non-vehicular transportation modes and improves connectivity.
ANTICIPATED RESULTS • Enhance environmental constraints • Preservation of ecosystem services, including Agricultural land. • Ensure source water protection and reduce water consumption.	ANTICIPATED RESULTS • Work toward advancing our position in the Partners for Climate Protection Program through GHG emissions reduction and energy planning work.	ANTICIPATED RESULTS • Work with partners to preserve and enhance resilience to sea level rise. • New requirements for buildings in flood prone areas. • Educate the community about climate change implications and initiatives.	ANTICIPATED RESULTS • Better leverage our partnership with Regional Transit Authorities. • Prioritize the creation of walkable and bikeable infrastructure. • Explore community/micro transit options.

2.3.4 SOCIAL EQUITY

Throughout the process of developing this plan, ‘Social Inclusion’ and ‘Equity’ have been key themes. The objectives reflect support for social and economic development of the town, and community members whose lives are affected by change, while trying to respect the Community’s vision of how we develop. The Town encourages contributions to community development and social wellbeing through policy by reflecting on current social structures and actively communicating with the public. The Social Equity objectives guide policies of this Municipal Planning Strategy and are identified with a symbol throughout this MPS. SE

CORE CONCEPTS SE

Universal Design

Design and composition that can be accessed, understood and used to the greatest extent possible by all people regardless of age, size, ability or disability.

Community Engagement

Providing participation opportunities for community members to voice opinion and have input on current municipal events and decision making, while providing inclusivity and transparency throughout the community-level planning process.

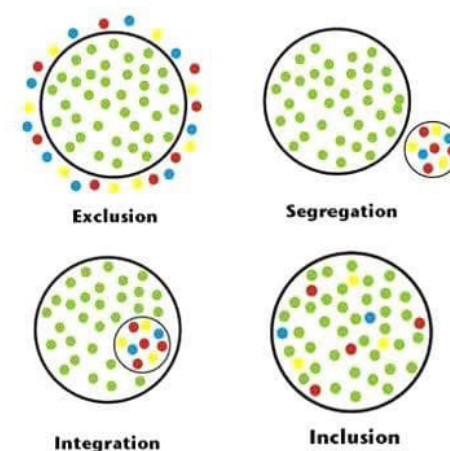
Age Friendly

A place that has an inclusive and accessible urban environment that promotes active aging and influences health and quality of life by providing the ability for older adults to age, in place.

In 2025 the Town adopted the 2025-2028 Accessibility Plan to ensure that all people can take part in community life and in society, whatever their abilities. This Plan has 5 areas of focus: built environment, information and communication, employment, goods and services, and transportation.

CORE CONCEPT: SOCIAL INCLUSION

When building new housing that promotes social inclusion the United Nations states “housing is not adequate if it is cut off from employment opportunities, health-care services, access to transit, schools, childcare centres and other social facilities, or if located in polluted or dangerous areas.



Source: Government of Canada- National Housing Strategy—“A Place to call Home”

OBJECTIVE SE	OBJECTIVE SE	OBJECTIVE SE	OBJECTIVE SE	OBJECTIVE SE
To make land use processes understandable, transparent and efficient.	To ensure an inclusive community through bold leadership, community education and inclusive engagement in the development and implementation of land use planning policy.	To work in alliance with our regional partners.	To provide age friendly, accessible, and affordable housing options for a mix of demographics to encourage social inclusion.	To ensure that all public facilities and infrastructure provide access to all potential users, and encourage other organizations to follow these standards.
ANTICIPATED RESULTS - Clearly articulate development requirements. - Provide clear parameters around community involvement in development applications.	ANTICIPATED RESULTS Regular community check-ins on growth management and what we are trying to achieve.	ANTICIPATED RESULTS Strengthen partnerships with NS Department of Seniors, Housing NS, NS Department of Health, Acadia University, and other partners.	ANTICIPATED RESULTS Ensure variety and adaptability in our housing options.	ANTICIPATED RESULTS Work with the Accessibility Advisory Committee to meet Accessibility goals as established in the Town's Accessibility Plan for 2025-2028.



*Our planning strategy is
how we, as human beings,
organize ourselves.*

*It's how we physically embody
our values in the built environment.*



PART 3

INVESTMENT FOR SUCCESS

Disclaimer

Small amendments have been made to this part of the MPS to reflect the current Council's strategic planning.

Further updates will be made to better align this Part of the document with Council's direction as we move toward adoption.

3.0 Creating Conditions for Success

This Municipal Planning Strategy emphasizes the importance of the public realm in creating an environment that attracts and supports growth, consistent with the Town's community priorities. Municipal investments in public buildings, water and wastewater systems, streetscapes, active transportation infrastructure, parks and open space, and other public infrastructure and facilities will be needed and should be coordinated with the policies of this plan to effectively work toward the Shared Future envisioned.

The Town of Wolfville is a municipal corporation managing a growing community that must balance change and innovation with ongoing support for existing infrastructure — physical and social.

The Municipal Planning Strategy is the Town's primary tool for managing growth and change yet this task requires us to understand how municipal assets, our economic development opportunities, and our culture and heritage assets are balanced so we can spend our resources (e.g. time, limited financial resources, etc.) toward achieving our shared future.

The way the Town invests in infrastructure assets, including natural assets, will have a significant impact on whether we move toward becoming more sustainable and resilient. Because of this, Council is committed to better integrating Land Use Planning and Asset Management, starting with the policies of this plan and focusing on improvements, over time, to ensure our Municipal, Economic, and Cultural success.

IT SHALL BE THE POLICY OF COUNCIL:

FW P

1. To incorporate policies and provisions of this Strategy into the capital budgeting and operational planning processes of the Town.
2. To ensure that the Town's capital budgeting and operational planning process for streets and sidewalks give consideration to projects based on the condition of the asset, risk rating, desired growth, pedestrian activity and community priorities.
3. To incrementally integrate asset management and land use planning for municipal, economic and cultural success.

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

3.1 Asset Management

In 2017-2018 R.V. Anderson Associates Ltd. was contracted to prepare an Asset Management Plan for the Town. The objective was to establish an initial plan that provides the information necessary for staff to ensure Town infrastructure is safe, sustainable and addresses the needs of the residents and businesses of Wolfville.

The Town of Wolfville currently operates two distinct sets of asset inventories. One is the Water Utility, composed of water facilities (water treatment facility, wells, and a concrete reservoir), water mains, and equipment. The other is the General Fund, composed of land improvements, buildings and structures, equipment, IT infrastructure, traffic lights and poles, sewer systems, sewer plants, sidewalks, streets and curbs, and vehicles and heavy equipment. These assets are not funded for long term sustainability which will directly affect resident's quality of life in the event of a failure. It is critical to work towards ensuring these assets are maintained and fully funded for the future.

The Town will continuously improve and refine its asset data and management plan over time. Doing so will allow the Town to ensure the Town can accommodate growth, consistently provide the services residents expect, and become more financially resilient over time.

IT SHALL BE THE POLICY OF COUNCIL:



1. To adopt and apply recognized asset management practices by:
 - a. Maintaining an inventory of the Town's existing infrastructure.
 - b. Forecasting future infrastructure costs, including for maintenance, replacement, and expansion.
 - c. Identifying revenue sources to address these costs, including through future growth.
2. To consider Asset Management and the Town's infrastructure deficit when considering development proposals where new infrastructure is proposed.
3. To consider the policies of the Municipal Planning Strategy and other Town plans when making decisions on Municipal Building Assets.
4. To recognize the value of Natural Assets in the overall management of Town infrastructure (e.g. dykelands as Flood control, watercourses as stormwater management, our drinking water from wells as a groundwater source, etc).
5. To recognize the significant challenges that unmitigated Climate Change could impose on Municipal Assets and how taking action on Climate Change (mitigation and adaption) is linked to Asset Management and the Town's long-term sustainability.
6. To adopt an Asset Management approach to support the Town's commitment to ensuring the sustainable delivery of services.

CORE CONCEPT – ASSET MANAGEMENT

Asset management enables local governments to strategically plan for community sustainability and resilience by:

- Proactively managing assets throughout their life cycle to deliver services sustainably to the community now and into the future.
- Prioritizing infrastructure decisions that balance costs, risks and services.
- Delivering services more efficiently and effectively.
- Ensuring long-term affordability of services.
- Meeting sustainability targets.
- Effectively working toward meeting targets for greenhouse gas emissions reduction.
- Integrating natural and built environments.
- Reducing deficits and debt.
- Attracting business and investment.

(from FCM's "Building Sustainable and Resilient Communities with Asset Management")

3.2 Revenue to Meet Financial Needs

This plan seeks to strengthen Wolfville's financial revenue so that it can meet its forecasted infrastructure needs. Strengthening Wolfville's financial position is critical to ensure the Town can continue to provide and expand the services it provides residents. It is also critical for ensuring Wolfville can prepare for, and respond to, climate risks, such as flooding. Wolfville seeks to strengthen its financial resilience to minimize the risk of insolvency in the face of such challenges.

This plan will therefore encourage growth on existing streets and infrastructure, so that the Town can grow its taxbase while minimizing additional costs. Downtown growth is particularly important for financial resilience. Already, it generates 14% of the Town's tax revenue (excluding special cases, such as Acadia), while comprising only 5% of its land. It is also important, however, to encourage appropriate growth throughout Wolfville neighbourhoods — such as accessory dwelling units, duplexes, triplexes, courtyard housing, and other missing middle development — to increase tax revenue on existing streets without increasing the tax burden on current residents.

IT SHALL BE THE POLICY OF COUNCIL:

1. To strengthen the Town's financial resilience by encouraging growth in areas with available infrastructure capacity.

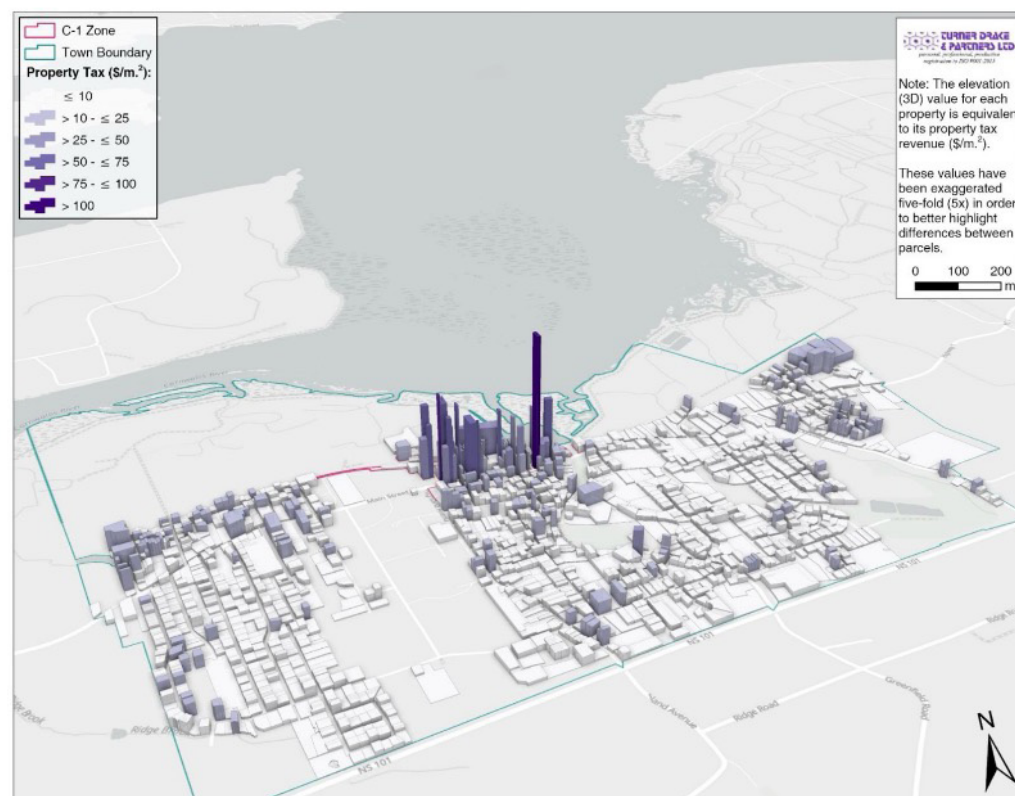


Figure 3.1—Land Value Map, showing tax revenue generated per square metre

3.3 Non-Market Housing

Growth can only benefit all Wolfville residents if all residents can find a home. It is especially important to provide affordable housing inside a walkable community like Wolfville, because when residents live close to shops and services, they enjoy higher rates of employment, greater long-term upward mobility, stronger social ties, and improved health. Often, people are priced out of walkable downtowns because these areas are some of the most desirable for all residents and there is considerable competition for housing.

A key aspiration for this plan is to build support for non-market housing. Wolfville will work with non-market housing providers to find opportunity for dedicated units for those who need them. The plan focuses on non-market solutions with the goal of providing deeply affordable prices that a broader group of residents can afford.

IT SHALL BE THE POLICY OF COUNCIL:

1. To maintain a non-market housing strategy and work to implement the strategy over time.
2. To create a grant program to support non-market housing development, growth and retention.
3. To create a disposal process for disposing of town owned and to non-market housing providers as allowed by the Municipal Government Act.
4. To explore other financial assistance to this sector including property tax adjustments.
5. To waive building and permit fees for non-market housing development applications.

3.4 Culture, Heritage and Economic Development

The economic potential of the municipality needs to be encouraged for residents to be able to live, work, shop and play locally and affordably. Supporting resident initiatives in cultural and small business activities enable Wolfville to adapt to a changing world with economic success. Strategic public investments; supportive partnerships with business, other orders of government, and institutions; and other incentives can act as catalysts for further economic growth and job creation while supporting local cultural activities.

Planning for culture incentivises opportunities for creative-economic activity, increases access and exposure to culture, and preserves and celebrates built and living heritage. Empowering individuals and organizations, and supporting those initiatives is key to fostering the culture and heritage of Wolfville. Residents and visitors bring value and contribute significantly to our rich and evolving cultural fabric through the creation of events, art, restoration of heritage, as patrons, by registering heritage properties, and many other methods.

Individuals are key to economic diversification and supporting culture. One of the best ways to support individuals is to develop the Town of Wolfville as a destination for visitors, new residents and talented workers by investing in high quality education, commercial areas, accessible public infrastructure and cultural activities within the municipality.

IT SHALL BE THE POLICY OF COUNCIL

FW P

1. To work toward all people in our community having equal access to decent livelihood and quality standard of living including equal access to:
 - a. Buildings,
 - b. Infrastructure and Public Spaces,
 - c. Information and Communications,
 - d. Employment,
 - e. Delivery of Goods and Services and Transportation.
2. To preserve and enhance places, sites, structures, streetscapes, archaeological resources, cultural landscapes and practices which reflect the town's diverse evolution, built heritage and culture in partnership with local organizations
3. To consider rezoning to any zone for registered heritage property, as outlined in the Land Use Bylaw
4. To use the Nova Scotia Heritage Property Act to preserve our built heritage and advocate for the strengthening of the Act over time.
5. To conduct a detailed heritage study to better understand heritage assets to protect and tools for heritage protection.
6. To strive to maintain a commercial vacancy rate of 7-10% consistent with Provincial and National averages.
7. To ensure that commercial uses are designed to be compatible with surrounding land uses, including consideration of built heritage and the impacts of land use on community character.
8. To ensure goods are produced and consumed locally using environmentally responsible practices, where possible.
9. To help business succeed through supportive policies and simplified procedures, including timely responses or approvals.



PART 4

CLIMATE CHANGE and THE ENVIRONMENT

Disclaimer

The Town is working through a process to update our Flood Line mapping done in 2020, based on new directions from the Provincial Government both in terms of regulation and the now confirmed changes in top elevation of both dyke walls adjacent to the Town.

4.0 The Defining Issue of Our Time

In 2019 the Town of Wolfville declared a climate emergency and has taken steps toward local adaptation and mitigation action, including setting carbon emission reduction targets and adopting a Climate Action Plan in 2021. As local responses to the issue are better formulated, Council acknowledges that it may be necessary to amend this Municipal Planning Strategy to better align with Climate Change Mitigation and Adaptation planning.

IT SHALL BE THE POLICY OF COUNCIL:

CA FW P

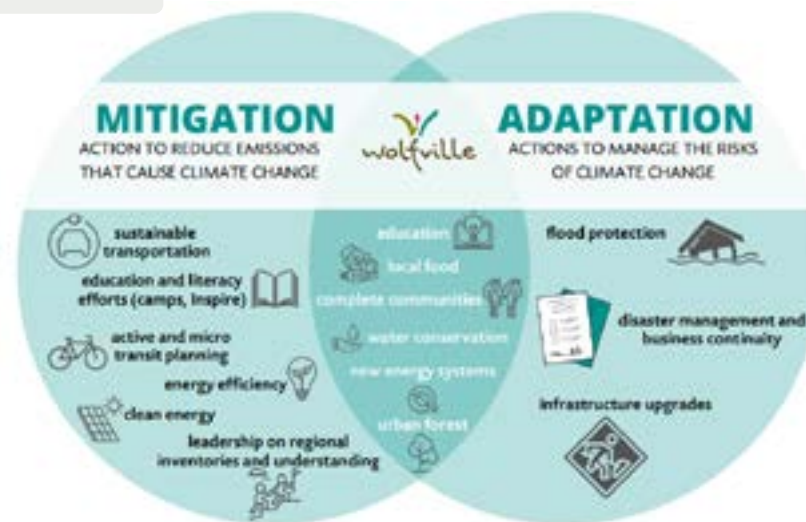
1. To recognize climate changes jeopardize the future of our planet and creates generational inequities.
2. To prioritize Climate Change adaptation and mitigation in the Town's Capital and Operating Plans.
3. To manage Town owned land in an ecologically sustainable manner that reduces individual ecological footprints, consistent with best practice.

4. To investigate means of land protection other than outright ownership, such as conservation easements, expropriation, land trusts, etc.
5. To improve air quality by protecting, replacing and enhancing the urban forest canopy.
6. To seek out and implement opportunities to reduce Greenhouse Gas emissions arising from the town's operations and facilities.
7. To educate, encourage, empower, and enable the Town's residents, businesses, and institutions to reduce their energy use and environmental impact.
8. To prioritize the Town's dykes and other barriers as a means of flood protection and to study and monitor flood risk into the future.
9. To explore solutions to the residual long-term risk of coastal water levels exceeding the top of the dykes such as a flood warning system (and/or other measures) where an emergency response plan would be triggered.

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.



4.1 Energy Transition

Most of our energy today comes from fossil fuels. Fossil fuels exacerbate climate change and create incidental issues such as social inequity and increasing poverty due to rising costs of gasoline, heating fuel and electricity due to the limited resource of fossil fuels that we rely on. A sustainable and more equitable energy future will include full use of renewable, clean sources of energy, and eliminate needless waste. As a result, energy will be affordable for all people in our community, and secure energy sources will sustain a thriving economy.

IT SHALL BE THE POLICY OF COUNCIL:

CA FW P

1. To work toward a future where all residents can better afford energy for their homes, businesses and transportation needs.
2. To prioritize GHG emissions reduction and energy planning in the Town, and recognize that dedicated resources are required.
3. To collaborate with regional and federal partners in energy efficiency and climate change mitigation efforts.
4. To enable the development of low-carbon and renewable energy sources in the Land Use By-law (solar, geothermal, district energy, etc), and to be open to innovative proposals from the community.



Council recognizes that the transition towards low-carbon energy sources can aid in issues of wellhead protection, ecological sustainability, and long-term economic growth.

4.2 Development Constraints

Unique landscape features pose constraints for development, such as wetlands, watercourses and sensitive natural areas. Environmental impacts and change can lead to significant erosion of environmental services, such as disruption of water-systems, lowered air quality, and the destruction of natural habitats. Erosion and change in the natural and built environment increases with unmitigated change in climate, which creates increased frequency of extreme weather events, increased sea levels, and other changes.

Natural areas of the Town of Wolfville vary greatly in slope, watercourses and hydrology, natural habitats, and flood potential. Council recognizes the need to regulate development in environmentally-sensitive areas.

The Province of Nova Scotia plans to increase height of the dykes to prepare for climate change. The Wolfville Waterfront does not currently have dykes, and the Town is working with the province to identify solutions to block seawater encroachment in the Waterfront area.

Schedule “B” of the Land Use Bylaw outlines development constraints.

IT SHALL BE THE POLICY OF COUNCIL:

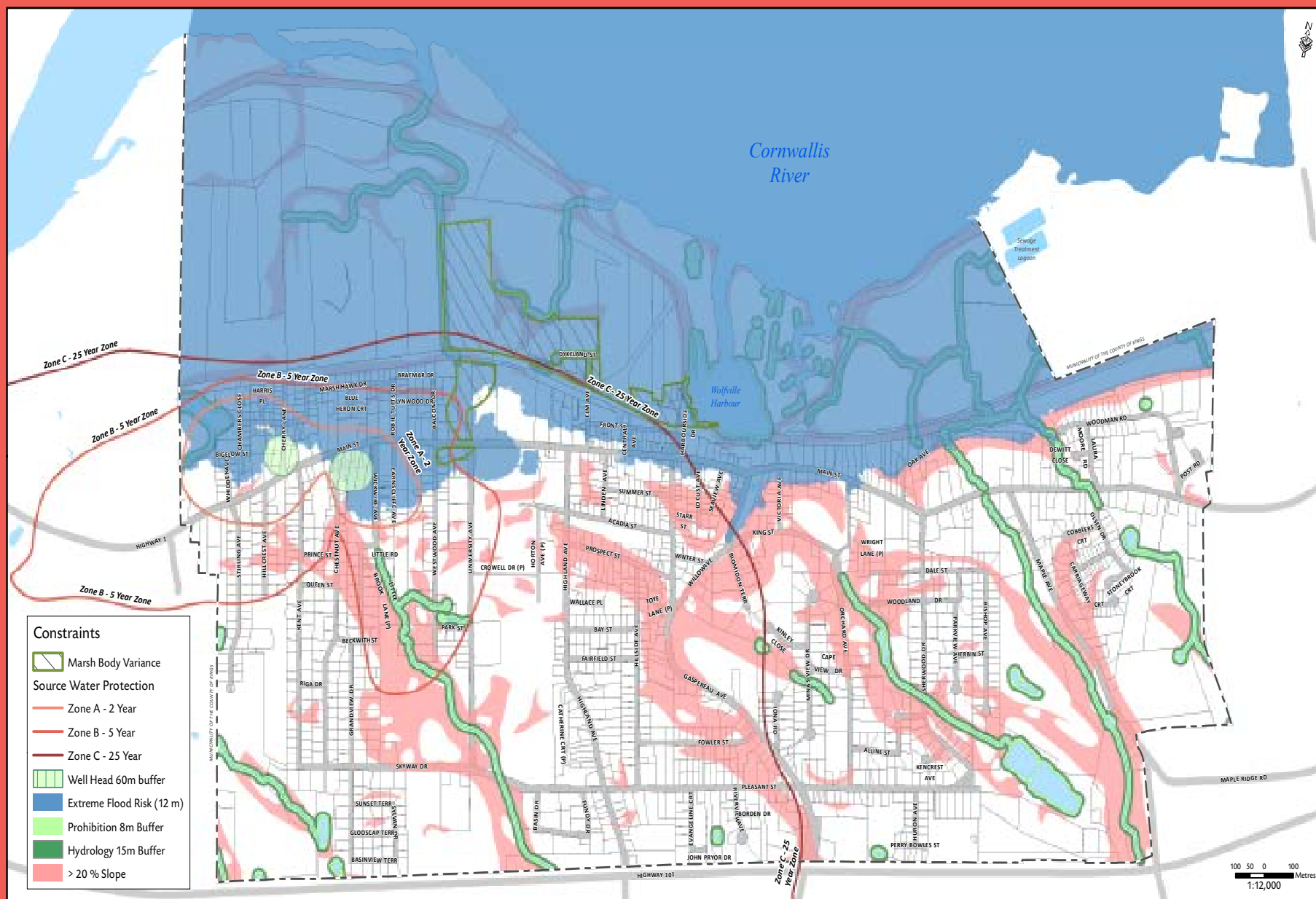
1. To spatially define and regulate development constraints in the Land Use Bylaw.
2. To require that new developments not adversely affect existing built form by giving particular regard to proper drainage, erosion control and site grading in the Land Use Bylaw.
3. To ensure that no development results in the following located where flooding or development constraints could pose a significant threat to safety of Town of Wolfville residents or environment:
 - a. a residential institution such as a hospital, senior citizen home, home for special care, or similar facility; or
 - b. a use associated with the warehousing or production of hazardous materials.
4. To identify and designate environmentally sensitive areas including, but not limited to, wetlands, steep slopes, flood risk areas, watercourses and associated ravines where development activity will be restricted or prohibited, as outlined in the Land Use By-law.
5. To require all new developments on or immediately adjacent to environmentally sensitive areas, as outlined in the Land Use Bylaw, conduct environmental studies, by

LU CA SE FW P

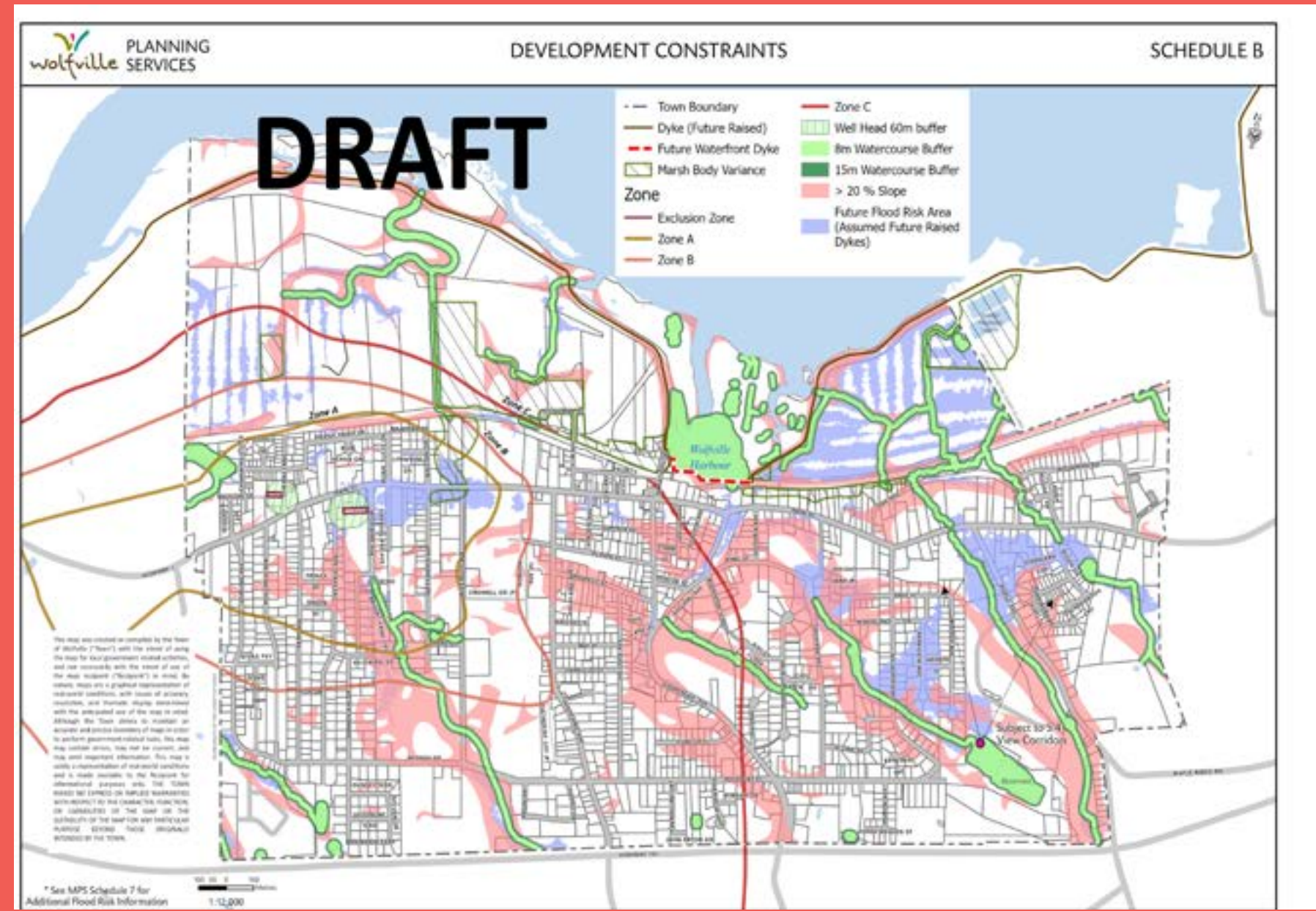
qualified professionals, that show no negative environmental impacts, and may address:

- a. natural drainage systems and watercourses;
 - b. flooding;
 - c. pollution of soils, water or air;
 - d. erosion or sedimentation;
 - e. geotechnical report;
 - f. stormwater management plan;
 - g. drainage;
 - h. erosion and sediment control plan.
 - i. other studies as listed in the Land Use Bylaw and/or Subdivision Bylaw.
6. To design the dykes and other flood control measures in a way that improves quality of life, including by enhancing their value as trails and public spaces.
 7. To protect view corridors as outlined in the Land Use Bylaw and identified on Schedule B: Development Constraints Map of the Land Use Bylaw.

The 2020 Flood Risk Study is going through an update to revise the Town's flood lines based on the forthcoming dyke topping project



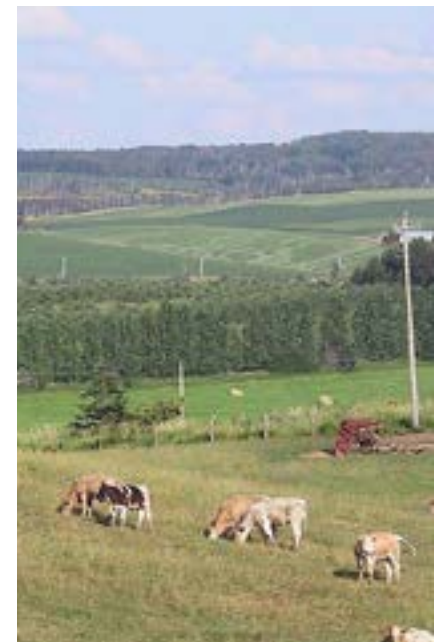
A Draft updated Constraints Map is provided here (below) and will be finalized by the end of this review.



4.3 Agriculture and Greenbelt

The Annapolis Valley contains some of the best arable land in the province. The preservation of agricultural land in and around the Town of Wolfville provides benefits such as local food production, food security, adjacent wildlife resources, ensuring pastoral view-planes, and preservation of the rural

character of the surrounding countryside. Farmland in the Town of Wolfville, as well as farmland east and west of the Town boundary constitute the active agricultural land that benefits Wolfville now and into the future.



IT SHALL BE THE POLICY OF COUNCIL:

FW P CA EP LU

1. To designate as Agriculture (A) areas located on the Dykelands, excluding areas within the Downtown Designation, on Map 1- The Future Land Use Map.
2. To establish an Agriculture (A) zone in the Land Use Bylaw, as shown on Schedule A: Land Use Zoning Map of the Land Use Bylaw where only Agricultural uses shall be permitted.
3. To discourage the re-zoning of land within the Agriculture Designation, except in areas identified on the Future Land Use Map, subject to approvals of the relevant Marshbody pursuant to the Agricultural Marshland Conservation Act.
4. To enable the exchange of farmland with Town-owned land to ensure the total amount of available farmland remains constant or expands, while making current farmland available for other strategic uses, such as public parking.
5. To encourage co-operation with the Municipality of the County of Kings in the protection and preservation of agricultural land in the areas adjacent to the Town boundary.
6. To regulate uses permitted in Agriculture (A) zone areas, as per the Land Use Bylaw.
7. To encourage and support the development of community gardens and other innovative agricultural activities, within the agriculture zone.
8. To encourage and support stewardship efforts that seek to educate the public about the history and ownership of the dykelands and the challenges faced by those practicing agricultural activities in an urban landscape.
9. To encourage and support conservation partnerships involving government, institutions, business, conservation organizations, landowners and individuals in the preservation and protection of agricultural land and/or significant wetlands.
10. To encourage the reduction of chemical fertilizers and pesticides on all agricultural lands and ensure that land owners follow a nutrient management plan in accordance with the Nova Scotia Department of Agriculture current “Best Practice” and/or Guidelines.

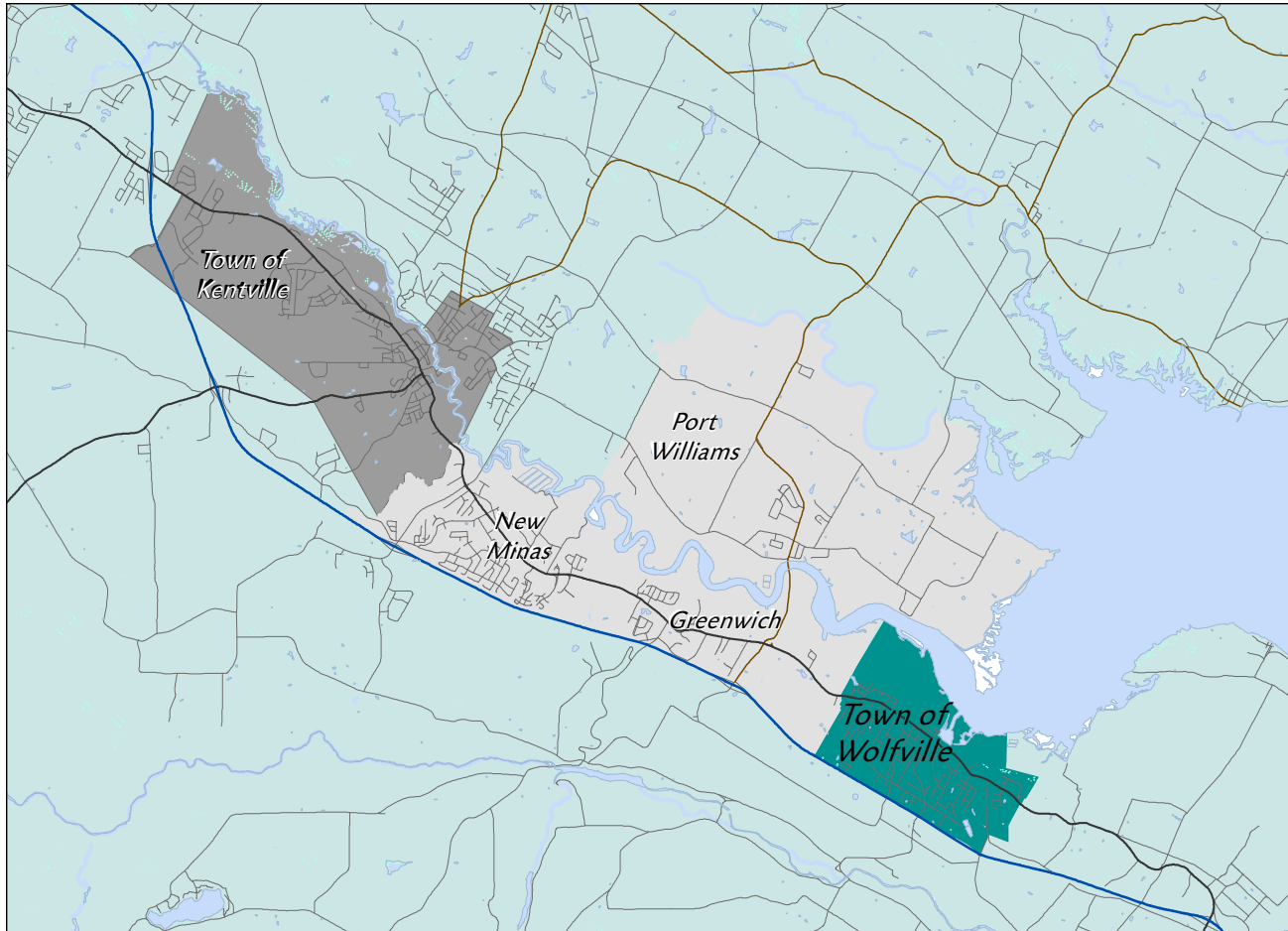


Figure 4.1—Excerpt of Kings County Generalized Future Land Use surrounding Wolfville

4.4 Food Systems

Food is a basic human need and one that shapes the Town of Wolfville in many ways. Council recognizes that access to nutritious, affordable food is critical to the health and well-being of all citizens. Markets, restaurants and cafés animate our community and bring people together. Farms in and around

town provide fresh, local food and contribute to our sense of place. Our food systems have major impacts on transportation patterns, employment, the environment, and waste management – locally, regionally, and worldwide.

IT SHALL BE THE POLICY OF COUNCIL:

1. To work towards enhanced food security:
 - a. At the individual level, ensuring that all citizens have access to healthy, affordable food;
 - b. At the community level, ensuring the resilience of local food system infrastructure; and
 - c. Without undermining the needs and boundaries of ecological and social systems.
2. To support efforts to increase food literacy and the adoption of healthy, sustainable diets.
3. To support the production and consumption of local food.
4. To work to reduce food waste and its impact.
5. To support a diverse, healthy, and vibrant food culture.
6. To develop a Wolfville Food Policy that captures and operationalizes all of these principles.

FW CA LU P SE EP



4.5 Urban Forest Management

The Town of Wolfville faces an important balancing act in its urban development. As the Town continues to grow and evolve, the need for an urban forest policy becomes increasingly urgent. Without a proactive approach to managing and preserving its urban forest, Wolfville risks losing its unique abundance of trees and high canopy cover. Wolfville's trees are threatened by certain development forms, climate change, and urbanization. If these trees are lost, a reduction in forested area and canopy cover can lead to decreased air quality, increased flood risk, and a loss of biodiversity. By establishing an urban forest policy, Wolfville can protect these natural assets, mitigate the impacts of climate change, and ensure a healthy, sustainable, and resilient forest community for generations. A forward-thinking urban forest policy is crucial to preserving Wolfville's character as a vibrant, tree-rich Town.

Urban forests can sequester carbon dioxide, reduce urban heat islands, and provide habitat for wildlife, while also enhancing community resilience and well-being. Street trees and urban woodlands can help improve air quality, reduce noise, offer opportunities for education and recreation, and increase residents' physical/mental health, property values, and quality of life (for a comprehensive list of urban-forest benefits, see the

papers by Canadian Urban Forest Research Group, 2013; and Duinker and others, 2015). Well established trees and vegetation systems can also form part of a stormwater management system by reducing overland flow and minimizing damage from significant storm events.

Globally, we are dealing with consequences of climate change and biodiversity loss, with far-reaching impacts on ecosystems, human health, and the economy. Rising temperatures and extreme weather events are altering ecosystems, leading to biodiversity deficiencies. By acting locally, Wolfville can contribute to global solutions and create a resilient forest community. By prioritizing the preservation and expansion of our urban forest, Wolfville can help protect biodiversity and enhance the natural environment that its residents interact with most often.

- The Urban Forest Policy and Work Plan (expected adoption 2025).
- LUB landscaping requirements (tree protection, tree planting/replacement requirements) and native species priority.



PART 5

MOBILITY

Disclaimer

The Town is working through a process to implement an Active Transportation network in the Town. The Policy to support this work is included here for discussion. A workshop is being held with Council on this work in early November 2025 and will inform changes to this chapter.

The Parking section has been updated.

5.0 Providing for Mobility Options

Transportation systems include the needs of drivers, cyclists, pedestrians and transit users. These needs include transportation infrastructure (walking paths, car lanes, and bicycle lanes) and how to transition transportation choices towards less fossil fuel reliance, regardless of ability, age or income.

Pedestrian and bike friendly streets, accessible and efficient transit as well as innovative car share programs are envisioned for the future of Wolfville. It should be possible for anyone to live comfortably in Wolfville and beyond without owning a private automobile. This Municipal Planning Strategy includes sustainable transportation policies that support and encourage that vision.

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

Council recognizes that demand for specific transportation options is significantly reduced where there is little or no infrastructure to support it, and that providing infrastructure for specific transportation options induces demand for said option.

IT SHALL BE THE POLICY OF COUNCIL:

1. To build cost-effective infrastructure that increases participation in active transportation and discourages reliance on fossil fuel vehicles in the Town of Wolfville.
2. To support sustainable transportation, reduce our reliance on fossil fuels, and promote health by striving to prioritize infrastructure development, in the following order of infrastructure;
 - 2.1. active transportation (walking, biking)
 - 2.2. public transportation options
 - 2.3. other shared mobility options
 - 2.4. private electric vehicles
 - 2.5. private fossil-fuel vehicles
3. To establish and maintain an active transportation network of bike lanes, sidewalks, trails and paths throughout the Town of Wolfville, as outlined on Schedule 3: Future Parks and Trails Map, dedicated to connecting existing services and parks with areas of residential dwellings.
4. To adopt Schedule 2: Future Streets Map and Schedule 3: Future Parks and Trails Map as part of the Municipal Planning Strategy and use this map as a guide in the approval of transportation networks in future developments.

FW P CA LU SE EP

5. To require developers to construct active transportation paths, and streets, within new subdivisions aligned with Schedules 2 and 3.
6. To require bicycle parking at new multi-unit buildings and at Harvest Moon Trail access points.
7. To collaborate with residents and stakeholders towards a cost-efficient and useful public transit service dedicated to connecting existing compact residential areas, services and parks with each other and other regions.



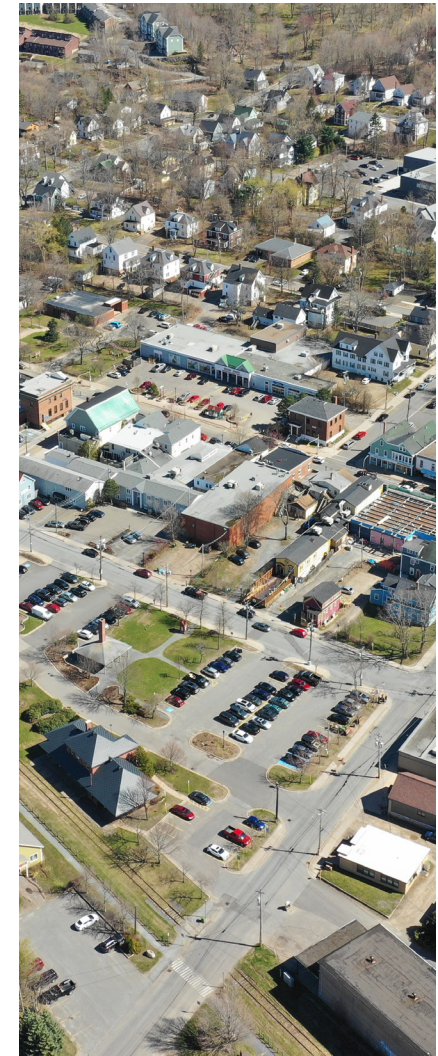
5.1 Streets

(To Be Considered at November 2025 Workshop and ongoing work on Mobility options in the Town)

It is recognized that in any transportation network each road provides a certain level of service to local and regional traffic and pedestrians. Road classifications allow the Town to dictate the inclusion of pedestrian and cycling infrastructure as well as on-street parking and landscaping provisions within the municipal right-of way. This also helps plan maintenance and ensure efficient transportation service is provided.

The Town has adopted a three-tiered roadway system of shared street, collector street and local streets for public right of ways and identifies specific policies for private laneways.

Schedule 2: Future Streets Map of the Municipal Planning Strategy indicates the location of future street development and the hierarchy of the existing road network to ensure an integrated long-term street development pattern. Council also seeks to reduce the dominance of the automobile within Town environs and promote public transit and active modes of transportation as viable alternatives.



IT SHALL BE A POLICY OF COUNCIL:

1. To adopt Schedule 2: Future Streets Map as part of the Municipal Planning Strategy, and use this map as a tool in supporting Schedule 3: Future Parks and Trails Map, in the approval of transportation networks in future developments.
2. To recognize that streets, including sidewalks, must accommodate a variety of transportation options and strive to prioritize in the following order:
 - a. active transportation (walking, biking)
 - b. public transportation options
 - c. other mobility options
 - d. private electric vehicles
 - e. private fossil-fuel base vehicles
3. To require all new streets and street-extensions to be constructed to standards set out in the Subdivision Bylaw and/or servicing agreement, which shall require:
 - a. consideration of one or more sidewalks or active transportation paths on all current and future roads in the Town of Wolfville, such as multi-use trails, bike lanes, or shared streets.
 - b. proper road connections and alignments developed and maintained as outlined by the Province of Nova Scotia, including the Department of Transportation.
4. To enable traffic calming measures as outlined in a Traffic Calming Policy to support the quiet and safe enjoyment of neighbourhoods
5. To establish a Street Network Structure, as shown on Schedule 2: Street Classification Map, to guide investments in future streets, traffic calming, active transportation, and street reconstruction by establishing a set of street cross sections to correspond with each Street Classification. Each classification will have its own distinct design principles and elements based on a complete streets model, and form part of the overarching transportation network. The Street Network Structure throughout the Town of Wolfville is as follows:
 - a. Connector Streets
 - b. Destination Streets
 - c. Local Streets
 - d. Living Streets

6. To designate as **Future Streets** all areas designated for future road development, including access easements, as shown on Schedule 2: Future Streets Map. These streets are anticipated to become local roads, unless otherwise specified.
7. To designate as **Living Streets** those that carry a moderate volume of local traffic, such as parks, schools, and shopping areas, where a high degree of multimodal conflicts is anticipated. The focus of these streets is contributing to enjoyment of and access to key Town destinations. These streets should feature high levels of traffic calming to achieve speeds of 10-20 km/h, which can help discourage through traffic. Living Streets are excellent candidates for investments in placemaking, playful elements, and public art, including street murals.
8. To consider Living Streets for improvements to accommodate:
 - a. Wider sidewalks
 - b. Brick road surface or coloured pavement (including street murals)
 - c. Street furniture, public art, and streetscaping
 - d. Parklets or café extensions
 - e. On-street parking
9. To use the Core Area Design Guidelines 'Streetscape' sections to inform improvements to streets in the Core Area.
10. To designate as Connector Streets the main routes connecting in and around Wolfville. Connector Streets also link the Town with rural areas beyond the Town limits, providing access to the Town from other areas of the county. While these streets operate as higher-speed rural roads beyond the built-up area, within the Town limits they should be designed for speeds of 40-50 km/h. Their right-of-way is generally wider than the standard local right-of-way, to accommodate dedicated facilities for people cycling and walking.

11. To consider Connector Streets for improvements to accommodate:
 - a. Transit and goods movement;
 - b. Higher traffic volumes;
 - c. Higher intensity land uses;
 - d. Pedestrian facilities on both sides of the street;
 - e. Dedicated facilities for cycling such as bike lanes or multi-use paved paths;
 - f. Regular designated crossings for pedestrians that are aligned with desire lines;
 - g. Intersections which are further apart;
 - h. Limited private accesses; and
 - i. Limited on-street parking.
12. To designate as Local Streets those that pass through residential areas and are intended to carry only vehicle traffic originating or terminating on the street. To enhance liveability for residents and safety for people walking and cycling, local streets should be designed for speeds of 30 km/h using traffic calming measures, which can also help to deter cut-through traffic. Some local streets can be designed as main routes for cycling, which includes wayfinding features.
13. To consider **Local Streets** for improvements to accommodate:
 - a. shared lanes of automobile and bicycle uses;
 - b. low vehicular traffic;
 - c. two way vehicle travel lane width of less than 7m;
 - d. limited or no transit service; and
 - e. on-street parking.



14. To designate as Transition Zones (as indicated on Schedule 2 Future Streets Map) locations where significant changes in street context and desired travel speed occur. At these locations, especially where target speed decreases, design interventions should be used to signify and reinforce the change in context such as chicanes, visual lane narrowing, roadside plantings, high-visibility crosswalks, roundabouts, speed tables, and pavement markings. Gateway features may be used at these locations, including landscaping, scenic elements, and placemaking. Types of transition zones include:

- a. Rural-to-urban transitions, where high-speed (60-70km/h) rural roads transition to moderate-speed (40-50 km/h) Connector Streets entering the Town limits.
- b. Downtown transitions, where moderate speed (40-50 km/h) Connector Streets transition to low-speed (10-30 km/h) Activity Streets entering areas with a high level of multimodal and curbside activity.

15. To designate as Destination Streets those that traverse areas with an intense sense of place and high levels of diverse user activity, where accommodation of through traffic remains a consideration. These streets should be designed to contribute to an exceptional pedestrian environment, with frequent crossing opportunities and traffic moving a maximum of 30 km/h. Destination streets are excellent candidates for investments in placemaking and streetscaping.

16. To consider for Destination Streets for improvements to accommodate:

- a. Wider sidewalks
- b. On-street parking
- c. Transit and goods movement
- d. Higher traffic volumes
- e. Higher intensity land uses
- f. Removal of private accesses

- g. Curb extensions and raised crosswalks at intersections
- h. Parklets or café extensions
- i. Shared lanes of automobile and bicycle uses
- j. Frequent designated pedestrian crossings
- k. Street furniture, public art, and streetscaping

5.2 Parking and Loading

The Town of Wolfville must rely on effective and accessible infrastructure to accommodate the needs of people shopping, living and working downtown.

As the Town of Wolfville transitions to a less fossil fuel reliant future, Council wishes to support a more walkable community through the creation of infrastructure dedicated to walking and biking while managing parking appropriately.

Removing minimum parking requirements in as of right development simplifies the path to increasing housing supply and allows the market to determine the appropriate parking ratios for new residential and commercial development.

Enhancing municipal-infrastructure for managing walking, biking and parking can be supported through new multi-modal transportation options and through negotiation of new development agreements where onsite structured parking forms part of the development.



MORE PARKING , LESS WALKABLE



LESS PARKING , MORE WALKABLE



Figure 5.4—Relationship between walkability and parking

IT SHALL BE THE POLICY OF COUNCIL:

CA LU FW P

1. To establish minimum off-street bicycle parking requirements for commercial, mixed-use, institutional and multi-unit residential development permitted as-of-right, in accordance with the Land Use Bylaw.
2. To ensure that all developments permitted by site plan approval and development agreement include well designed automobile and bicycle parking to serve the development.
3. To establish design standards and other parking regulation for all development in accordance with the Land Use Bylaw.
4. To establish minimum off-street loading requirements for certain uses in accordance with the Land Use Bylaw.
5. To encourage alternatives to impermeable surfaces on parking areas.
6. To permit temporary parking uses associated with a construction site, special occasion or a holiday, as established in the Land Use Bylaw.
7. To acknowledge that the Town will continue to play a key role in owning, managing, and maintaining public parking in the Core Area.
8. To consider implementing a paid “variable priced” parking management system and to ensure that money collected is reinvested in the parking system and other Downtown placemaking initiatives.
9. To improve parking security and management to reduce the chance for overflow parking to surrounding streets and between Acadia and Downtown.

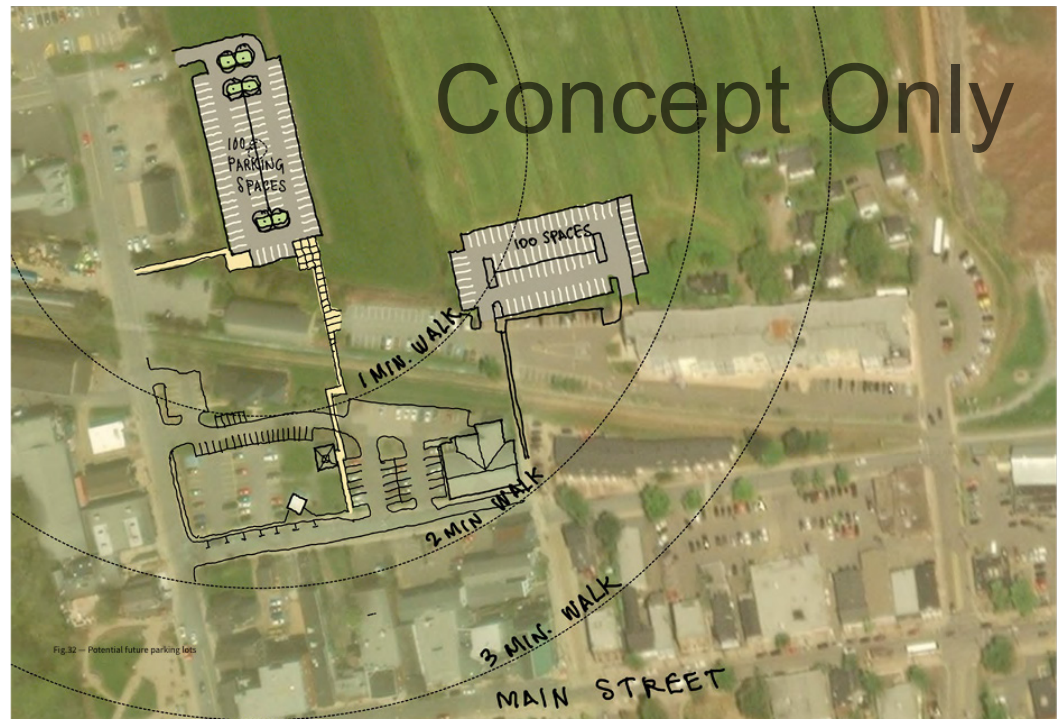


Figure 5.1—Core Concept for New Town Owned and Operated Parking (Implementation Framework), this also supports policy direction in the Investment for Success section of this MPS (Part 3)

5.3 Downtown Parking

Parking will remain central for Wolfville's economic success for the foreseeable future. However, large parking lots also impose costs. They undermine the pedestrian experience and generate less street life, tax revenue, and social value than homes, businesses, or parks would on the same land. Currently, 10% of Downtown's land is devoted to parking.

There are opportunities to better use existing parking. Some on-street parking spots are occupied by a single vehicle all-day. Some parking areas are underutilized. Visitors often struggle to find available parking, which increases traffic, as visitors circle blocks looking for empty spots.

The Town aims to manage parking in a way that provides easy, desirable options for parking; that maximizes the value of each parking spot for drivers; and that makes greatest use Downtown land for the economy and quality of life. The following strategies will help achieve these goals.

The Town will explore opportunities for building more public parking in the Downtown area in locations that will minimize their visual impact on existing streets. Wayfinding tools will be used to better direct visitors to public parking.

Paid parking will be introduced in the Downtown to encourage greater turnover. "Performance parking" strategies will be explored, meaning the price of parking may adjust over-time on each block with the goal of ensuring one parking space is available on all blocks at all times, to minimize traffic created by people looking for spots. This combination of strategies will provide ample free parking for those who want it, while ensuring spots on Main Street are consistently available for those who prefer to pay.

IT SHALL BE THE POLICY OF COUNCIL:

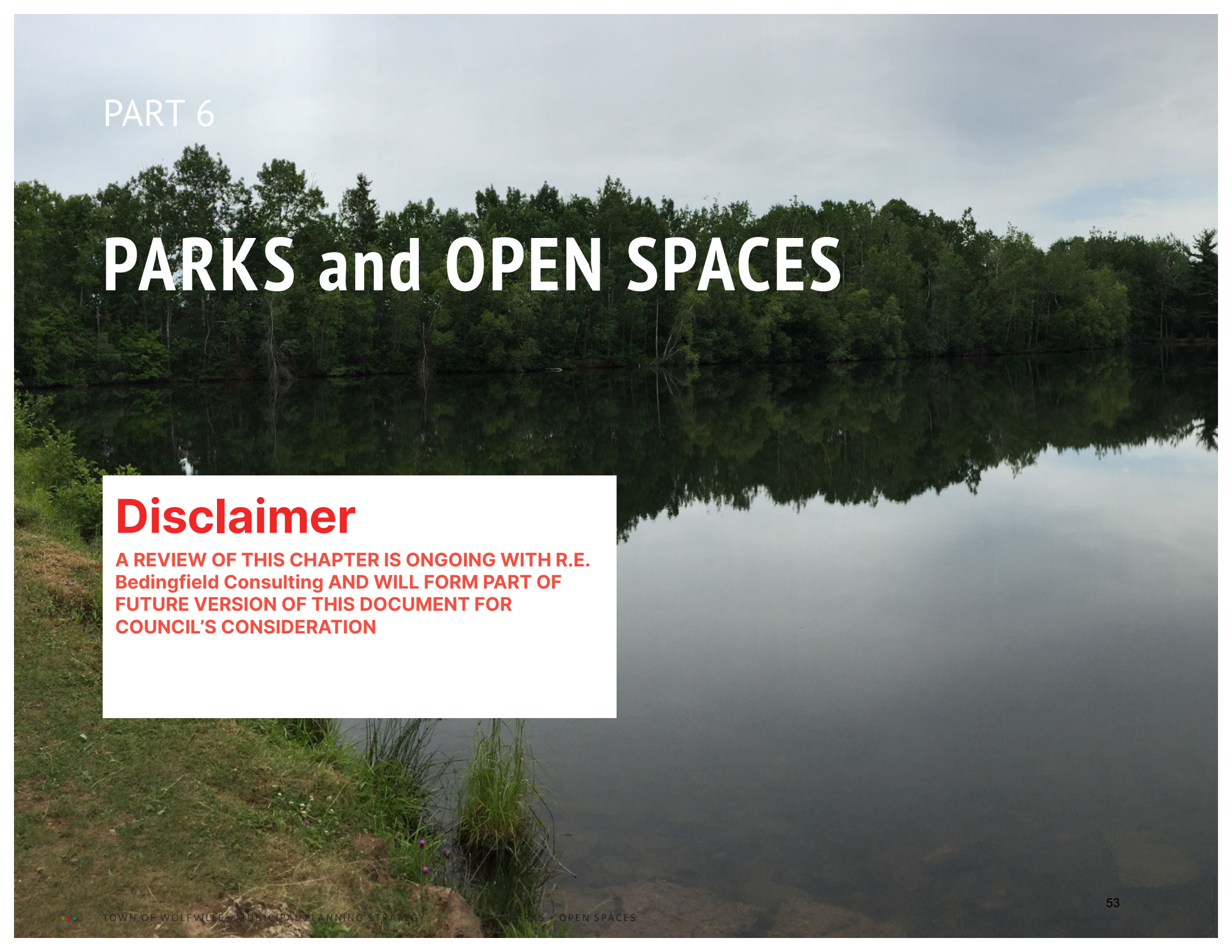
1. To pursue opportunities for the creation of new public parking lots downtown in locations that will minimize their visual impact on Downtown streets.
2. To introduce paid parking in high-demand areas of downtown to encourage greater turnover.
3. To improve parking security and management to reduce the chance for overflow parking to surrounding streets and between Acadia and Downtown.
4. To invest in wayfinding to ensure visitors can easily find parking options that best meet their needs.
5. To establish parking enforcement.
6. To explore options to manage parking impacts on residential streets, such as permit parking



PARKING. IT'S ALL ABOUT SPACE

There are often trade-offs between the amount of **available parking**, the **cost of homes and businesses**, and the **ability to walk** to destinations in your neighbourhood.





PART 6

PARKS and OPEN SPACES

Disclaimer

A REVIEW OF THIS CHAPTER IS ONGOING WITH R.E. Bedingfield Consulting AND WILL FORM PART OF FUTURE VERSION OF THIS DOCUMENT FOR COUNCIL'S CONSIDERATION

6.0 Recreation and Quality of Life

Public open space is important to the physical, social, cultural and ecological fabric of the community. The Town of Wolfville has a total of 28 existing parks and open spaces in 30 hectares (75 acres) of Town-owned, or leased, land developed or dedicated to park or open space purposes (5% of total land in Wolfville). Acadia University and the Rotary Club of Wolfville also provide park and open space lands in Town.

The availability of parks and open space areas that provide opportunities for leisure activity is regarded by Council as necessary for the health and well-being of residents of the Town, and Council will continue to play a key role in providing space and facilities where the need is identified, and as financial circumstances dictate.

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

IT SHALL BE THE POLICY OF COUNCIL: FW P LU

1. Identify current and future parks and open space areas on Schedule 3: Future Parks and Trails Map.
2. Permit the development and use of land in all zones for park, playground and open space uses.
3. Recognize the established park types/classifications in line with the Town's Parks and Open Space Master Plan (2012) as outlined in Schedule 6: Park and Open Space Classification, which include Neighbourhood Park; Community Park; Active Sports Park; Linear Park; and Priority Park, and use this as a guide to determine future parkland needs.
4. Encourage maximum use and enjoyment of these resources by community residents by ensuring park spaces are planned and provide opportunities for multiple activities, i.e. picnicking, play structures, passive play areas, and active play areas.
5. Develop all parks in accordance with a concept plan, developed by a licensed Landscape Architect or other qualified professional, which clearly indicates appropriate uses, functional areas of the park, natural and environmentally sensitive areas, and relationships among park activity areas.
6. Employ accepted design principles, as warranted, to guide the layout of the park and the provision of park equipment, such as CSA standards for playgrounds, Rick

Hansen Accessibility Standards, and Crime Prevention Through Environmental Design (CPTED) guidelines. Park proposals will be evaluated with respect to their contribution to other approved municipal planning documents (e.g. Physical Activity Strategy).

7. To recognize that the current supply of parks and open space areas is adequate. Additional open space therefore will only be assembled if the property in question has unique economic, environmental, heritage, connectivity or cultural significance. The Town will be guided by new parkland as generally identified on Schedule 3: Future Parks and Trails Map as future park.
8. Within Lands zoned Comprehensive Development District (CDD), additional open space shown on Schedule 3: Future Parks and Trails Map is conceptual in nature and would be subject to a secondary planning study.
9. If areas are identified as having unique environmental, heritage, or cultural significance, Council shall explore options to protect the land through mechanisms other than land acquisition, which may include, but is not limited to, a long term lease, conservation easement or other legal means, including expropriation.
10. To facilitate and maintain two well-equipped neighbourhood parks, one in the east and one in the west, at a location that will allow most residents to reach

a park with informal, active recreational opportunities within 500-600 m.

1. To require the regular review of the Parks and Open Space Master Plan and/or the Parks and Open Space Policies of the MPS.

12. To work in cooperation with Acadia University and other government bodies in the provision of recreation and cultural opportunities for town residents.

1. That Public Engagement shall take place during Parks and Open Space planning and redevelopment efforts, as per the Town's Public Engagement Policy.



Core Concept

Goals of Recreation Nova Scotia:

Active Living—To foster active, healthy living through recreation.

Inclusion and Access—To Increase inclusion and access to recreation for populations that face constraints to participation.

Connecting People and Nature—To help people connect to nature through recreation.

Supportive Environments—To ensure the provision of supportive physical and social environments that encourage participation in recreation and build strong, caring communities.

Recreation Capacity—To ensure the continued growth and sustainability of the recreation field.

6.1 Parkland Dedication

Ensuring the provision of future public open space is enabled by the Municipal Government Act and embodied in Municipal Planning Strategy policy and the Town's Subdivision Bylaw. The Subdivision Bylaw requires that every developer of new lots

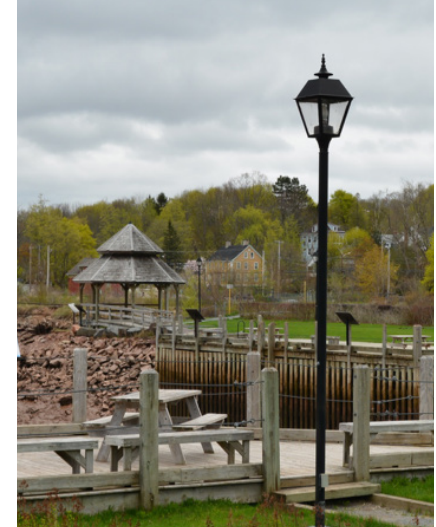
contribute either useable land or a cash-in-lieu equivalent to 10% of the area/value of land shown on the final plan of subdivision exclusive of streets, roads, walkways and the residue of land owned by the sub-divider.

IT SHALL BE THE POLICY OF COUNCIL:

1. To require the provision of parkland with all new lot creation in accordance with the provisions under Section 271 (13h) of the Municipal Government Act, and as described in the Subdivision Bylaw.
2. To require developers to dedicate useable land (in the amount of 10% of the area of the lots to be approved on a final plan of subdivision) or cash in lieu of such land for recreation purposes when subdividing land:
 - a. Any and no general identification on Schedule 3: Future Parks and Trails Map may not be accepted unless the property in question has unique economic, environmental, heritage, connectivity or cultural significance.
3. That notwithstanding Policy 6.1(2), new lots created in the Downtown zone shall not be subject to open space requirements or cash-in-lieu provisions.
4. To require developers to provide space for leisure or recreation facilities in association with new multi-unit residential developments with more than 4 dwelling units as per Land Use Bylaw.
5. To require that any land transfer for future public use as a trail, park or playground must:
 - a. Be useable land, or equivalent value, and be identified on Schedule 3: Future Parks and Trails Map; or
 - b. In the view of Council, have demonstrated cultural, environmental, connectivity, or historical value; or
 - c. If the land being subdivided has frontage on a public amenity must maintain public access.

6. To adopt the following as a guide for parkland dedication processes in Secondary Planning and CDD areas:
 - a. To the greatest extent possible, parks will be fully developed at time of transfer to the Town and provide opportunities for multiple activities, at the sole cost of the developer.
 - b. Parks and open park areas will be developed and maintained in a manner that is safe for all users and protects the interests of the Town.
 - c. New parks must be within the financial capacity of the Town to operate and maintain.
 - d. All parks will be developed in accordance with a concept plan by a qualified professional which indicates the functional areas of the park, natural and environmentally sensitive areas, and relationships among park activities.
 - e. All parks will be guided by the Town's Accessibility Plan, CPTED principles, other municipal documents and strategies and CSA standards for equipment.
 - f. To the extent possible, trails, sidewalks and paths will link parkland and open spaces to promote healthy living, encourage tourism and protect significant natural features.
 - g. Some form of public engagement will be carried out as part of Secondary Planning and CDD processes.

FW P LU



6.2 Park and Open Space Classifications

Parks include both neighbourhood parks, community parks and other typologies described in Schedule 6: Park Classification Systems. They are public spaces which serve their immediate neighbourhood, provide for general community needs and can

be venues for a wide range of activities and interests, such as sports. Other areas for socializing, walking, exercising, or gardening are also appropriate. These parks accommodate a range of interests at the neighbourhood or community level.

IT SHALL BE THE POLICY OF COUNCIL:

1. To zone all Parks and Open Space (e.g. Neighbourhood Parks, Community Parks, etc) as “Parks and Open Space (P-1)” and permit uses in the Land Use Bylaw, including but not limited to:
 - a. Parks and playgrounds, including associated facilities;
 - b. Interpretation centres;
 - c. Pavilions and sunshades;
 - d. Museum;
 - e. Community gardens;
 - f. Occasional, incidental, temporary outdoor or indoor markets, bake sales, flea markets and vegetable and produce markets, with appropriate permissions, licensing, and permits;
 - g. Special open areas;
 - h. Linear parks; and
 - i. Non-recreational open spaces
2. To locate Neighbourhood Parks within a 500-600 metre walking distance of residential areas with safe pedestrian access.
3. Uses permitted in the P-1 zones will vary and the zone standards will be flexible to reflect that these park types can have irregular shapes and may have limited opportunity for road frontages.

4. To consider the completion of a masterplan prior to undertaking any long-term new or re-development of Parks and Open Space, which demonstrates:
 - a. Multiple opportunities for the public to access the site, particularly for linear parks.
 - b. Passive shaded areas and amenities for passive activities (benches, picnic tables,
 - c. Where there are environmental constraints, the plan should consider how to mitigate the impact on people on the landscape and ecosystem.
 - d. Access from major roadways and connection to trail systems and other parts of the open space system where possible.
 - e. Integration and use of CPTED principles, Rick Hansen Accessibility Standards and other relevant guidelines in the design of the spaces.



6.3 University Open Space

Significant open green-space areas within the Town of Wolfville are owned and managed by Acadia University. These open space or park amenity areas are accessible to the public and contribute to the quality of life to residents of Wolfville and the region – at times attracting thousands of visitors to the Town of Wolfville for local, regional and national events.

IT SHALL BE THE POLICY OF COUNCIL:

P FW LU

1. To create a zone “Parks and Open Space - University” (P-2) within the Land Use Bylaw.
2. To work with Acadia, through a partnership agreement (memorandum of understanding), to ensure public benefit of these lands.
3. To permit uses as outlined in the Land Use Bylaw, which may include uses associated with University functions or park functions.

UPDATE PENDING



PART 7

MUNICIPAL ASSETS and SERVICES

Disclaimer

This section is in draft.

7.0 Sustainable Service Delivery

Council recognizes that our Municipal Assets and Public Services are the foundation that enables the Town to thrive; however, like all Canadian municipalities, Council realizes we will face significant social, economic and environmental challenges that will affect the Town's ability to manage assets and deliver services sustainability over the long term. The way the Town invests in infrastructure assets, including natural

assets, will have a significant impact on whether we move toward becoming a more sustainable and resilient community. Because of this, Council is committed to better integrating Land Use Planning and Asset Management, starting with the policies of this plan and focusing on improvements over time.

Community Priorities

- EP** Economic Prosperity
- SE** Social Equity
- CA** Climate Action
- LU** Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

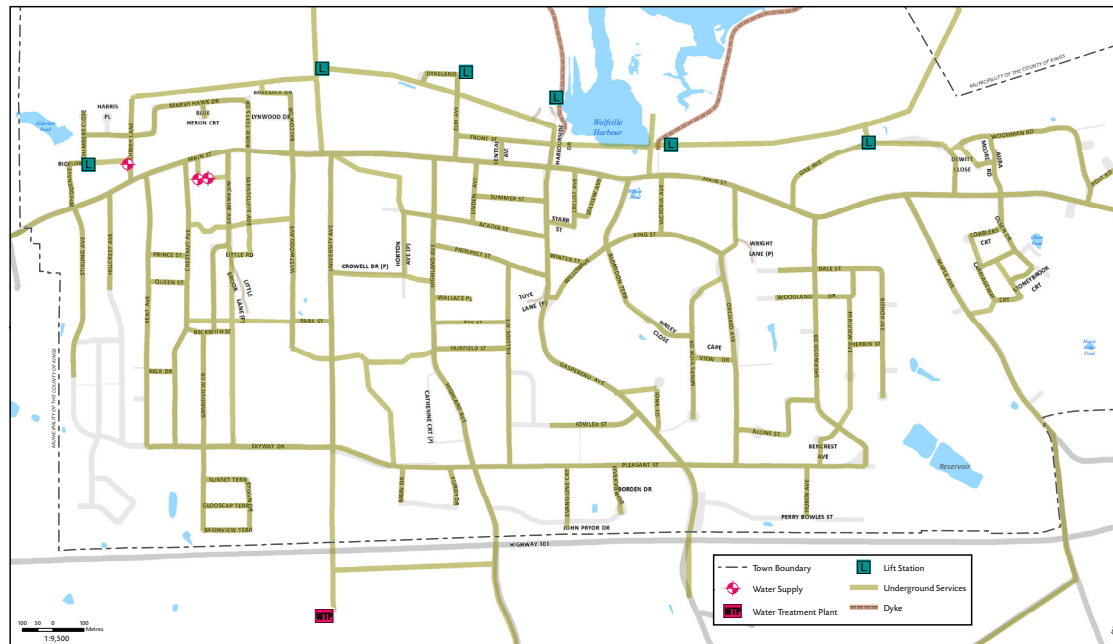


Figure 7.1—Town Asset and Public Service overview map

7.1 Water Supply

A safe and reliable water supply is essential to public health and community stability. Water is the foundation of a viable social and economic community structure. In fact, the abundance and quality of a community's water supply is a definitive indicator of sustainable development. Council wishes to help protect this vital resource and the public investment in the central water supply and distribution services owned and operated by The Town of Wolfville. The Town has ensured the continued availability of the historic watershed lands, in perpetuity, which are protected by a conservation easement between the Nova Scotia Nature Trust and the Town. Wolfville's current water supply consists of two wells constructed in the surficial sand and gravel glacial deposits in the area of West Main Street. The groundwater from these two wells is abundant in supply and of good quality. Protection of the well head areas and controls on the types of land use that occur in close proximity to these well heads is provided in this Municipal Planning Strategy and the Land Use

Bylaw. The Town's Source Water Protection Plan has been integrated into this document and is the first step in better managing and protecting this vital resource.

The water utility is a separate corporation from the Town and is funded through water rates. There are approximately 41 kilometres of water distribution lines throughout Town that supply potable water daily to residents and customers. Water that originates in the aquifer is pumped to a 12 million litre concrete storage reservoir located just beyond Town boundaries on the Ridge Road. It is then fed by gravity to the water distribution lines throughout Town. In order to ensure that the water supply is safe and potable it must meet rigorous standards and guidelines. The water treatment facility uses chlorination to ensure that the water is safe from bacterial contamination. Regular testing and monitoring occur in order to ensure that Wolfville's drinking water meets or exceeds the required government standards with respect to water quality.



IT SHALL BE THE POLICY OF COUNCIL:

1. To identify areas in proximity of the wellheads as development constraint areas and restrict land use activities in these areas in the Land Use Bylaw;
2. To require a developer to design and install water distribution lines in accordance with appropriate municipal standards as provided for in the Subdivision Bylaw and/or servicing agreement;
3. To recognize the role of the Source Water Protection Advisory Committee in advising on the management and protection of the Town's water source.
4. To monitor risks and future needs of the Town's water supply.

LU CA FW

7.2 Sewer Collection and Treatment



One of the foremost environmental considerations of any community is how waste is managed. All areas of the Town are serviced by a sanitary sewage collection system. All sanitary sewage is collected by a piped system which collects the sewage and directs it, via a pumping station, to a treatment plant on the eastern boundary of the Town on the Dykelands. This plant is an aerated lagoon system, which is a common system for the Annapolis Valley, and has been in operation

since 1978. The treated effluent is discharged to the Minas Basin. In response to capacity issues and future demands on the Town's infrastructure, Phase 1 of upgrades to the Waste Water Treatment Plant added new influent screens and replaced the disinfection system between 2019-2021. Phase 2 began in 2025, which will implement aeration upgrades and the construction of a third aerated lagoon cell to improve performance.

IT SHALL BE THE POLICY OF COUNCIL:

1. To provide efficient and responsive sanitary collection and treatment programs for the Town.
2. To require a developer to install sanitary sewers within a development, at the Developer's cost, and that the sewers are connected to the Town's sanitary sewer system.
3. To require all sanitary sewer systems to be designed and constructed to meet the standards set out in the Subdivision By-law, sewer By-law or service agreement.



7.3 Stormwater Management

Wolfville is located on the north slope of the Wolfville Ridge. All rainfall within the Town has historically been directed north down this slope to the Cornwallis River via a series of small brooks or natural watercourses. As the Town has developed, this basic pattern has not changed, however a system of piped and daylighted storm sewers now directs drainage from the developed areas to these watercourses.

Upgrading of storm sewer system has been ongoing, in conjunction with other infrastructure, on a strategic basis. Although the piped storm sewer collection system is for the most part separate from the sanitary sewer collection system, there are some instances where storm water is discharged to the sanitary sewer. This results in an unnecessary load on the sewage treatment plant and wasted energy. These connections

should be removed as part of a regular maintenance and upgrading program.

Council also recognizes the need to implement new stormwater management policies which provide clear direction on the development of individual properties in order to help minimize the impact that development can have on the Town's stormwater drainage system. Council therefore is interested in exploring small-scale stormwater management measures through the Town's Land Use Bylaw. Low Impact Development approaches - particularly those that are aimed at maximizing stormwater retention and infiltration - will form an integral part of any future watershed management plan between the Town and the County.



IT SHALL BE THE POLICY OF COUNCIL:

1. To provide efficient and responsive stormwater management programs for the town.
2. To look to increase biodiversity and natural capital when considering stormwater management solutions (e.g. vegetative cover, wetlands, ponds with biological or social value).
3. To require a developer to install stormwater management at the developer's cost, within a development and negotiate the costs of providing sanitary sewers, and storm sewers, outside the development sufficient to connect to the town's sanitary sewer system.
4. To require all of the sanitary sewer and storm sewer system to be designed and constructed to meet the

standards set out in the Subdivision Bylaw, servicing agreement and that they meet or exceed all legislative requirements with respect to protection of the natural environment and habitat protection.

5. To continue a program of removing storm sewer discharges to the sanitary sewer system.
6. To utilize natural water courses, where appropriate, for conveyance of storm water run-off from serviced areas, subject to statutory provisions for utilization and alteration of watercourses. In so doing, Council will ensure that appropriate planning, design, and engineering techniques are employed to minimize any impact that alteration of the natural water flow may have.

CA LU

7. To develop a stormwater management plan and standards which have regard for the anticipated impacts of climate change. They may include quantitative, performance standards for:
 - a. sediment concentrations (total suspended solids);
 - b. chemical concentrations, such as phosphorous or nitrogen;
 - c. peak flow rates and total stormwater volumes.
 - d. storm water management education program.
8. To develop and maintain Stormwater Management Design Guidelines for the Town.
9. To encourage development that either maintains, enhances or, if not technically feasible, minimizes impact to the pre-development hydrologic regime through innovative site design and engineering techniques aimed at infiltrating, filtering, evaporating, harvesting and retaining runoff, as well as mitigating pollution.
10. To adopt performance standards for each land use zone that specifies a maximum percentage for impervious surface in the Land Use Bylaw. Development that exceeds the maximum percentage of impervious surface will be

permitted, up to a specified limit, where development implements on-site stormwater retention and maximizes infiltration by incorporating design elements as described in the Town's Stormwater Management Design Guidelines, such as:

- a. permeable pavement;
 - b. infiltration trenches/chambers and bio-retention;
 - c. rainwater harvesting systems in building design;
 - d. green roofs; and
 - e. roof drains directed to permeable areas with stormwater infiltration measures in place. To encourage the use of existing natural drainage systems and the integration of stormwater management infrastructure into the context of the surrounding land uses.
11. Where requested by the Town Engineer, require that a grading and stormwater drainage plan be submitted as part of any new multiple lot subdivision or development.
 12. To enable stormwater treatment facilities to be located on municipal lands.

7.4 Service and Utility Use

Service and utility uses differ from other uses in that they provide the infrastructure for all other development. The majority of these uses use rights-of-way which must cross zone boundaries. Sometimes these same uses often have offices, facilities and supply or storage structures.

IT SHALL BE THE POLICY OF COUNCIL:

CA LU

1. To permit service and utility linear facilities throughout the Town.
2. To permit service and utility facilities, other than rights-of-way but including storage, sewage treatment facilities, water treatment facilities, water reservoirs, etc. only in accordance with the Land Use Bylaw.
3. Where financially sustainable, shall maintain, improve, and expand municipal service facilities and programs.



7.5 Servicing Beyond Our Borders

There is merit to provide municipal services (e.g. water, sewer) outside Wolfville's borders when linked to health and safety issues or to stimulate economic development opportunities. Services must be extended in a considered manner to avoid development that is not in the best interest of Wolfville and the broader region. Service extensions must also be done in cooperation with our neighbouring municipal unit.

IT SHALL BE THE INTENTION OF COUNCIL:



1. To complete a review of the Town boundary and any extensions.
2. To review a boundary extension plan



7.6 Solid Waste

The Town is a partner with other Municipalities in the Annapolis Valley in the Valley Solid Waste Management Authority. This Authority has established a world class solid waste collection and disposal system which has seen a reduction in the amount of waste going to landfills. All recyclable and compostable materials are collected and used as resources and hazardous waste can be dropped off at local management centres for appropriate disposal. The Town is proud to be a partner in this initiative and will continue to support the sound environmental waste management practices of this Authority.

IT SHALL BE THE POLICY OF COUNCIL:

CA P FW

1. To continue to support the economically efficient and environmentally responsible management of solid waste.



7.7 Police, Fire Protection, and Regional Emergency Management

Twenty-four hour police protection is currently provided to the Town by the Royal Canadian Mounted Police (RCMP). Police protection is recognized by Council as an essential service and Council is committed to providing the best possible and most cost-effective police protection for the Town.

In 1890 Wolfville took steps to organize a fire service known as the Wolfville Fire and Protection Commission. In the 1940's this service was reorganized and re-named the Wolfville Volunteer Fire Department. The Wolfville Volunteer Fire Department is a member of the Valley Mutual Aid Association through which all fire departments in the Valley region have made provision for reciprocal firefighter and equipment assistance.

As with police protection, Council acknowledges that fire protection is an essential service. Council will continue to support the Wolfville Volunteer Fire Department in its efforts to provide fire protection to properties within the Town, while giving the department the authority that will enable it to function as a self-regulating group.

Council recognizes that fire prevention is an important component of a comprehensive fire protection program. As required by section 19 of the Fire Safety Act 2003, the Town employs a Fire Inspector who is responsible for identifying potential fire hazards and for informing property owners of appropriate fire prevention techniques.

IT SHALL BE THE POLICY OF COUNCIL:

P FW

1. To continue to support effective police, fire, and emergency services for the Town.



PART 8

NEIGHBOURHOODS

Disclaimer

This has been a core focus of the Housing Accelerator Fund work

8.0 Where We Find Ourselves

Wolfville's Housing Needs Assessment is central to guiding appropriate growth across the Town's neighbourhoods. Council's Housing Accelerator Fund Action Plan acknowledges the need to provide a wider range of housing options for residents, students, and seasonal visitors, while also strengthening the Town's financial resilience through an expanded tax base. This enables better investment in infrastructure, high-quality services, and preparation for future risks

To meet the community's needs in a holistic and equitable manner, neighbourhood policies are organized into four Focus Areas: Housing Choice and Affordability, Land Use Compatibility and Placemaking, Sustainable and Resilient Development, and Neighbourhood Entrepreneurship. These Focus Areas align with Wolfville's Community Priorities—Economic Prosperity, Land Use & Design, Climate Action, and Social Equity—and provide the policy guidance needed to shape development patterns toward Our Shared Future.

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

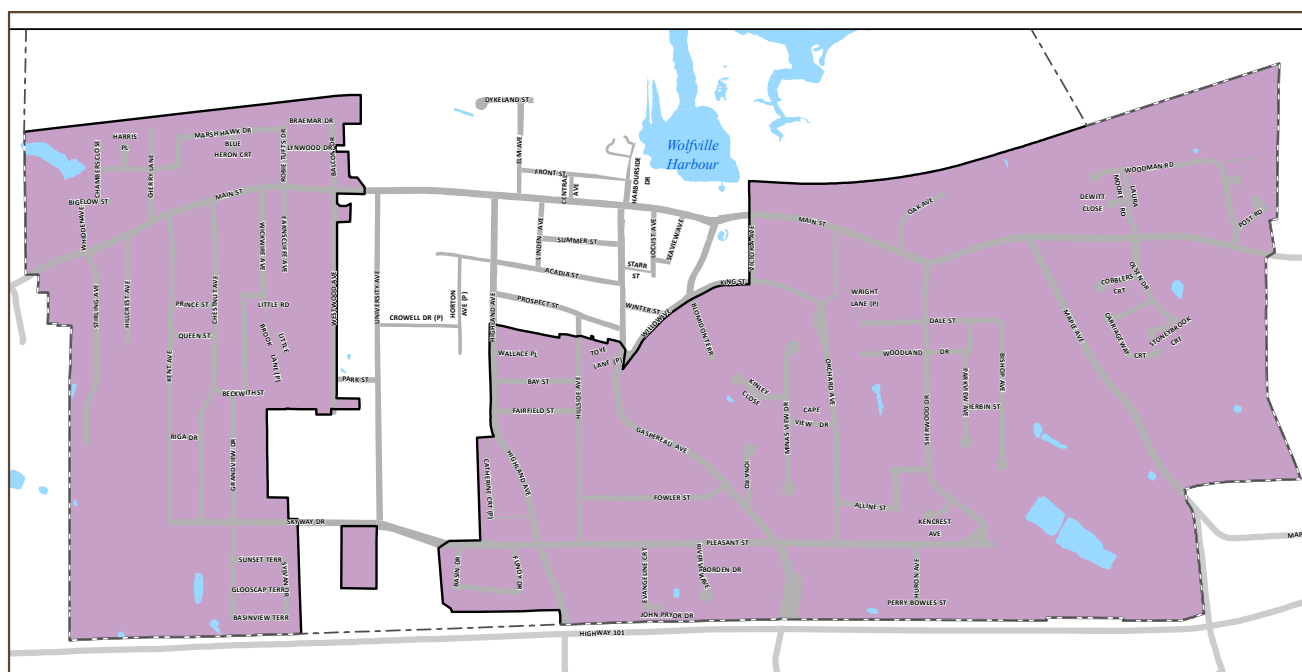


Figure 8.1—Neighbourhood Designation

8.1 Neighbourhood Residential Designation

The Neighbourhood area designation primarily represents the Town's residential neighbourhoods which also contain a number of other uses such as home occupations, parks and open space, home based businesses, neighbourhood commercial uses and Institutional uses such as cemeteries and small scale community buildings.

Population growth and additional development are important for the long-term viability of the Town yet Council recognizes

that growth must be managed and considered appropriately. The policies contained in this Municipal Planning Strategy provide Council with tools to achieve a balanced approach to development that enhances and complements the existing quality of life with infill development and intensification of dwellings to improve community harmony, housing choice, walkability while making sustainable use of municipal services.

IT SHALL BE THE POLICY OF COUNCIL:

1. To establish the Neighbourhood Designation in this Plan and that it be reflected on Schedule 1: Future Land Use Map.
2. To enable the following zones within the Neighbourhood Area Designation and that they be reflected on the Zoning Map of the Land Use Bylaw:
 - a. Residential Low Density - Restricted (RL-R)
 - b. Residential Low Density (RL)
 - c. Residential Medium Density (RM)
 - d. Residential High Density (RH)
 - e. Mixed-use (MU)
 - f. General Institutional (I-1)
 - g. Comprehensive Development District (CDD)
 - h. Parks and Open Space (P-1, P-2)
3. To recognize other zones may be warranted to address future issues and would be considered by amendment to

this plan and/or the Land Use By-law, as per Part 11 – Implementation of this Plan.

4. To enable the rezoning of properties within the Residential Low Density (RL) zone to the Residential Medium Density (RM) zone. The criteria of Section 11 – Implementation is used to evaluate rezoning proposals.
5. To enable the rezoning of properties under Development Agreement to various residential zones that have similar densities and that enable similar uses. The criteria of Section 11 – Implementation is used to evaluate rezoning proposals.
6. To consider new General Institutional uses in the Neighbourhood Designation through a rezoning to the General Institutional (I-1) zone of the Land Use Bylaw.
7. To work toward discharging existing Development Agreements, once developments are complete, and applying the appropriate zoning category.
8. To allow but not require parking in new development.

Core Concept: Housing Choice

The Council recognizes that:

- Providing a variety of housing options allows residents to choose a dwelling that suits their lifestyle.
- A variety of dwelling unit types and sizes can improve affordability.
- Better social sustainability can be achieved within mixed neighbourhoods.
- Housing choice can improve population attraction, retention, and aging in place.
- Missing middle, gentle density, and infill development can make more sustainable use of existing municipal services and land.

8.2 Low Density Residential

Over 40% of residential land in the Town of Wolfville is low density. Most is between 1-to-6 units per acre, which is a very low for a town with centralized services. This plan enables “gentle density” in these areas, meaning housing that fits the low-rise physical form of existing neighbourhoods while enabling more units. Examples include secondary suites, duplexes, townhouses, and cluster homes.

By enabling gentle density, this plan aims to provide a wider range of options for people seeking homes, offer homeowners more options, and increase the Town’s tax base without increasing the tax burden on individual households. In

particular, the plan aims to enable elderly homeowners to subdivide large houses into smaller, more manageable units, and to provide them with rental income, to help them age-in-place.

This plan establishes two variations of the Low Density Residential. The “Residential Low Density” (RL) zone enables up to 5 dwelling units per lot, home-based businesses, and cluster housing. The “Residential Low Density - Restricted” RL-R zone only allows two dwelling units per lot, and limits commercial land uses to home occupations (personal offices).

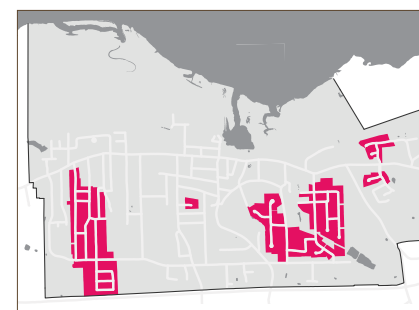


Figure 8.2—RL-R

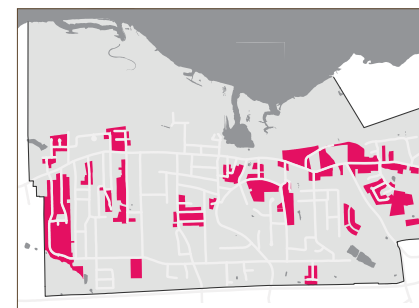


Figure 8.3—RL



IT SHALL BE A POLICY OF COUNCIL:

1. To establish 2 categories of Low Density Residential zoning, Residential Low Density – Restricted (RL-R) and Residential Low Density (RL), which permit a range of low density residential uses including, but not limited to, single-unit dwellings, accessory dwelling units, two-unit dwellings, home-based businesses, short-term rentals and parks as outlined in the Land Use Bylaw.
2. To permit up to 2 dwelling units in the Residential Low Density - Restricted (RL-R) zone and up to 5 in the Low Density Zone, with conditions.
3. To permit townhomes and small-scale multi-unit dwellings in the Residential Low Density (RL) zone, with conditions.
4. To enable cluster housing in the Residential Low Density (RL) zone by site plan approval.
5. To consider innovative housing types that require unique, unusual, or site-specific considerations by development agreement.
6. To regulate accessory dwelling units in the land use bylaw to ensure their compatibility with nearby homes.
7. To consider a future review of the Residential Low Density – Restricted (RL-R) zone to assess how these areas are consistent with the stated community priorities of this plan, other Town priorities and future needs, and provincial priorities.
8. To enable the rezoning of properties within the Low Density Residential (RL) zone, but not the Low Density Residential - Restricted Zone (RL-R), to the Medium Density Residential zone (RM). The criteria of Section 11 – Implementation is used to evaluate rezoning proposals.

8.3 Medium Density Residential

The Medium density residential areas outside of Wolfville's core are generally focused around key transportation corridors – Main Street, Maple Avenue, Skyway Drive and Pleasant Street running east-west, and areas between Gaspereau and Highland Avenues running North-South. Medium density areas face more pressure for redevelopment and change as they are located close to amenities found at the University or the Core area and at the east and west end of the Town.

This Plan encourages more infill and missing middle housing forms such as townhouses, low and mid rise apartments, and cluster developments by enabling additional units on existing serviced lands. Allowing additional units makes increased housing choice possible, contributes to the diversity and vitality of neighbourhoods, and contributes to housing affordability. Additional provisions have been instituted to ensure developments are compatible with neighbourhood character.

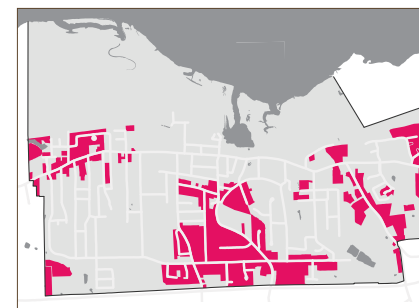


Figure 8.4—Medium

IT SHALL BE A POLICY OF COUNCIL:

1. To establish Residential Medium Density (RM) zoning in the Land Use Bylaw that permits a range of medium density residential uses including, but not limited to, townhouses, cluster housing and low-rise multi-unit dwellings, parks and playgrounds, short-term rentals and home based businesses as outlined in the Land Use Bylaw.
2. To enable townhomes, cluster housing, and multi-unit dwellings, and other uses outlined in the Land Use Bylaw, in the Medium Density Residential (RM) zone by site plan approval.
3. To consider certain uses that require unique, unusual, or site-specific considerations, and projects that exceed a unit count defined in the land use bylaw, in the Medium Density Residential (RM) zone by development agreement.
4. To enable the rezoning of properties within the Medium Density Residential (RM) zone to the Mixed Use zone (MU). The criteria of Section 11 – Implementation is used to evaluate rezoning proposals.

FW LU CA SE



8.4 High Density Residential

Wolfville contains a number of large lots that offer an opportunity to build higher-density housing types. Such development can help Wolfville increase its housing stock and offer a wider range of price points. Multi-family buildings also tend to be more energy efficient than other housing types, and can also help reduce the average environmental footprint of Wolfville residents.

Most of these opportunity sites are within a twenty-minute walk of Downtown. It will be important to offer walking, biking, trail connections, and transit between these development sites and Downtown to ensure residents enjoy a variety of options to reach work and other destinations.

The design of high-density buildings is guided by regulations on height limits, landscaping, amenities, parking design, and active transportation connectivity, so that these buildings are compatible with their surroundings and benefit the community.

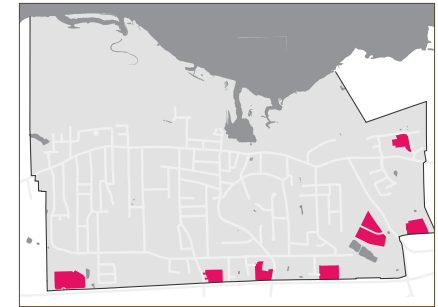


Figure 8.5— High

IT SHALL BE THE POLICY OF COUNCIL:

1. To establish High Density Residential (RH) zoning in the Land Use Bylaw that permits a range of high density residential uses, including but not limited to townhomes, multi-unit dwellings, short-term rentals, home-based businesses, neighbourhood commercial, short-term rentals and parks and playgrounds as outlined in the Land Use Bylaw.
2. To enable town homes, multi-unit residential buildings, and cluster housing in the High Density Residential (RH) zone by site plan approval.
3. To consider certain uses that require unique, unusual, or site-specific considerations in the High Density Residential (RH) zone by development agreement.
4. To include in the Land Use Bylaw special regulation for high density residential uses related to parking areas, landscaping, massing, amenity space, and other matters to ensure neighbourhood compatibility.

FW LU SE



8.5 Mixed Use Areas

The mixed-use zone aims to enable a variety of institutional, commercial, and residential uses to expand the variety of assets available in Wolfville neighbourhoods. These uses may include private schools, inns, nursing homes, shops, and apartments. The zone aims to be flexible so that a variety of building sizes and usage types are viable.

The Mixed Use Zone is the only area outside the Downtown Zone that allows full-time short-term rentals. This policy seeks to enable sufficient accommodations for the tourism industry while preserving the majority of dwelling units, in the majority of neighbourhoods, as housing.

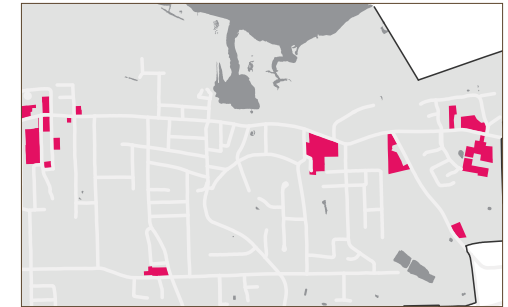


Figure 8.7—R-MU Zone

IT SHALL BE THE POLICY OF COUNCIL:

1. To establish a Mixed Use (MU) zone in the Land Use Bylaw within the Neighbourhood Designation of the MPS.
2. To enable a range of development types as of right within the Mixed Use (MU) zone, and to require site plan approval for any development type that requires additional regulatory considerations.
3. To enable developments in the Mixed Use (MU) zone to feature a mix of commercial, institutional, park, open-space, accommodations, and residential uses, in a variety of forms.
4. To enable full-time short-term rental units and short-term bedroom rentals in the Mixed-Use (MU) zone.
5. To ensure the following criteria are met when Council is considering development proposals in the Mixed Use (MU) zone:
 - a. The minimum net-density of residential dwelling units shall be an average of 10 dwelling units per acre.
 - b. Discourage parking lots between the building and the street in favour of:
 - i. below ground parking; or
 - ii. rear yard or side-lot parking; or
 - iii. on street parking.

8.6 Comprehensive Development District (Future Neighbourhoods)

Previously, the Comprehensive Development Districts in Wolfville were assigned to four large land holdings including the “West End lands”, the “Kenny Farm lands”, the “Maple Avenue lands”, and the “Woodmands Grove lands”. These areas have gone through either a secondary planning process or development agreement to allow development to proceed.

These previous CDD parcels have been zoned to low, medium, mixed use or high density depending on neighbourhood context.

No land is currently zoned as CDD. The Town would like to maintain this zone to allow consideration of future CDD zones on lands greater than one hectare in the future.

IT SHALL BE THE POLICY OF COUNCIL:

1. To establish a Comprehensive Development District (CDD) zone in the Land Use Bylaw for any lands that may be obtained by the Town through an expansion of the Town boundary.
2. To require secondary plans and/or development agreements to enable development within a Comprehensive Development District (CDD) zone.
3. To enable concurrent creation of secondary plans and development agreements for Comprehensive Development Districts.
4. To require all proposals in the Comprehensive Development District (CDD) zone to enable a mix of commercial, institutional, park, open-space, and residential uses, in a variety of forms.
5. To enable certain existing uses on Comprehensive Development District (CDD) zoned properties, as outlined in the Land Use Bylaw.
6. To enable future consideration of CDD zoning on properties greater than one hectare which could be subjected to the zone requirements and secondary planning processes.
7. To enable the rezoning of CDD areas once a development is substantially complete in favour of existing zoning categories with similar development rights.
8. To ensure the following criteria are met when Council is considering development proposals in the Comprehensive Development District (CDD) zone:
 - a. The minimum net-density of residential dwellings units shall be an average of 10 dwelling units per acre.
 - b. Require all power and communication infrastructure to be located underground (both primary and secondary) except in the following situations:
 - i. when 3-phase power is required; and
 - ii. when undergrounding of services negatively impacts significant environment features, such as watercourses, wildlife habitat, areas of steep slopes and similar situations.
9. To require a secondary plan for areas zoned Comprehensive Development District (CDD) that responds to the Town’s ‘Sustainability Checklist’ (Schedule D: of the Land Use Bylaw - Sustainability Checklist for Comprehensive Development Districts).

8.7 Neighbourhood Focus Areas

The following four sections address neighbourhood focus areas and associated policies: Housing Choice and Affordability; Sustainable and Resilient Neighbourhoods; Land Use Compatibility and Placemaking; and Neighbourhood Entrepreneurship. These subsections directly support our Community Priorities (Economic Prosperity, Land Use and Design, Environmental Sustainability, and Social Equity) outlined in Part 2.3 of this Plan and provide actions - in the form of policies that shape our development patterns - to move toward Our Shared Future.



ACTIONING OUR COMMUNITY PRIORITIES

To provide clear linkages to the Town's stated Community Priorities (See Part 2), a policy key / legend has been prepared. Policy in the document has been linked back to themes related to the Town's stated community priorities. Also included are indications where future work or partnerships will be essential. The policy key is included here for reference.

POLICY KEY

EP	Economic Prosperity	LU	Land Use and Design
SE	Social Equity	CA	Climate Action
P	Partnership	FW	Future Work

8.7.1 HOUSING CHOICE AND AFFORDABILITY

The Community Profile and Housing Needs Assessment provides an overview and analysis of Wolfville’s housing needs, growth trends, and points to policy action that can address our community priorities and move us toward the goals outlined in Part 2 of this MPS, Our Shared Future. These documents will be updated over time. Key housing challenges can be addressed through appropriate municipal and other strategies, as outlined in Figure 8.7. These challenges are interconnected to multiple factors and provide many opportunities to improve our quality of life and housing needs.

Council recognizes that certain types of housing require policy interventions to take action on our Community Priorities and address the housing challenges outlined in Figure 8.7. The

policies of this plan are directly linked to the identified priority housing types/forms outlined in Figure 8.7.

These policies are founded on the assumption that increasing the availability of all forms of housing, especially smaller units and ground-oriented attached housing forms, will increase affordability and choice for all residents. With importance placed on age-friendliness and accessibility, the policies also seek to encourage more residential development in areas that are in close proximity to existing or future neighbourhood amenities. Innovative development models, including smaller lot sizes and cluster development or group dwellings, are also a means of increasing housing choices in existing neighbourhoods.

Housing and quality of life issues are complex and require a multi-pronged approach

Housing Supply and Choice

The main recommendation emerging from the Affordable Housing White Paper and the Housing Symposium is for Wolfville to focus its policy efforts on increasing the housing choices available to its residents – in terms of unit types, sizes, densities, and price points.

Key Housing Challenges / Needs	Land Use Strategies
- We need to address the middle (both demographically and in terms of our housing stock). - We need to make housing more affordable for first-time buyers and young families and enable more ‘mortgage helper’ accessory dwelling units. - We need to make sure rental housing is well-managed and safe. - We need to provide more options for the aging population who wish to downsize and/or retire to Wolfville. - We need to build on existing expertise in the community and work toward better non-market housing options.	- Facilitating housing choices; - Encouraging higher densities in strategic areas; - Introducing dwelling type mix targets; - Creating a policy framework that is receptive to innovative housing proposals; and - Focusing priority housing types/forms of: - Attached housing forms and ground-oriented dwelling units; - Supportive housing; - Accessible housing; - Well managed, safe rental housing; - Subsidized, non-market housing; and - Innovative Housing.

Figure 8.7—Key Housing Challenges

IT SHALL BE A POLICY OF COUNCIL:

FW P LU SE

1. Update the Community Profile and Housing Needs Assessment every 5 years or as needed.
2. To encourage the majority of new growth to be missing middle housing to better meet the Town's current housing needs.
3. To set a target for 5% of new dwelling units to be non-market housing. "Non-Market Housing" means housing that is owned, operated, or managed by a non-profit entity that is intended to serve households whose incomes are insufficient to secure adequate housing in the private market.
4. To consider proposals which contribute to the overall supply and diversity of Wolfville's housing stock, which include innovative housing forms and development techniques as outlined in the Land Use Bylaw.
5. To work in partnership with the County, Regional Housing Authority, Provincial partners, government agencies, the private sector and other stakeholders to encourage the development of housing that is affordable for low and moderate income households or individuals.
6. To work within Wolfville to encourage the development of housing that is affordable for low and moderate income households or individuals by:
 - a. Supporting incorporated housing organizations in their efforts (e.g. existing co-ops), such as by obtaining funding for community-based affordable housing initiatives or entertaining proposals on public land;
 - b. Prioritizing the processing of development proposals for non-market and subsidized affordable housing, including by offering maximum flexibility in development agreements;
 - c. Encouraging new affordable housing units to locate near existing community facilities, existing or potential transit and active transportation routes, and in areas of level topography along Main Street and in the Core Area;
 - d. Considering the development of **non-market** housing when disposing of municipally-owned surplus lands.

8.7.2 SUSTAINABLE AND RESILIENT NEIGHBOURHOODS

Wolfville aims to be a more sustainable community and to enhance its environmental, economic, social and cultural strengths to create an unmatched quality of life for both current and future generations. Building compact development within Wolfville is essential for supporting these goals because it reduces development on natural landscapes elsewhere, encourages walking and biking and improves the average energy efficiency of housing. Simultaneously, compact growth strengthens the Town's economic resilience by

bringing more customers to local businesses, and by increasing the tax base on existing infrastructure.

Wolfville cannot meet its environmental, fiscal, and social goals if low-density residential development remains the dominant form for new development. This section establishes neighbourhood policies that require better use of our existing infrastructure; contribute to healthy, walkable neighbourhoods; reduce our share of GHG emissions; and preserve valued natural areas.

IT SHALL BE THE POLICY OF COUNCIL:

FW P CA LU

1. To identify areas where residential development will not be permitted or only permitted subject to certain conditions, in accordance with the Development Constraints and the Zoning Map of the Land Use Bylaw.
2. To encourage infill development throughout all residential areas in Wolfville, with the aim of creating inclusive mixed income neighbourhoods, contributing to the community's vibrancy, increasing housing choice, and improving the Town's financial resilience, by:
 - a. Working with landowners of properties with very large parcels to encourage infill development.
 - b. Enabling in the Land Use Bylaw a range of Secondary Suites (attached/in-home and detached).
3. To limit the size of future residential street blocks to promote efficient development patterns and a walkable transportation network.
4. To prohibit the development of new cul-de-sacs and dead ends to improve traffic flow and neighbourhood walkability, except where geographical or environmental constraints require them.
5. To seek opportunities to establish new streets or paths to reduce the size of large blocks, with the aim of making the street network more walkable.
6. To encourage urban agriculture on residential properties, and if community interest is shown, consider expanding the permitted uses beyond laying hens for affordability, food security, and sustainability reasons.
7. To consider other means of land protection other than outright ownership, such as conservation easements and land trusts.
8. To ensure integration of land use planning and energy/ GHG emissions reduction, consider amendments to this Plan upon the adoption of a GHG emissions reduction, community energy, or similar plan for the Town.
9. To work with other agencies, institutions, organizations and levels of government to ensure the protection of environmentally sensitive and ecologically significant areas.
10. To manage Town owned and leased land in an ecologically sustainable manner according to best management practice and in compliance with applicable legislative requirements

8.7.3 LAND USE COMPATIBILITY AND PLACEMAKING

Council wishes to ensure Wolfville’s neighbourhoods remain healthy, vibrant, safe and people-oriented places to live. The continued success is attributed to the continued growth, and the continued preservation of community character within the Town. To ensure these factors continue in a compatible way, a carefully managed response is required. This response must appropriately recognize impacts of infill or intensification on neighbours balanced with needs of the community for growth and change.

This plan addresses compatibility by focusing on the look and feel from the street, important urban design elements, as well as uses. A development may read as lower density from the street but is provided with the flexibility to provide additional density or use (e.g. home based business or secondary suite) if certain conditions can be met and potential issues mitigated.

IT SHALL BE THE POLICY OF COUNCIL:



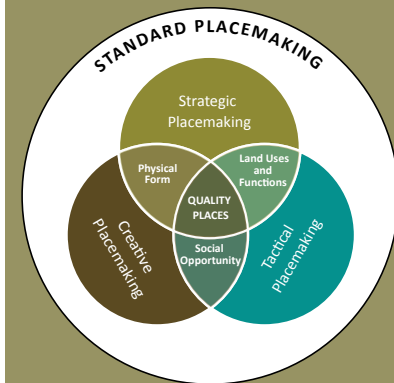
1. To set height limits that preserve the low-rise character of Wolfville neighbourhoods.
2. To introduce the Land Use of ‘Single Room Occupancy’ in the Land Use Bylaw and enable this use in certain zones.
3. To encourage a variety of housing types, designs, sizes and styles.
4. To recognize that compatibility comes in many forms and a particular architectural style or element does not dictate compatibility but rather the overall project and neighbourhood context.
5. To recognize that when considering development proposals, any single development is not a precedent for the approval of another, particularly when conditions have changed over time.

6. To encourage the creation of public spaces that demonstrate:
 - a. clustering of public uses to create a community hub and improve access to services for all residents; and
 - b. distribution of smaller attractions/points of interest throughout the town to improve access to public amenities for users of all ages, incomes and abilities.
7. To encourage Social Inclusion by striving to develop a framework to consider specific small scale projects (including art) that align with the priorities of Council and/or that present an innovative and diverse approach to the temporary or permanent use of space in parks/public spaces.

Core Concept: PLACEMAKING

Compatibility and Social Integration cannot be achieved without Placemaking—the process of creating quality places that people want to live, work, play and learn in. Core Placemaking principles include:

- clustering public uses;
- distributing smaller attractions/ points of interest throughout the town;
- providing municipal support for low-cost, community-led projects;
- allowing more diverse temporary uses in parks/public spaces.



8.7.4 NEIGHBOURHOOD ENTREPRENEURSHIP

Council recognizes that there is a need to create new commercial spaces and studios in Wolfville that can attract and retain entrepreneurs and small business that add value to our local economy. Council also recognizes the need to add additional walkability and density to neighbourhoods. Fostering home-based or live-work opportunities in existing

Neighbourhood areas can support these goals. Allowing shops and services on Connector streets offers an opportunity to give residents more convenient access to their needs, without attracting car traffic or other potential nuisances onto local streets.

IT SHALL BE THE POLICY OF COUNCIL:



1. To enable “Home Based Commercial” uses on the frontage of any parcel, in any zone, that:
 - a. Faces a Connector Street.
 - b. Is within 50 metres of the intersection of two Connector Streets.
2. To use Site Plan Approval for the consideration of new “Home Based Commercial” uses, as outlined in the Land Use Bylaw.
3. To regulate “Home Based Commercial” uses in the Land Use Bylaw to ensure their compatability with surrounding neighbourhoods.
4. To enable economic development opportunities in the Neighbourhood Designation (see Future Land Use Map) in an effort to diversify our local economy, encourage entrepreneurship, reduce people’s reliance on the automobile, and contribute to housing affordability.
5. To permit “Home Occupations” anywhere in the Neighbourhood and Downtown Designations.
6. To permit “Home Based Businesses” within the neighbourhood designation, save and except the Low Density Residential - Restricted (R-1) zone, subject to the specific provisions of the Land Use Bylaw.
7. To establish General Institutional (I-1) zoning in the Land Use Bylaw that permits a range of Local Institutional Uses that are compatible with the Neighbourhood Designation area.
8. To use Site Plan Approval in the General Institutional (I-1) zone of the Neighbourhood Designation as outlined in the Land Use Bylaw for the consideration of certain uses that require additional regulatory considerations.
9. To use Development Agreements in the General Institutional (I-1) zone of the Neighbourhood Designation as outlined in the Land Use Bylaw for the consideration of certain uses that require unique and site-specific considerations.
10. To enable the rezoning of properties within the Neighbourhood Designation, to the General Institutional (I-1) zone where the property can meet the requirements of the Land Use Bylaw and the criteria of MPS Section 11 - Implementation.
11. General Institutional (I-1) rezoning shall not be considered in the Low Density Residential – Restricted (R-1) zone.
12. To include provisions in the Land Use Bylaw to balance impacts on abutting residential properties.

PART 9

Downtown

Disclaimer

This has been a core focus of the Housing Accelerator Fund work

9.0 Downtown Designation

Downtown Wolfville has a unique sense of place, with a mix of colourful buildings, architectural styles, parks, amenities and businesses, all within an easy walk of each other. The street is rich in visual variety, in part because it is composed of many smaller buildings and shops. The Town's charming Main Street continues to be a destination for residents and tourists to shop, eat, drink, socialize, and enjoy the Nova Scotian lifestyle.

Encouraging growth in the Downtown area meets multiple needs. It will help provide more housing in the area of Town with the most services. It will help meet the need for commercial and office space, which is currently lacking. It will also

strengthen the Town's fiscal resilience, because the Downtown generates a disproportionate share of the Town's tax revenue. Growth will also help create more street life and vibrancy, and provide more destinations for visitors and local residents.

The Downtown Designation in the Future Land Use Map covers the same area as the Downtown Zone. This Designation includes some neighbourhoods where commercial uses are allowed, but where commercial uses are not yet prominent. These areas are regulated by the Mixed Commercial Overlay, which allows greater flexibility to match the existing built form.

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

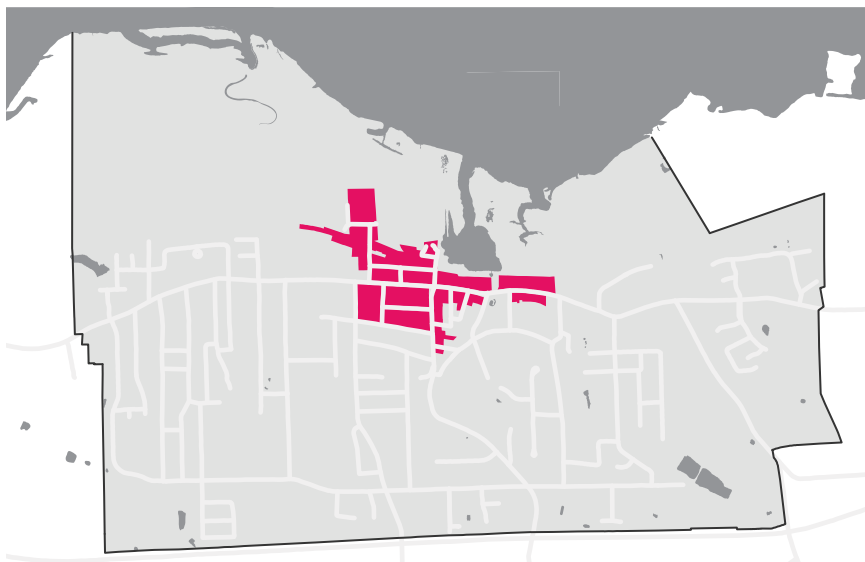


Figure 9.1—Downtown Designation

9.0 DOWNTOWN DESIGNATION POLICIES

IT SHALL BE A POLICY OF COUNCIL:

1. To establish the Downtown Designation within the Municipal Planning Strategy and reflect that on Schedule 1 – Future Land Use Map.
2. To enable the following zones within the Downtown Designation and that they be reflected on Schedule A - Land Use Zoning Map of the Land Use Bylaw:
 - a. Downtown (DT)
 - b. General Institutional (I-1)
 - c. All Parks and Open Space Zones (P-1, P-2, etc)
3. To establish a maximum building height in the Town as defined in the Land Use Bylaw and informed by area Design Guidelines.
4. To prohibit drive through facilities and adult entertainment in association with a commercial use in the Downtown Designations and zones.
5. To allow for directed studies, specific Bylaws, incentives, design guidelines and priority funding to be targeted to this area, specifically:
 - a. To support necessary directed studies or area specific planning for the Downtown Designation, specifically, but not limited to, the area around the Wolfville Memorial Library, Farmer's Market, and Active Transportation Corridor Lands.
 - b. To support the implementation of Nova Scotia Legislature Bill 177 to provide commercial development incentives in defined areas including parking lots and underused land, but not including the Main Street's character-defining historic buildings.
6. To support the creation and incremental enhancement of both the east and west end gateways in an effort to punctuate the sense of arrival into the Downtown and consider integration of, and compatibility with, these gateways when evaluating development proposals in close proximity.
7. To encourage and support the use of transit and provide safe and accessible transit stops within the Downtown.
8. To maintain Design Guidelines for the Downtown Designation, as outlined in the Land Use Bylaw.
9. To require the front yard setbacks for all new development within Wolfville's Downtown Designation be limited in depth, as set out in the Land Use Bylaw and Design Guidelines, except where explicit exceptions are identified in that Bylaw, to provide for a sense of enclosure and pedestrian interest on the street and sidewalks.
10. To encourage appropriate light permeability throughout the Downtown to ensure sufficient sunlight and minimize shadow impacts on the public realm.
11. To retain key views of the dykelands from Central Avenue and of Blomidon from Harbourside Drive.
12. To encourage the provision of mixed use and residential attached development opportunities throughout the Downtown Designation in an effort to encourage contextually appropriate intensification in proximity to the Town's primary service area, and increase the number of adequate, appropriate and affordable housing options.
13. To encourage the growth of the non-market housing sector.

“Wolfville’s Downtown is alive, inviting and connected.

It is a place for everyone—with streets full of busy shops, cafes, entertainment and thriving business.

It is a hub for both commerce and creativity.

It is connected to the broader community through a series of trails, walkways and parks.

It provides gathering spaces and an overall sense of identity for the Town.”

9.1 Downtown Zone



9.1 DOWNTOWN ZONE POLICIES

IT SHALL BE THE POLICY OF COUNCIL:

1. To establish Downtown (DT) zoning in the Land Use Bylaw that permits a broad range of commercial and service uses of a certain size, residential and mixed-use development, parks and playgrounds.
2. To recognize other zones may be warranted to address future land use issues and would be considered by amendment to this plan and/or the Land Use Bylaw, as per Part 11 – Implementation of this Plan.
3. To enable development as-of-right that meets the requirements of the Land Use Bylaw.
4. To use the Site Plan Approval process in the Downtown zone for the consideration of certain uses that require additional regulatory considerations.
5. To use Development Agreements in the Downtown Designation for the consideration of certain uses that require unique and site-specific considerations, or for non-market housing projects above 4 stories.
6. To establish specific criteria in the Land Use Bylaw for new or expanded large format commercial development related to items such as parking, landscaping, loading, signage, access, and outdoor storage, and where such stores are allowed.
7. To establish Design Guidelines for the Town as outlined in the Land Use Bylaw, that recognize the importance of historical forms of architecture and inform future developments in the Downtown.
8. To provide special abutting zone requirements in the Land Use Bylaw where a commercial zone abuts a residential zone.
9. To require a commercial or other active use on the ground floor of buildings in the Downtown as defined in the Land Use Bylaw, including any exceptions.
10. To investigate potential reuse and redevelopment options for vacant Town owned commercial properties including the possibility of preparing conceptual development plans for these parcels and taking the necessary steps to have these lands pre-approved for development.
11. To support the creation of small lots through the reduction of minimum lot sizes and frontages in the Downtown, to enable the creation of new lots and the construction of dwelling units and/or mixed uses in appropriate locations, as outlined in the Land Use Bylaw and Subdivision Bylaw.
12. To maintain the Town's fine-grain visual diversity by limiting the width of storefronts on Main Street in the area shown in Figure 9.2, and by exploring the potential to limit the width of new and consolidated parcels in this area.



Figure 9.2—Area of Downtown where a maximum storefront width will be applied.

9.2 Mixed-Commercial and Large-Format Commercial Areas

The Downtown Zone is being expanded to enable more housing, offices, and commercial uses. Some of the newly included areas are residential in nature, and these areas are likely to remain partly residential for the foreseeable future. A Mixed-Commercial Area has been established for these areas to enable a smooth transition over time into a more commercial character, which offers builders more flexibility to blend the design of their projects with surrounding buildings. The Schedule enables buildings to be residential-only, requires less window coverage and enables a greater setback from the street, among other policies.

There are multiple motivations to expand the downtown into nearby neighbourhoods. It will help provide more compact housing to meet Wolfville's housing goals. It will help Wolfville

remain financially secure in the face of future risks, given that the Downtown generates a disproportionate share of the Town's tax income. A larger downtown will also better meet the need for more commercial space and for offices. Today, there is little space for entrepreneurs to open new businesses, or for professionals—such as for lawyers, accountants, and dentists—to offer their services.

The Downtown also includes a large-format commercial property that sells building materials. This property cannot meet the design guidelines of the rest of the Downtown due to the nature of the business. A Large-Commercial Format Area has been established to enable its distinct needs.

9.2.1 MIXED COMMERCIAL & LARGE FORMAT COMMERCIAL AREAS POLICIES

IT SHALL BE THE POLICY OF COUNCIL:

1. To establish a Mixed-Commercial Area in Schedule A1 - Area A of the Downtown Zone of the Land Use Bylaw.
2. To allow greater flexibility in the Mixed-Commercial Area to enable buildings to better match the design of existing built environment, if desired, with policies in the Land Use Bylaw that enable:
 - a. Residential-only buildings
 - b. Larger setbacks from the sidewalk.
 - c. A lower proportion of windows on the streetwall.
3. To encourage existing buildings in the Mixed-Commercial Area to be repurposed as businesses, professional offices, live-work spaces, and for other commercial or semi-commercial uses.
4. To encourage business types in the Mixed-Commercial Area that will attract street life and vibrancy, including cafés, restaurants, and patios.
5. To establish a Large-Format Commercial Area in Schedule A1 - Area B of the Downtown Zone of the Land Use Bylaw.
6. To allow greater flexibility in the Large-Format Commercial Area including with policies in the Land Use Bylaw that enable:
 - a. Unlimited setback from the street.
 - b. Parking lots between the sidewalk and the building.
 - c. Unlimited building dimensions.
7. To consider eliminating Schedule A1 - Area B in the future if its current land use changes, or if the opportunity emerges to better integrate the area into the Downtown.

9.3 Downtown Rail Line Corridor

The Harvest Moon Trail is an active-transportation route that connects Wolfville to towns and destinations across the Annapolis Valley. The trail is also a tourist destination in its own right, and one of the Downtown's greatest strengths. It is critical to protect and enhance this active transportation corridor.

There are opportunities to improve the trail. It runs through parking lots and the back side of buildings, and it is not visibly connected to the Downtown. The Town can better leverage the trail by investing in placemaking interventions and improved public spaces along its length to create a more inviting experience. Public space investments on Front Street also has the potential to better connect Main Street to the trail, inviting trail users to visit Main Street, and vice versa.

The corridor is privately owned and leased by the Town. There is an opportunity to work with the landowner to encourage the

construction of new buildings along wider sections of the corridor that will help announce one's arrival in Town, while protecting the core recreation function of the trail. These buildings can be required to include businesses facing the trail to help create a lively atmosphere and to better meet the needs of trail users. Enabling new buildings along the trail will require greater flexibility with regard to frontage and servicing requirements.

Secondary Planning work has been done (revisiting work done in 2011) on the Downtown Rail Line sections to enable infill development, where appropriate, and width allows. Site Plan approval is being used for less complex areas and Development Agreements where public/private interests would need further consideration

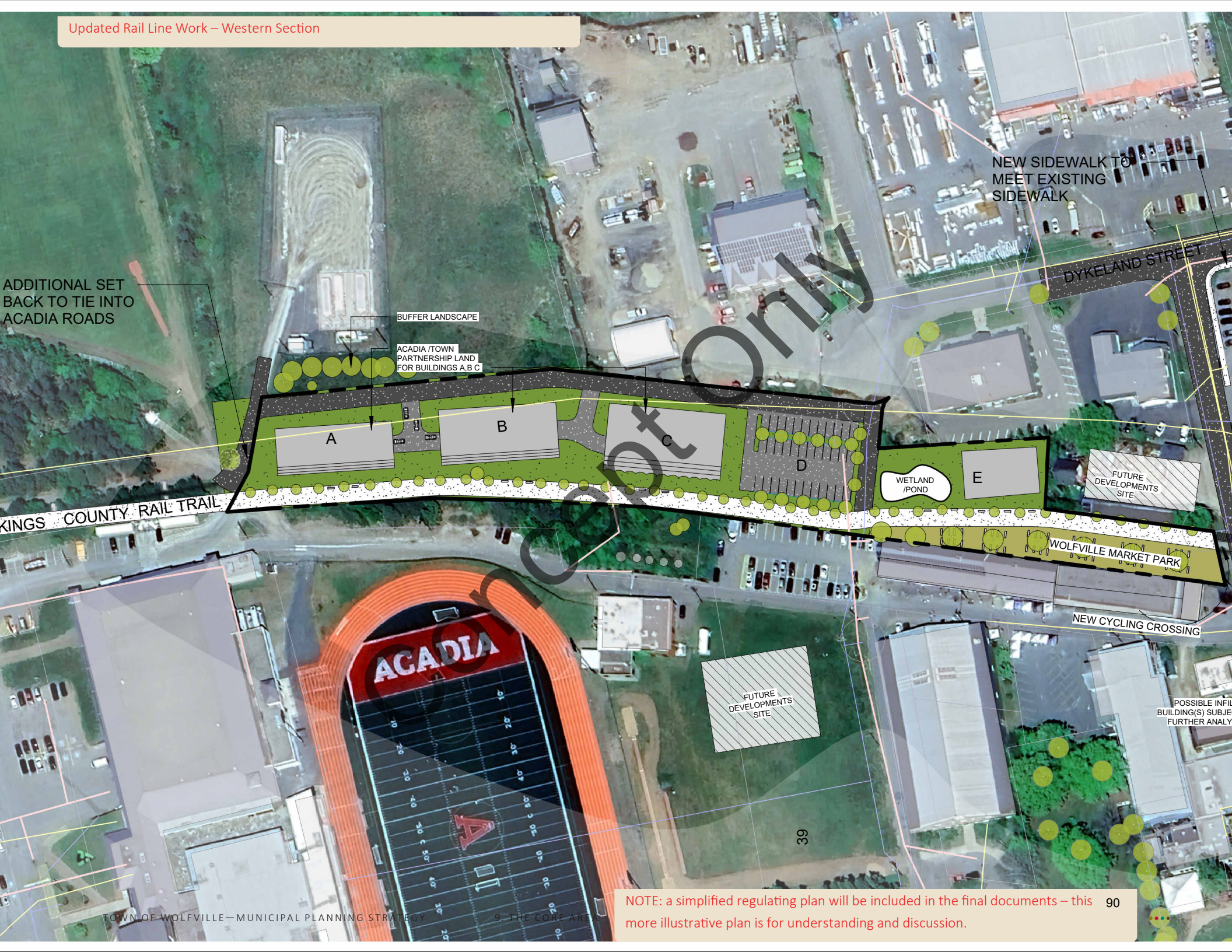
9.3.1 ACTIVE TRANSPORTATION CORRIDOR POLICIES

IT SHALL BE THE POLICY OF COUNCIL:

1. To invest in public spaces along the Harvest Moon trail to create a more inviting, lively experience.
2. To invest in connections between Main Street and the Harvest Moon Trail, including with strategic public space and streetscape improvements.
3. To enable the development of new buildings in specific places along the active-transportation route by site plan approval in Area C identified on Schedule A1 of the LUB.
4. To enable greater flexibility on frontage and access requirements for these new buildings, so that they may face laneways and/or parking lots rather than streets.
5. To require these buildings to treat the trail as a street for the purposes of design and massing regulations in the Land Use Bylaw and in the Design Guidelines.
6. To allow greater flexibility in the rail corridor with policies in the Land Use Bylaw that enable, where needed, development with no direct road frontage, and the ability to have in-direct access for vehicles and/or water, sewer, and power services, if easements are provided to the satisfaction of the Development Officer.



NOTE: a simplified regulating plan will be included in the final documents – this more illustrative plan is for understanding and discussion.



ADDITIONAL SET
BACK TO TIE INTO
ACADIA ROADS

BUFFER LANDSCAPE

ACADIA/TOWN
PARTNERSHIP LAND
FOR BUILDINGS A,B,C

A

B

C

D

WETLAND
/POND

E

FUTURE
DEVELOPMENTS
SITE

KINGS COUNTY RAIL TRAIL

WOLFVILLE MARKET PARK

NEW CYCLING CROSSING

ACADIA

FUTURE
DEVELOPMENTS
SITE

POSSIBLE INFILL
BUILDING(S) SUBJECT
FURTHER ANALYSIS

39

NOTE: a simplified regulating plan will be included in the final documents – this more illustrative plan is for understanding and discussion.



NOTE: a simplified regulating plan will be included in the final documents – this more illustrative plan is for understanding and discussion.



NOTE: a simplified regulating plan will be included in the final documents – this more illustrative plan is for understanding and discussion.

9.4 Downtown Heritage

Heritage is essential for the long-term growth and prosperity of Wolfville's Downtown. It is the Main Street's unique heritage streetscapes that draw visitors to Wolfville, and that gives new residents reason to live here. Heritage also plays a central role in defining local identity and pride.

The top priority area of the Downtown to preserve heritage is on Main Street, between Highland Ave. and Seaview Ave, as

shown in the map. This is also the central economic engine of Wolfville. It is therefore critical to establish policies that enable growth and investment in this area of Main Street while also preserving and enhancing its heritage value.

9.4.1 DOWNTOWN HERITAGE POLICIES

IT SHALL BE THE POLICY OF COUNCIL:

1. To conduct a Heritage Study for the Downtown Main Street area to identify heritage assets and to explore tools for preserving these features.
2. To consider identifying properties, based on this study, where development should receive greater regulatory scrutiny due to their heritage significance.
3. To work with The Heritage committee and other Heritage groups in designating additional Heritage properties in the Downtown Designation, under the Nova Scotia Heritage Property Act.
4. To explore incentives to encourage landowners to register their properties under the Heritage Property Act.
5. To ensure Heritage Assets are documented and that proposals in proximity to registered properties are appropriately regulated in the Land Use Bylaw, informed by the Design Guidelines.
6. To consider creating a fund to help landowners invest in preserving and enhancing heritage assets along the Downtown Main Street Area.



9.6 Development Incentives

In 2016, the Province of Nova Scotia passed Bill 177, which enables municipalities to incentivize development in identified “commercial development districts” by phasing in commercial property taxes over a set period of up to ten years. Such a tool could help Wolfville attract infill development for its Downtown in strategic places.

These incentives can help encourage investment in the new Mixed-Commercial Areas, where there are currently few or no businesses. An incentive can help establish an initial critical mass of commercial activity to attract street life, which can then provide customers to attract new businesses. Similarly, incentives could be used to encourage mixed-use development along the Rail Corridor to encourage greater vibrancy on the trail.

These incentives would also be valuable to encourage land owners to redevelop parking lots other underused land with the goal of providing more housing and commercial space in the downtown. In particular, the incentives could help attract development to Front Street, where large parking lots undermine the pedestrian experience. Figure 9.4 provides an illustration of what this commercial development district may include, though this may be updated during the process of writing the enabling bylaw.

There is also an opportunity to encourage private investment on Front Street by making investments in publicly-owned land on the street. If a new free public parking lot is built, it creates an opportunity to explore the potential to repurpose public parking on Front Street for new buildings, affordable housing, or expanded public space.



Figure 9.4—Areas to apply development incentives under Bill 177.

9.6.1 DEVELOPMENT INCENTIVE POLICIES

IT SHALL BE THE POLICY OF COUNCIL:

1. To establish a bylaw to enable a commercial property tax phase-in incentive for certain areas of the Downtown, which will outline at minimum:
 - a. The objectives of the tool.
 - b. The “commercial development district” where the incentives apply, and the justification for its boundaries.
 - c. The eligibility parameters for incentives.
 - d. The phase-in period for commercial property taxes, and the formula for phasing in property taxes.
 - e. The mechanism for reducing commercial property taxes.
2. To not include historic buildings on Main Street in the incentives program.
3. To consult property owners, the business community, and the public on the enabling bylaw for these incentives before enacting it.
4. To explore investing in new buildings, non-market housing, and/or expanded parks on public parking spots on Front Street to help encourage investment on this street.
5. To time public investments on Front Street with the proposed new public parking lot to avoid reducing available parking downtown.

9.7 Placemaking

Downtown Wolfville benefits from beautiful, vibrant streets and public spaces, including Main Street, the Waterfront Park, Harvest Moon Trail, and the Farmer's Market (on relevant days). However, many of the streets and paths that connect these destinations offer much less to see or do. Streetlife exists in pockets in the Downtown, but is separated by large gaps. There is also little visual connection between these destinations. A visitor standing on Main Street may not realize that the trail, waterfront, and market are there, and vice versa.

The Town of Wolfville will explore the potential to establish a placemaking program to make small, incremental, yearly investments in the Downtown to bring more life and activity,

and to help create visual connections between destinations. Such investments have the potential to attract development in strategic places, such as Front Street. A yearly program would also offer residents, business owners, and other stakeholders the opportunity to regularly participate in selecting and implementing these improvements, strengthening local social ties and cooperation.

Current examples of placemaking in the Downtown include the Waterfront hammocks and the Willow park splashpad. The Town will explore potential sources of funding for such a yearly investment program, such as parking revenue or future tax uplift in the area.



9.7.1 PLACEMAKING POLICIES

IT SHALL BE THE POLICY OF COUNCIL:

1. To explore the potential to create a program for yearly placemaking investments.
2. To explore potential funding sources including paid parking to ensure that such a program is financially sustainable.
3. To use placemaking interventions to better connect destinations throughout the Downtown, including, but not limited to, Main Street, the Waterfront, the Harvest Moon Trail, and the Farmer's Market.
4. To involve the public, business owners, and other stakeholders in selecting and implementing placemaking projects.
5. To encourage and participate in the beautification and upgrading of the Downtown through streetscaping and public art initiatives to create intrigue and comfort for pedestrians at street level.
6. To encourage and support initiatives aimed at increasing pedestrian traffic and fostering "life on the street" such as sidewalk cafés, outdoor markets, concerts and entertainment.

PART 10

ACADIA UNIVERSITY INSTITUTIONAL

Disclaimer

This is a Draft document for review.

10.0 Town and Gown

Acadia has offered exemplary learning opportunities in a historic, small town environment since it's creation in 1838. The University owns more than two hundred acres of land that stretches from the Dykelands, north of the former rail line, southward to Highway 101 and from Westwood Avenue to Highland Avenue. One hundred and twenty acres of this land holding is developed.

The presence of the University exerts a substantial influence on the economic, cultural and social fabric of the community by providing employment and identity to the region. The identity, ongoing vitality and economic prosperity of both the town and the University are intrinsically linked through shared uses, places and populations. The future of the Town of Wolfville is reliant on a shared understanding of the needs and capacity of both the University and the Town.

New development on Acadia campus could offer a variety of benefits, including new income streams for the university, and new shops and services for students and residents. Additional housing would also better meet the campus's housing needs while helping to address the Town's overall housing shortage. This plan therefore seeks to encourage investment in new buildings on Acadia campus.

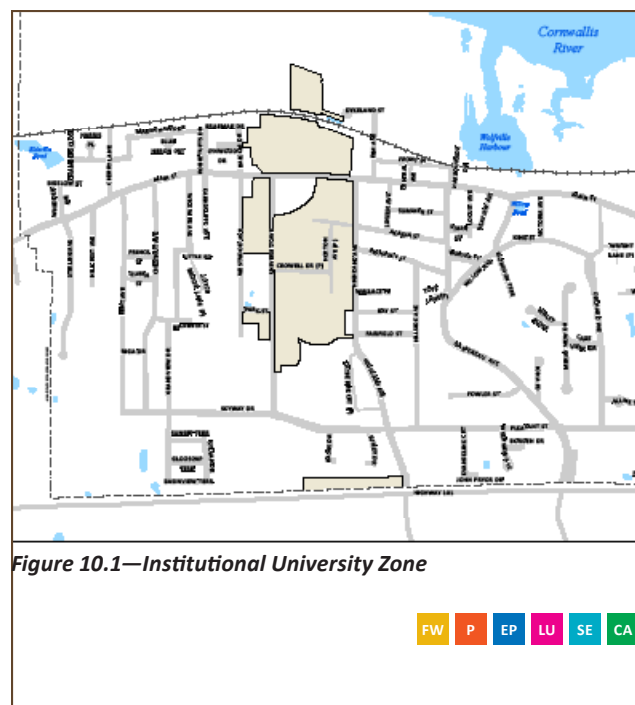


Figure 10.1—Institutional University Zone

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

IT SHALL BE THE POLICY OF COUNCIL:

1. To maintain a 'Partnership Agreement' with Acadia University in order to address areas of mutual concern and benefit, including Land Use Planning.
2. To support the following objectives with respect to Acadia University:
 - a. to recognize Acadia University as a distinct community of interest that is dispersed throughout the Town;
 - b. to foster co-operation between the Town and the University in terms of such matters as the provision of parking, student housing, servicing, conservation of

heritage buildings and areas, access and development proposals;

- c. to support the growth and redevelopment of the University and to encourage its long-term vitality within the Town;
 - d. to encourage, in particular, the construction of new student housing on Acadia-owned lands;
 - e. to minimize any adverse effects the University may have as a major activity centre on adjacent and surrounding neighbourhoods;
 - f. to recognize the various University areas as having different land use mixes, land use characteristics, locational factors, and different surrounding uses; and
 - g. to encourage Acadia University to protect and conserve their various heritage properties whether designated or not.
3. To designate as University those lands of Acadia University and currently being used for University purposes and generally bounded by Westwood Ave, Highland Ave, The Dykelands and Highway 101 as shown on Schedule 1 - Future Land Use Map.
 4. To establish an Institutional University (I-2) zone in the Land Use Bylaw. This zone is intended to include existing University uses.
 5. To recognize the role the University campus plays in the provision of Wolfville's public open spaces and to zone open space portions of the campus Parks and Open Space – University (P-2) as outlined on Schedule A of the Land Use ByLaw.
 6. To use Site Plan Approval in the Institutional University Designation for the consideration of certain uses that require additional regulatory considerations, as outlined in the Land Use ByLaw.
 7. To use Development Agreements in the Institutional University Designation for the consideration of certain

uses that require unique and site-specific considerations as outlined in the Land Use Bylaw.

8. To establish appropriate regulation for the uses permitted (as-of-right) or enabled for consideration in the Institutional University (I-2) zone of the Land Use Bylaw.
9. To permit, in the Land Use Bylaw, building heights in the Institutional University (I-2) zone congruent with the type of use and scale of present buildings located in the core of the University campus.
10. To restrict the setbacks of new buildings in the Institutional University (I-2) zone as outlined in the Land Use Bylaw, on those properties which abut residential zones or have frontage on Main Street to ensure greater architectural compatibility.
11. To establish abutting yard requirements in the Land Use Bylaw, which may include but is not limited to fencing, screening, parking lot location and standards where any yard in the Institutional University zone abuts a residential zone.
12. To recognize that the University development on the Dykelands area is intended to be used for accessory and utilitarian uses that support the University and not to accommodate principal University buildings or residences, as outlined in the Land Use Bylaw.
13. To recognize that complementary land uses to those permitted on Institutional University (I-2) land area those which allow the University to function as a community and shall be enabled through the Land Use Bylaw. These uses may include, but not be limited to:
 - a. classrooms, laboratories and similar educational and research facilities
 - b. residential buildings for the primary use of housing students and faculty
 - c. places of assembly
 - d. foodservice facilities for the primary use of students



- e. sport facilities
 - f. University offices
 - g. bookstores for the primary use of students
 - h. student services buildings
 - i. convention facilities as a secondary use of any other facilities
 - j. public and community gardens
14. To recognize that housing accommodation for post-secondary school students has specific land use characteristics and impacts on surrounding areas that require regulation through zoning and other means.
 15. To recognize that the Town shares a responsibility, along with Acadia University, and private landowners for ensuring that students have access to accommodation that is affordable, safe, accessible and sanitary.
 16. To support Acadia University to create an update to the most current Acadia University Campus Masterplan (2003) which establishes the location for:
 - a. Recreational and Active Sports Parks;
 - b. Open Spaces;
 - c. New residential uses;
 - d. Access road onto the public road network and Active Transportation opportunities; and
 - e. Community Buildings.
 17. That if campus areas currently designated University become available for non-University development, to undertake the completion of a secondary plan for redevelopment that includes analyses of transportation, infrastructure, and financial implications to be reviewed by the Town.
 18. To recognize the role of the University in Core Area management and consider this during development applications as outlined in the Land Use Bylaw.





PART 11

IMPLEMENTATION

Disclaimer

This is a Draft document for review.

11.1 Municipal Government Act (MGA)

This Plan will be implemented through a variety of means including the Land Use Bylaw, Core Area and Neighbourhood Design Guidelines, Subdivision Bylaw, development approvals (as-of-right, site plan, development agreement), rezonings, incentives (e.g. Bill 177), secondary or small area planning, public engagement and other mechanisms. It will be necessary for the Town to make investments necessary to support growth while maintaining a high quality of life for residents.

This plan is meant to be a living document that will be monitored for its effectiveness in achieving our community priorities.

The Municipal Government Act (MGA) is the Provincial legislation that sets out the regulations that govern municipal planning and development in Nova Scotia. The MGA describes the various methods of development control available to Nova Scotian municipalities for regulating development and land use decisions within their jurisdiction.

IT SHALL BE THE POLICY OF COUNCIL:

1. To ensure planning processes meet the requirements of the MGA when considering development agreements, making amendments, and other planning decisions.
2. To work with the Province and other partners to improve Provincial planning legislation, policy and procedures.

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The policies of this plan action our Community Priorities and move us toward Our Shared Future.

11.2 Municipal Planning Strategy (MPS)

The Municipal Planning Strategy is the main document through which the future growth and development of the Town shall be encouraged, controlled and co-ordinated. The policies of the Municipal Planning Strategy will be implemented through the powers provided Council in the Municipal Government Act and other relevant statutes. The principle regulatory documents that are used to implement the policies of the Municipal Planning Strategy are the Land Use Bylaw, Design Guidelines and the Subdivision Bylaw.

This Municipal Planning Strategy and any subsequent amendments shall be reviewed pursuant to Subsection 214(2)

of the Municipal Government Act when deemed necessary by the Minister of Municipal Affairs or by Council. The policies of this Municipal Planning Strategy will be monitored as to their effectiveness in achieving the stated community priorities and amendments made, as needed. Council is committed to ensuring this is a living document.

The Municipal Planning Strategy approved by the Minister of Municipal Affairs on September 23, 2008 and subsequent amendments thereto is repealed upon the date of coming into force and effect of this Municipal Planning Strategy.

IT SHALL BE THE POLICY OF COUNCIL:

1. To enable the planning process and the implementation of the Municipal Planning Strategy with advice and assistance from the Planning Advisory Committee and Design Review Committee.
2. To evaluate the effectiveness of MPS policies and their enabling regulations with regard to the implementation of the 'Shared Future' (Vision), Community Priorities and Objectives established in this plan. Any evaluation process must:
 - a. establish indicators for each objective in Part 2 of this Plan.
 - b. involve consultation with the public and other stakeholders.
 - c. establish an ongoing feedback process that communicates the findings of the evaluation to Council and the public.
3. To consider having regular evaluations corresponding to regular macro events such as Municipal elections or the release of census data.

4. To consider the Municipal Planning Strategy a living document and require an amendment:
 - a. where any policy intent is to be altered; or
 - b. where a text or map amendment to the Land Use Bylaw would conflict with the text or maps of the Municipal Planning Strategy; or
 - c. where an amendment to the Subdivision Bylaw would conflict with the text of the Municipal Planning Strategy; or
 - d. where a future land use map change is deemed appropriate; or
 - e. where an amendment is needed to conform with a Statement of Provincial Interest, or to implement a recommendation of a Priority Plan that has been approved by Council; or
 - f. where an amendment could significantly advance municipal objectives under statutes or regulations enacted by Provincial or Federal Government; or where Provincial or Federal programs are introduced that can benefit the Municipality and support the goals and objectives of this Plan; or
 - g. where an economic, environmental, cultural or social opportunity arises that is unforeseen, is of regional or Provincial significance, and would further the objectives of this plan; or
 - h. where housekeeping/minor amendments are required.

5. To use other Municipal Plans (e.g. Asset Management Plan, Accessibility Plan, Climate Change Action Plan, etc.) and their evolution moving forward, to inform potential amendments to the Town's planning documents and aid in the implementation of this Plan.

11.3 Secondary Planning Strategies and Area Specific Plans

A Secondary Strategy is a Town-led planning approach ideally suited for areas of the Town where there are unique and diverse planning issues that need to be considered. A Secondary Strategy (or area-specific plan) should, as much as possible, be consistent with the policies in the general municipal-wide Strategy to eliminate any confusion between the two documents. The policies may differ, to address unique circumstances in the Secondary Strategy area. If appropriate, the Secondary Strategy would form part of the Town-wide

MPS and be referenced in the document, once adopted by Council.

This is a new process for Wolfville brought into effect through this Municipal Planning Strategy review. Secondary Plans have primarily recommended for the large vacant land parcels designated as Comprehensive Development District on Schedule A of the Land Use Bylaw but may be warranted in the Core Area as the area faces development pressure in key areas.

IT SHALL BE A POLICY OF COUNCIL:

1. To recognize secondary strategies and small area plans provide more detailed direction for development in specific areas of the Town.
2. To use the Comprehensive Development District Zone's 'Sustainability Checklist' (Schedule D of the Land Use Bylaw) as a tool when considering secondary or small area plans.
3. To ensure other policies of this plan, and associated Land Use Bylaw and relevant Design Guidelines are considered in the formulation of a secondary strategy or area planning exercise.
4. To require a secondary plan be prepared prior to approval of development in areas zoned Comprehensive Development District (CDD) in the Neighbourhood Designation.
5. To consider new secondary or small area plans be prepared prior to approval of development in other complex areas of Town (e.g. along the Active Transportation Corridor).

11.4 Land Use Bylaw and Development Agreements

The Land Use Bylaw shall be the principal means for implementing the policy statements included within this Municipal Planning Strategy (MPS), pursuant to Sections 219 and 220 of the Municipal Government Act. The Land Use Bylaw sets out zones, permitted uses, general provisions, and development standards that reflect the policies of this

Strategy, as provided for by the MGA. The Zoning Map of the Land Use Bylaw shows graphically and specifically the area to which the provisions of individual zones shall apply. Development Agreements are negotiated contracts between the Town and a Land owner to create site-specific land use controls.

IT SHALL BE THE POLICY OF COUNCIL:

1. To adopt a comprehensive Land Use Bylaw, setting out specific requirements to implement this Municipal Planning Strategy.
 2. To appoint a Development Officer to administer the Land Use Bylaw and Subdivision Bylaw to issue, deny, or revoke any permit or approval under the terms of these Bylaws.
3. To require in the Land Use Bylaw that any development permit issued shall lapse and become null and void if the development has not begun within one (1) year of the date of issue of the permit. Any development permit issued may be revoked by the Development Officer where there is reason to believe that the development permit was issued as the result of mistaken or false information.

11.4.1 LAND USE BYLAW (LUB) AMENDMENTS

Proposals for rezonings, text amendments to the Land Use Bylaw and/or development agreements may be required to ensure community needs and desires are being met. Specific

policy criteria are outlined in this section to ensure a standard and uniform approach is taken by Council in responding to these requests.

IT SHALL BE THE POLICY OF COUNCIL:

1. To consider amendments to the Land Use Bylaw provided the amendment is consistent with the intent of the Municipal Planning Strategy.
 2. To consider zone-specific considerations for rezonings, including locational criteria or street hierarchy requirements, in the Land Use Bylaw.
 3. To use the general criteria, outlined in Section 11.4.3, as a guide in the evaluation of Land Use Bylaw amendments.
 4. To consider an application for amendment to the Land Use Bylaw (rezoning) only if the application has identified
- a proposed use for the property. Council shall give consideration to both the proposed use and to the impact of other uses permitted in the requested zone.
 5. To consider an application for amendment to the Land Use Bylaw only if the site meets all of the lot size and zone standards for the zone sought.



11.4.2 DEVELOPMENT AGREEMENTS

Development agreements are primarily intended to be used for developments proposed beyond the limits of the applicable Land Use Bylaw requirements, particularly in instances where Council believes land use conflicts are more likely to occur. For example, these may be developments of large scales, alternative design, a comprehensive nature, or uses of a particularly sensitive or potentially high-impact nature.

Development agreements are also useful in providing Council with the ability to negotiate terms and conditions of a project, while retaining the approval authority. This is important in ensuring large scale and sensitive developments are carried out in a manner that keeps the overall benefit of the community and the Town in mind.

IT SHALL BE THE POLICY OF COUNCIL:

1. To indicate uses considered by development agreement in the Land Use Bylaw.
2. To enter into a development agreement pursuant to the Municipal Government Act on the terms and conditions set forth in this Municipal Planning Strategy and a development agreement shall:
 - a. specify the development, expansion, alteration, or change permitted; and
 - b. specify the conditions under which the development may occur; and
 - c. set forth the terms by which Town Council may terminate the agreement.
3. To have the provisions of the Land Use Bylaw prevail after discharge of any development agreement.
4. To enable the Development Officer to bring certain as-of-right and site plan applications forward to Council for consideration by Development Agreement where the application meets the intent of this planning strategy but is unable to meet specific, prescriptive requirement(s) of the Land Use Bylaw.

11.4.3 CRITERIA FOR LUB AMENDMENTS AND CONSIDERATION OF DEVELOPMENT AGREEMENTS

IT SHALL BE THE POLICY OF COUNCIL:

1. That, where warranted, the developer shall be responsible for providing further study of specific issues identified in the development review process, and/or implementing mitigation measures to address anticipated issues such as flooding and erosion threats.
2. That designation and/or zone specific development agreement and rezoning requirements shall be included in the Land Use Bylaw.
3. That when considering any proposed development agreement or amendment to the Land Use Bylaw, it shall be a policy of Council to have regard for the following matters:

GENERAL CONSIDERATIONS

1. The financial ability of the Town to absorb any costs relating to the proposal; and
2. The proposal's conformance with the intent of the Municipal Planning Strategy and to all other applicable Town Bylaws and regulations.
3. The proposal's conformance with relevant portions of the Town's 'Sustainability Checklist', where applicable.

COMPATIBILITY OF THE DEVELOPMENT

4. Compatibility of the proposed land use with adjacent land uses; and
5. Compatibility of the development with adjacent properties in terms of height, scale, lot coverage, density, bulk, and architectural style; and
6. Compatibility of the development with adjacent properties in terms of lighting, signage, outdoor display, outdoor storage, traffic impacts, and noise; and
7. Integration of the development into the surrounding area by means of appropriate landscaping, with screening provided by existing and new vegetation and fencing as required; and
8. The proposal protects and preserves matters of public interest such as, but not limited to:
 - a. historically significant buildings;
 - b. public access to shorelines, parks, and public and community facilities; and
 - c. important and significant cultural features, natural land features and vegetation.

SERVICING CONSIDERATIONS

9. The adequacy of sanitary services, water services, and storm water management services; and
10. Contribution of the proposal towards an orderly and compact development pattern that makes efficient use of existing and new municipal infrastructure and services.

MOBILITY CONSIDERATIONS

11. The adequacy of the road network in, and adjacent to, or leading to the development, regarding connectivity, congestion and traffic hazards; and
12. The adequacy of site access as determined by the Traffic Authority; and
13. The ability of emergency services to respond to an emergency at the location of the proposed development; and
14. The adequacy of active transportation infrastructure to support walking and cycling to and from the proposed development; and
15. The provision of off-street vehicle and bicycle parking to prevent significant congestion, nuisance and inconvenience in the area.

ENVIRONMENTAL CONSIDERATIONS

16. Consideration of any previous uses of the site which may have caused soil or groundwater contamination; and
17. Suitability of the site in terms of slope and flood and erosion risk in accordance with Schedule E of the Land Use Bylaw; and
18. Consideration of any anticipated environmental impacts resulting from the development, such as air and water pollution, soil contamination, and potential for the

contamination or sedimentation of watercourses. Where Council determines, on the advice of a qualified person, that there is a significant risk of environmental damage from any proposed development, an environmental impact assessment shall be carried out by the developer for the purpose of determining the nature and extent of any impact and no agreement shall be approved until Council is satisfied that the proposed development will not create or result in undue environmental damage; and

19. The application of sustainable design principles and energy efficient technology, including but not limited to renewable energy infrastructure; and
20. Environmentally friendly paving alternatives, provision of alternative transportation parking, integration of landscaping into the design of parking lots, green roofs, etc.

11.4.4 DESIGN GUIDELINES

Council intends to control or influence certain aspects of the Town's built form and the impacts on the public realm by using Design Guidelines in certain areas of the Town. The Guidelines are also meant to educate the public, developers and other stakeholders on the Town's Architectural Heritage and general design principles.

Built form requirements (must dos) are included in the Land Use Bylaw or Development Agreements while the Guidelines are tools (encouragement and guidance) used during the

application process to inform specific aspects or entire projects.

Council also recognizes that the Town is never finished and ever evolving and that design and architectural innovation must be accommodated and encouraged. The Design Guidelines and a Map outlining the areas of the Town where the guidelines are applicable, shall be included as schedules to the Town's Land Use Bylaw and aid in the implementation of this plan.

IT SHALL BE THE POLICY OF COUNCIL:

1. To guide the design and appearance of the Town's built form through mapping and guidelines included as Schedules of the Land Use Bylaw.
2. To use the Design Guidelines to inform Town built form projects (buildings, streets, sidewalks, etc), where applicable.
3. To use the Design Review Committee to complement the Design Guidelines when considering Development Applications, as outlined in the Land Use Bylaw.
4. To recognize that the Design Guidelines are used to inform development processes and that they are not prescriptive in nature. Mandatory requirements shall be defined in the Land Use Bylaw and Development Agreements.
5. To regard the Heritage Property Bylaw as the principal means of control for substantial alteration, demolition and other matters under the Heritage Property Act, to registered heritage buildings, streetscapes and areas.
6. To incorporate special provisions in the Land Use Bylaw for the lands where the Design Guidelines are applicable,

including how and when the Guidelines and Design Review Committee will be used for different development consideration mechanisms.

7. To require that all developments located within a designated Design Guidelines Area respond to the design principles and guidelines of the Design Guidelines and that the Design Review Committee and a Design Guidelines Checklist be used in certain circumstances, as defined in the Land Use Bylaw and Design Guidelines (Schedules of the Land Use Bylaw).
8. To encourage the preservation of the cultural and built heritage of Wolfville through cooperation with the Wolfville Historical Society and other similar heritage organizations that contribute to a sustainable built heritage in Wolfville.
9. To maintain Town owned registered heritage buildings in a state of good repair and carry out any alterations in a manner which enhances the historic character of the Town.
10. To encourage and enable the use of solar panels and alternative forms of energy for both existing and new buildings located within a Design Guidelines Area.
11. To allow the Development Officer to permit proposals in a Design Guidelines Area, as-of-right or by Site Plan Approval, where the Development Officer is satisfied that the proposed development is consistent with the intent of the this Municipal Planning Strategy and the requirements of the Land Use Bylaw. In considering proposals, the

Development Officer may seek the advice and opinion of the Town's Design Review Committee concerning matters defined in the Design Guidelines, including but not limited to the following matters:

- a. the proposal's response to the design review checklist;
 - b. aspects where a range of design options may meet the overall intent of the MPS and Land Use Bylaw or where site conditions require some form of negotiation or discretion be applied by the Development Officer in a Site Plan approval situation;
 - c. the proposal's architectural design and its compatibility with heritage architectural styles found in the neighbourhood;
 - d. the compatibility of the proposal with any abutting registered heritage properties in terms of its height, bulk, and scale;
 - e. the location of any proposed parking areas and how well they are screened from neighbouring properties and from the street; and
 - f. the suitability of any signage, lighting, fencing, landscaping or other elements.
12. To require the submission of detailed elevation drawings, a site plan, and any other information that the Development Officer and other Town officials deem necessary to evaluate a development proposal in a Design Guidelines Area.

11.4.5 SITE PLAN APPROVAL

Site Plan Approval is a development control tool in the Municipal Government Act that enables a municipality, through its planning process, to negotiate certain site specific items with a developer as a condition of receiving a development permit. The Site Plan Approval process is a way of dealing with site related issues in a less formal way than the typical development agreement process. This is a new process for Wolfville brought into effect through this Municipal Planning Strategy review.

As a planning tool, the Municipal Government Act s.231(4) describes the aspects of an application that can be determined through Site Plan Approval. Council will support the use of Site Plan Approval through the policies of this plan.

The Site Plan Approval process will be used as a tool in Wolfville for applications of a certain scale, to provide greater transparency and clarity for the public and streamline the application process for municipal staff and applicants. In addition, while Council still supports the use of development agreements for significant development applications, the triggers for where and when they are required have been revised in this Plan, in effort to create a better development consideration process where requirements and expectations are clear for all stakeholders. The MGA enables an appeal process, to Council, in applications considered by Site Plan.

IT SHALL BE A POLICY OF COUNCIL:

1. To assess various land uses, in all designations and zones, through the Site Plan Approval process and to create clear requirements for the use of this process in the Land Use Bylaw.
2. To require written undertaking from the landowner, on agreement to the approval of the Plan, prior to issuance of a Development or Building Permit.
3. To require future land owners who wish to change the land use approved under Site Plan Approval, to revisit the original negotiated site-plan.
4. To inform the applicant early in the process that a Site Plan Approval is specific to the property and will continue to apply in the event the property is sold unless discharged by Council.
5. To establish site plan notification, approval and appeal pursuant to the requirements of the Municipal Government Act and policies of this MPS.
6. To enable the Design Review Committee to be used by the Development Officer when considering applications by Site Plan Approval.

11.5 Subdivision Bylaw and Lot Access

The Municipal Government Act and the Provincial Subdivision Regulations set out the basic requirements for information that must be shown on plans of subdivision as well as the essential procedures which must be followed in reviewing and in approving plans of subdivision. The subdivision of land is closely related to the provision of streets and services as well

as the orderly progression of land development. Within this framework, the Subdivision Bylaw deals with matters such as lot size, access to lots, the extension of sewer and water lines to any new lots, and the provision of recreation space. The Subdivision Bylaw must be closely linked to the Land Use Bylaw, especially in the matter of lot size and access to lots.

IT SHALL BE THE POLICY OF COUNCIL:

1. To adopt a Subdivision Bylaw, setting out specific requirements to implement this Municipal Planning Strategy.
2. To specify in the Land Use Bylaw minimum lot areas and frontages required for new lots created by subdivision within the various zones established by the Land Use Bylaw.
3. To provide that all new lots created by subdivision shall satisfy the requirements for lot area and lot frontage as specified in the Land Use Bylaw, with the following exceptions:
 - a. lots created by the consolidation of two (2) or more lots;
 - b. lots which, having been expanded or made larger, but are nonetheless below the minimum requirements;
 - c. lots created around a main building, where the main building was built or placed on the land prior to April 16, 1987, the date of validation of real property transactions under the Municipal Government Act, and where the lot is serviced with central sewer and has a minimum lot frontage of 6 metres (20ft) on a public street;
 - d. two lots created where the lot dimensions and area are not less than 90 percent of the required minimum in accordance with section 279 of the Municipal Government Act.
 - e. new lots created through a Development Agreement process.
4. To specify in the Land Use Bylaw the uses of land and the location of structures permitted upon lots in the various zones and enable those uses and structures on lots which do not meet the minimum requirements for lot area or lot frontage or both, provided:
 - a. such undersized lots were created prior to April 16, 1987, the date of validation of real property transactions under the Municipal Government Act; or
 - b. such undersized lots were created after 16 April, 1987, the date of validation of real property transactions under the Municipal Government Act, by any instrument to which the Municipal Government Act does not apply.
5. That prior to approval of a Final Plan of Subdivision, the subdivider fulfill the open space dedication requirements in accordance with MPS Section 6: Parks and Open Space.
6. To work toward including in the Subdivision Bylaw 'Municipal Specifications' that outline requirements for the construction of sanitary sewers, storm water drainage, and water supply systems to service new lots created by the subdivision of land.
7. To work toward including provisions for the establishment of infrastructure charges, pursuant to Section 274 of the Municipal Government Act.
8. To enable Flag lots in accordance with the Land Use Bylaw.
9. To enable variation to lot frontages, as outlined in the Land Use and Subdivision Bylaws.

11.6 General Development

11.6.1 NON-CONFORMING STANDARDS

Non-conforming standards address how to treat land use, structures and other features of development that were legal when established, but are no longer permitted under current Land Use Bylaw regulations. These standards allow for certain

“grandfathering” of nonconformities, while prohibiting or strictly limiting the expansion, renovation or reconstruction of the development in an effort to protect the overall interests of the community.

IT SHALL BE THE POLICY OF COUNCIL:

1. To regulate non-conforming structures or uses in accordance with Sections 238 – 241 of the Municipal Government Act.
2. To prohibit the recommencement on a non-conforming development that has been discontinued for a period of one (1) year or more.
3. To consider expansion, alteration, or extension of non-conforming uses or structures only by Development Agreement.

11.6.2 VARIANCE

Section 235 of the Municipal Government Act allows the Development Officer to grant variances in circumstances where a proposed development may not be able to meet the requirements of the Town’s Land Use Bylaw or a development agreement. A variance is a relaxation of certain zoning

requirements where such variance will not be contrary to the public interest and where, owing to conditions peculiar to the property and not the result of the actions of the applicant, a literal enforcement of the zoning requirement would result in unnecessary and undue hardship.

IT SHALL BE THE POLICY OF COUNCIL:

1. To allow the Development Officer to grant variances in accordance with Sections 235-236 of the Municipal Government Act and the Land Use Bylaw.
2. To allow the Development Officer to grant variances to sidewalk cafe provisions established in the Land Use Bylaw.
3. To allow the Development Officer to grant variances for lot standards for Non-Market Housing.

11.6.3 ACCESSORY BUILDINGS AND STRUCTURES

An accessory structure is a structure or building that is normally incidental, subordinate, secondary or ancillary to the main use, structure or building and is exclusively devoted to that main use, structure or building and located on the same property. Accessory structures may include but are not limited to storage buildings, warehouses, garages, pools, sheds, mechanical buildings, workshops, and gazebos. Town Council recognizes the demand and need for various accessory structures in town, but also recognizes the need to limit their

impact on adjacent properties, particularly in residential neighbourhoods.

In an effort to promote the use of affordable housing alternatives, and to promote the concept of “aging in place”, Council shall permit the use of accessory buildings as dwelling units in residential neighbourhoods where the zoning so provides. Accessory structures may also be used for small-scale business uses permitted, where the zoning provides (restricted in the R-1 zone).

IT SHALL BE THE POLICY OF COUNCIL:

1. To regulate the use, size, number and location of accessory structures in the Land Use Bylaw.
2. To enable in certain zones, the use of an accessory building as a dwelling or other compatible use such as home based businesses or studios.

11.6.4 LANDSCAPING AND FENCING

Sensitive landscaping can reduce the negative impact of new development on nearby properties and can improve the living environment such as preventing erosion, controlling dust, screening unattractive elements, or managing other development issues. Fencing is valuable for a variety of

purposes including privacy, screening, security and aesthetics. Council recognizes that landscaping and fencing can also produce negative impacts if left unregulated. As a result, Council has established policies to manage these issues.

IT SHALL BE THE POLICY OF COUNCIL:

1. To require landscaping in all development subject to site plan approval and development agreement, in order to enhance or preserve the character of the areas, to provide screening and buffers, or to reduce conflict with surrounding land uses.
2. To use the Design Guidelines for landscaping and fencing requirements for development located in a Design Guidelines Area.

3. To encourage the use of native plant species and plants selected for their water efficiency in all landscaping proposals.
4. To limit the area of impermeable surfaces in all residential development permitted as-of-right in order to reduce heat-island effect and to promote the natural infiltration of stormwater, in accordance with the Land Use Bylaw.
5. To encourage the development of green roofs as part of all commercial, industrial and multi-unit residential development, in accordance with the Land Use Bylaw.

6. To regulate the height and location of fencing in all zones, in accordance with the Land Use Bylaw.
7. To require fencing for the screening of storage areas that contain unsightly materials, in accordance with the Land Use Bylaw.
8. To require fencing in any development permitted by site plan approval and development agreement, where Council believes fencing will help preserve the character of the area, provide necessary screening, or help to reduce potential conflicts with surrounding land uses.

11.6.5 OUTDOOR LIGHTING

Outdoor lighting, useful for safety and security reasons, can create a nuisance if it is improperly directed. Excessive lighting can also contribute to light pollution and increased energy use. Where lighting is proposed in a development,

consideration will be given to the type and location of lighting and its proper installation to avoid interference with nearby properties.

IT SHALL BE THE POLICY OF COUNCIL:

1. To regulate the type and location of outdoor lighting in all new developments in order to minimize the energy used

by lighting fixtures and reduce the impact lighting creates on properties in the surrounding area.

11.6.6 SIGNAGE

Signs and public displays of advertising are a component of many types of development. Signs have the potential for creating public nuisance and hazards. Signs are also a subject of frequent debate because they raise concerns about appearance, maintenance standards and compatibility with

nearby architecture and neighbourhood character. This Municipal Planning Strategy intends to regulate signage to help ensure traffic safety and to promote positive community aesthetics.

IT SHALL BE THE POLICY OF COUNCIL:

1. To prohibit signs that create hazards to traffic or pedestrians, constitute a public nuisance, or are

incompatible with the character of Wolfville, in accordance with the Land Use Bylaw and Design Guidelines.

2. To regulate the height, maximum area, illumination and location of signs throughout the town in order to minimize hazards and nuisances, in accordance with the Land Use Bylaw and Design Guidelines.

3. To control the impact of signs on the landscape, streetscape and skyline; and ensure that the nature, size and location of signs permitted in the various zones are appropriate and compatible with the character of the areas within those zones, in accordance with the Land Use Bylaw.

11.6.7 TEMPORARY DEVELOPMENT

A variety of temporary uses and structures will be permitted in all zones when operated on a temporary basis. Examples range from model suites associated with development projects, large tents, markets, fairs, festivals and more. The

types of temporary uses permitted shall vary according to zone type, to ensure that existing residential neighbourhoods are not adversely impacted.

IT SHALL BE THE POLICY OF COUNCIL:

1. To regulate temporary uses and structures in accordance with the Land Use Bylaw.

11.6.8 OUTDOOR STORAGE + DISPLAY

Outdoor display of goods for sale and the outdoor storage of raw materials, supplies and equipment can be unsightly and create nuisance and hazards. In order to reduce the impact of

this type of land use, controls will be placed on outdoor storage and display.

IT SHALL BE THE POLICY OF COUNCIL:

1. To regulate the outdoor display and storage of goods and materials in all residential and commercial areas, in

accordance with the zoning provisions and the abutting zone requirements of the Land Use Bylaw.

11.6.9 TELECOMMUNICATIONS INFRASTRUCTURE

Industry Canada is the federal licensing authority for all forms of telecommunication facilities. Health Canada provides advice on safe levels of exposure to radio frequency fields to Industry Canada. Industry Canada requires that all radio stations/communications towers are operated within the guidelines established by Health Canada. The Federal Government does recognize that municipalities may have an interest in the location of antenna structures and has therefore instituted a policy to allow for community review and

comment. The intent is that municipal concerns may be taken into consideration when determining whether to issue a license or any conditions that may be imposed.

In an effort to inform citizens and landowners of any such proposed infrastructure within town limits, upon notification of intent of the proponent to construct such utilities, the Town will notify all adjacent landowners and advise them of the proposal details, the proponent, and the Federal agency responsible for regulating the industry.

IT SHALL BE THE POLICY OF COUNCIL:

1. To notify landowners within 305m (1,000ft) of the location of a proposed communications facility with the following information:
 - a. The intent of the proponent; and
 - b. The relevant details of the proposal; and
 - c. The contact information for the Federal authorities responsible for regulating the proposed infrastructure; and
 - d. The procedure involved in processing such applications; and
 - e. Details on how the general public can influence the decision making process.

11.7 Public Engagement

A Municipal Planning Strategy is the articulation of a community's values and principles regarding future growth and development. Council commits to ensuring that the MPS is informed and guided by the process where the public is engaged in meaningful and effective ways.

Council adopted a Public Participation Program and has designed a development approval process that provides for clear expectations and outcomes. Emerging technologies are allowing for even more engagement of the public in new and different ways, all with the goal of informed and progressive

development policies that will allow Wolfville to grow and prosper toward an agreed upon vision for the future.

The Municipal Government Act identifies various types of development control techniques available to municipalities to regulate and plan for future development. The MGA recognizes that community engagement is required for the approval of policies that guide specific regulations. Council also recognizes that public engagement is essential in effective and transparent decision making and that public education and awareness of policy and development decisions is important.

IT SHALL BE THE POLICY OF COUNCIL:

1. To follow notification requirements and process outlined in the Public Participation Program policy (610-006), effective 2020-06-30.
2. To adhere to all public engagement (e.g. development notification and other planning requirements) as outlined in the Municipal Government Act and establish clear processes that enable additional public input, where appropriate.
3. To consider a Public Information Meeting as an initial step in Development Agreement and Amendment applications to provide an early opportunity to identify issues and outline policy and process for all stakeholders.
4. To strive for plain language in documents to ensure process is understood by all stakeholders.



PART 12

MONITORING and REPORTING

12.0 Evaluating the Planning Strategy

As the town continues to grow and change over time, it is necessary to regularly review the Town's growth strategy to ensure that it continues to reflect the priorities and objectives of the community and Council. To monitor and evaluate progress, priorities within this Plan should be broken down by metrics to track progress and measure success while also revealing possible constraints to the goals of the Town. While some goals and metrics may be more influenced by external forces, only by monitoring the progress of this Plan and adapting can we evaluate this planning strategy effectively.

To do this, Council aims to “measure what we treasure” and focus on achieving the community goals we value.

IT SHALL BE THE POLICY OF COUNCIL:

1. To work on formalizing a more robust Measuring and Reporting framework for the Town, with a focus on Quality of Life.
2. To update and maintain a Housing Needs Assessment for the Town on a regular review cycle.
3. To update and maintain a Community Profile for the Town on a regular review cycle.
4. To review the Municipal Planning Strategy in accordance with Section 11 Implementation, and:
 - a. Consider a housekeeping review 1 year after implementing this strategy.



PART 12

MONITORING and REPORTING

Disclaimer

This is a Draft document for review.

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PART 13

SCHEDULES

Disclaimer

This is a Draft document for review.

1. FUTURE LAND USE MAP

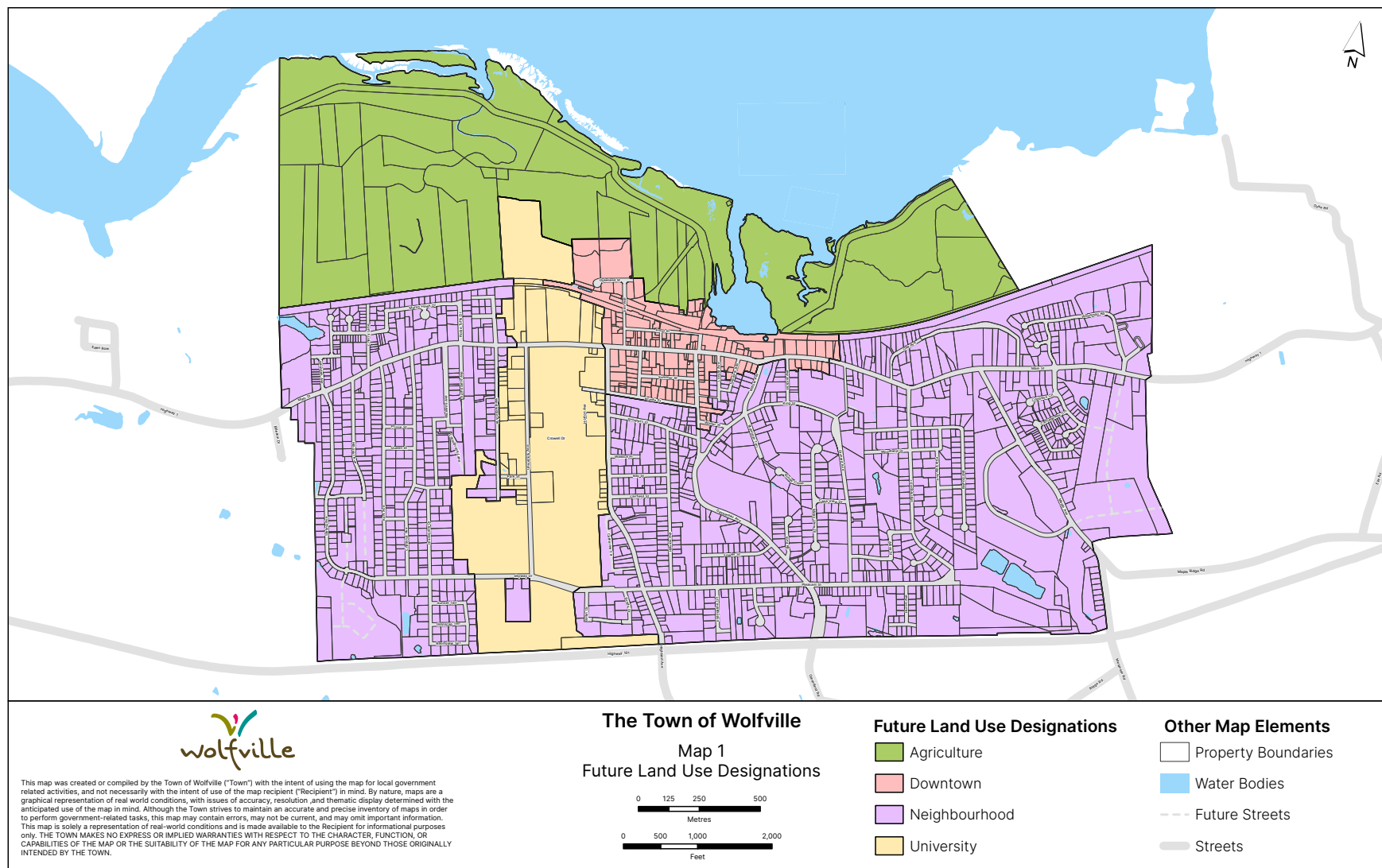


Figure S1.1—Future Land Use Map

2. STREET CLASSIFICATIONS

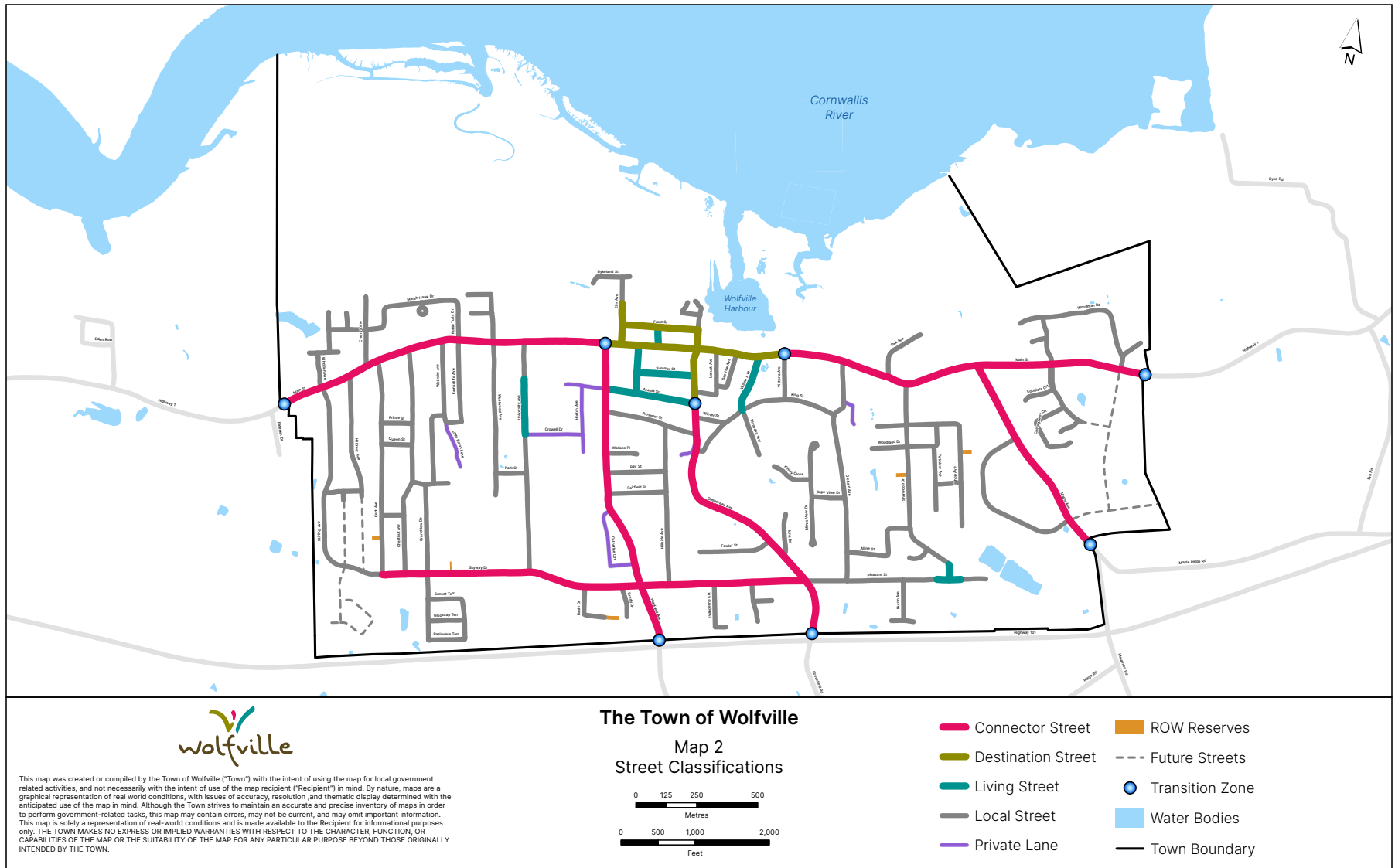


Figure S2.1—Street Classifications Map

3. FUTURE PARKS AND TRAILS MAP

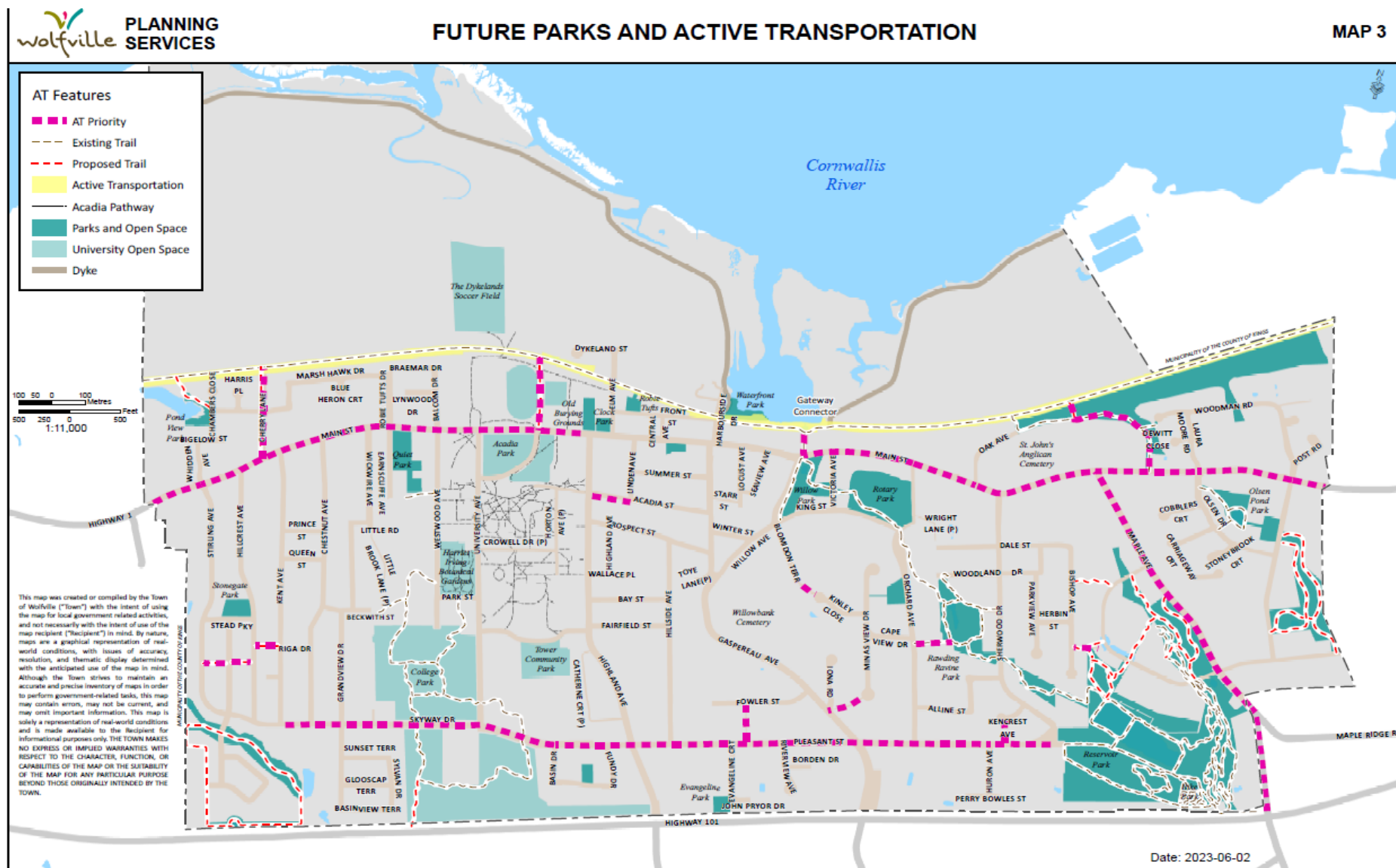
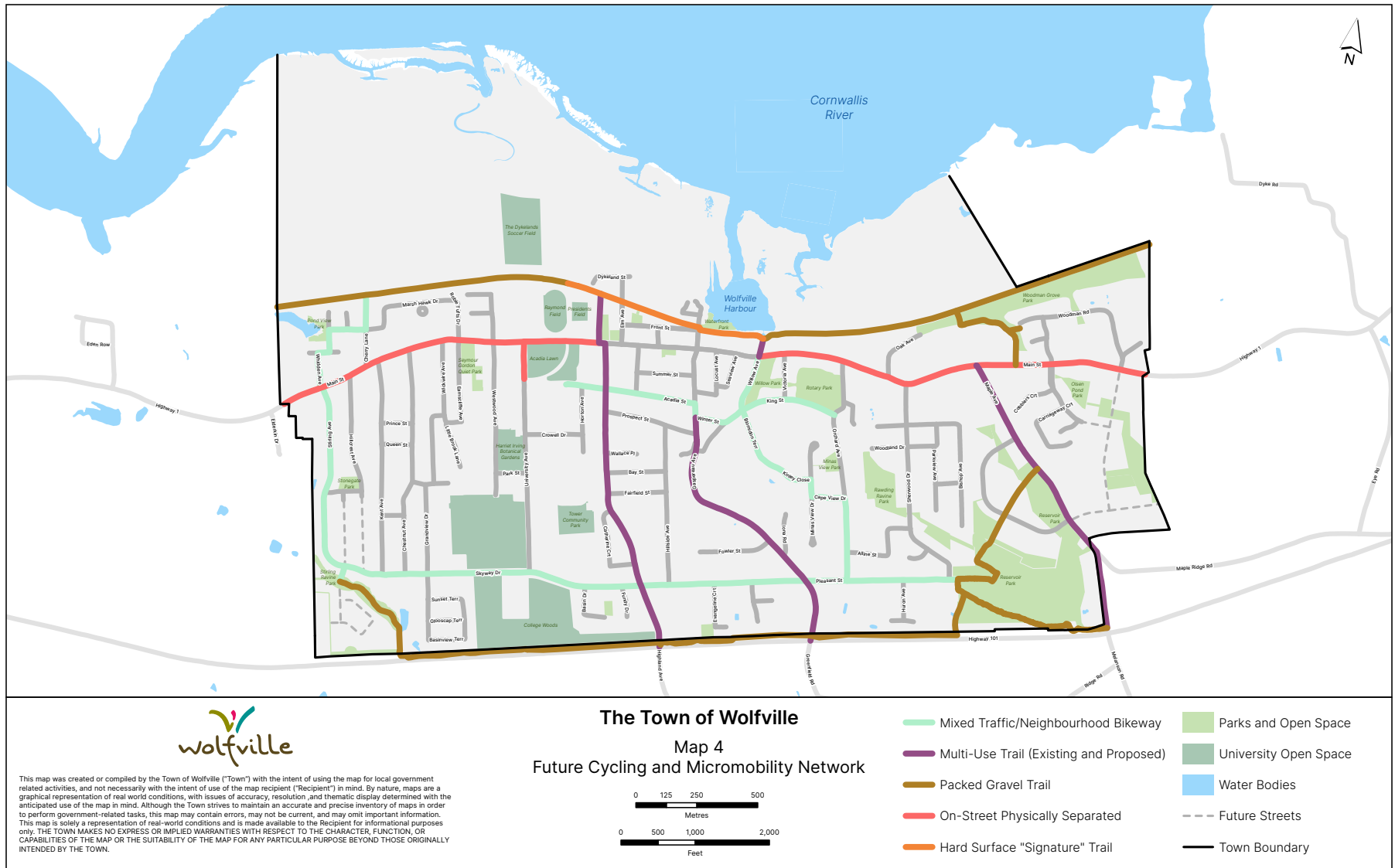
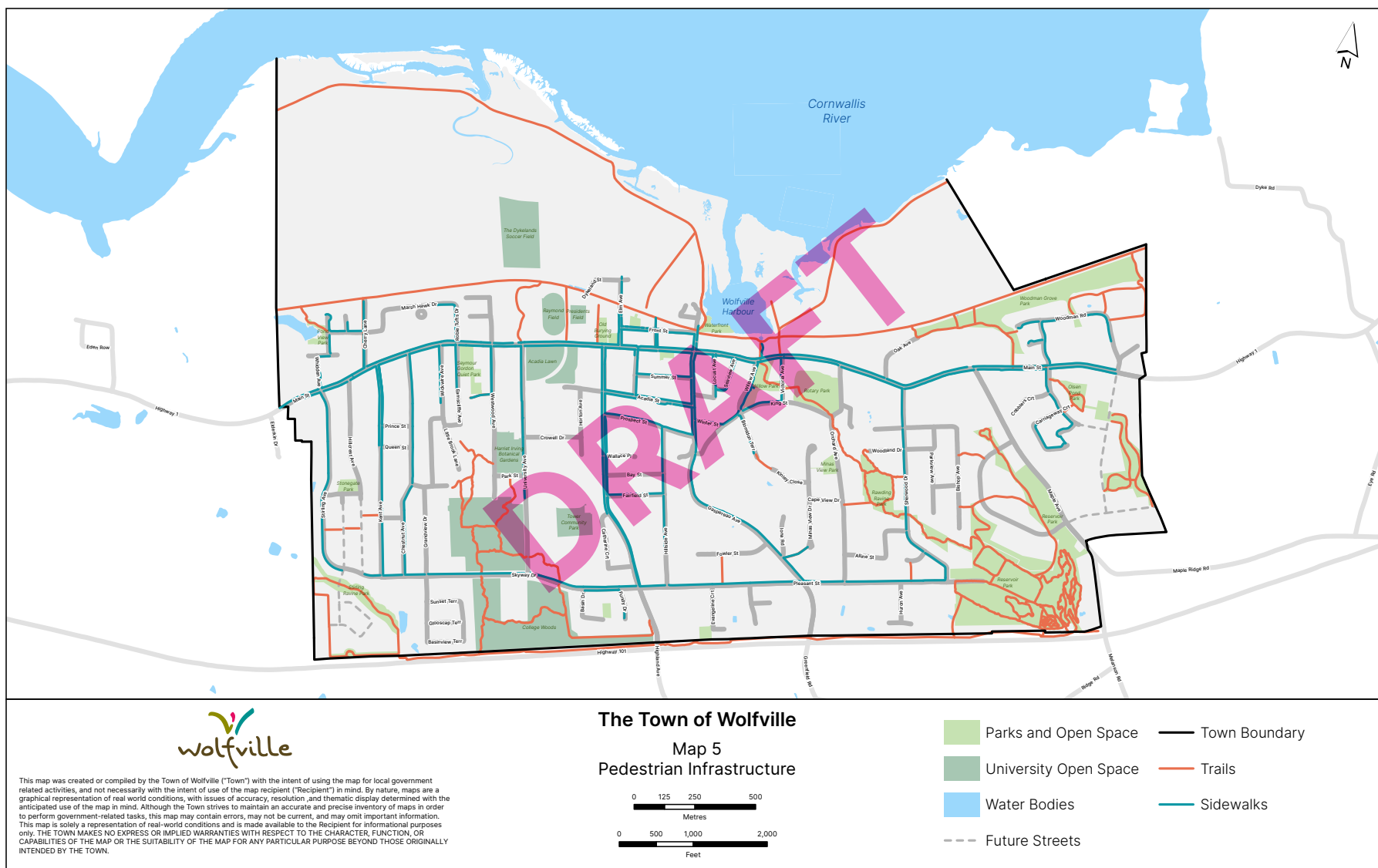


Figure S3.1—Cycling and Micromobility Network Map

4. FUTURE CYCLING AND MICROMOBILITY MAP



5. PEDESTRIAN INFRASTRUCTURE



6. PARK and OPEN SPACE CLASSIFICATIONS

The provision of both public and private recreation facilities within the Town of Wolfville offers an extensive choice of recreational opportunities in a small Town environment. These facilities range from arenas and an indoor pool to small playgrounds, family parks and sports fields. An extensive trail system throughout Town interconnects residential neighbourhoods with the central downtown area and the University campus and provides residents with a variety of opportunities to choose active transportation options over the automobile.

Council recognizes the importance of providing physically active alternatives to the automobile and will endeavor to expand the Town's existing trail system and work with Acadia University to link Town trails with University trails.

Parks and open spaces also play a vital role in protecting wildlife habitat, maintaining biodiversity and hydrological balance, filtering pollutants and providing public access to natural areas. Public parks and open spaces often provide a buffer between the natural and the built environment as well as protection for environmentally sensitive areas.

The Town of Wolfville has adopted a park classification system to assist with the planning of its parks and open space resources. The classification system forms the foundation for building a Town-wide community park system. It provides guidance on the anticipated use of the park or the functional requirements that should be reflected in the park's planning and design.

Park classification	Features
Community parks	• Passive recreation space (public) • Major indoor and outdoor facilities with amenities (lighting, seating, shaded areas, etc.) • Accessible to pedestrian and/or bicycle circulation system
Neighbourhood parks	• Located in/near residential neighbourhoods • Amenities to serve non-organized recreational facilities (playgrounds, multi-purpose court, etc) • Accessible to pedestrian and/or bicycle trail system
Non-municipal active sports park	• Sports fields (private) • Community programming
Linear parks	• Recreation trails (public), • Recreation lands (public) • Accessible to pedestrian and/or bicycle trail system
Special open space areas	• Passive lands (public) • Community purpose areas (heritage sites and squares, civic gardens, etc) • Educational/limited recreational activities
Non-recreational open space area	• Storm retention ponds • Vegetated areas

Table S6.1—Park classification systems

7. FLOOD RISK MITIGATION PLAN SUMMARY

CONTEXT

The flood lines shown on our Development Constraints Map (Schedule B of the Land Use By-law) have been developed by CBCL Consulting Engineers. CBCL is continuing with updates to their studies of flood risk that will be summarized here when complete.



Town *of* Wolfville

LAND USE BY-LAW

2025 AMENDMENTS

DRAFT OCT 9, 2025

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PART 1 Title and Purpose

This Bylaw shall be known and may be cited as the “Land Use Bylaw” of the Town of Wolfville.

The purpose of this Bylaw is to implement and enforce the policies and intent of the Municipal Planning Strategy (MPS) for the Town of Wolfville, in accordance with the provisions of Section 219 of the Municipal Government Act, by regulating the use of land and the location, size, spacing, character and use of buildings and structures within the Town of Wolfville.

1.1 EFFECTIVE DATE

This Bylaw shall take effect when approved by the Provincial Minister of Municipal Affairs, whereupon any applicable previous Land Use Bylaw is repealed.



FIGURE 1.1 MPS Report Cover

Community Priorities

EP	Economic Prosperity
SE	Social Equity
CA	Climate Action
LU	Land Use and Design

The land use bylaw actions our Community Priorities and move us toward our shared future.

PART 2 Administration

2.1 ADMINISTRATION

This Bylaw shall be administered by the Development Officer appointed by Council.

- (1) **Acting Development Officer:** In the absence or incapacity of the Development Officer, the acting Development Officer appointed by Council shall act in the Development Officer's stead.

2.2 DEVELOPMENT PERMIT

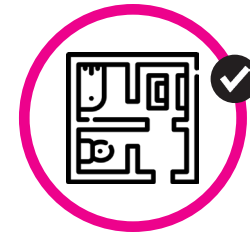
- (1) Unless otherwise stated in this Bylaw, no person shall use any land or erect, construct, alter or use any building or structure in the Town of Wolfville without first obtaining a development permit from the Development Officer.
- (2) The Development Officer shall only issue a development permit in conformance with this Bylaw or a duly executed and approved development agreement.
- (3) A development permit, whether issued before or after the coming into force of this Bylaw, shall expire within 12 months from the date issued if the development has not commenced.
- (4) Any decision of the Development Officer to refuse to issue a development permit shall be given by written notice served by registered mail.
- (5) No developer shall deviate, or allow deviations to be made, from the description of the proposed

development which is contained in the development permit unless approved by the Development Officer.

- (6) The Development Officer may revoke a development permit where information provided on the application is found to be inaccurate. Any revocation shall be sent by registered mail and become effective five (5) days after the mailing date, or immediately on personal service of the notification to the applicant.

2.3 NO DEVELOPMENT PERMIT REQUIRED

- (1) Notwithstanding Part 2.2, no development permit shall be required for:
 - (a) A development that involves the interior renovation of a building that will not change the footprint of the building or increase its volume and/or floor area, will not add more dwelling units or bedrooms, or will not involve a change in use of the building; or
 - (b) A development that involves an exterior renovation of a building façade that is located outside of a Design Guidelines Area (See Schedule G); or
 - (c) Temporary uses or temporary signs as specified in section 4.35; or
 - (d) The following or similar miscellaneous minor structures provided they meet the



requirements of *Section 4.6* Corner Vision Triangle:

- i) Clothes poles
- ii) Flag Poles
- iii) Garden Trellises
- iv) Cold Frames
- v) Fences
- vi) Retaining Walls
- vii) Children's Play Structures
- viii) Doghouses
- ix) Monuments
- x) Interpretive Displays
- xi) Barrier Free Ramps
- xii) Roof-Mounted Solar Panels

2.4 APPLICATION REQUIRED

Every developer wishing to obtain a development permit must submit an application for such development permit to the Development Officer in the form prescribed from time to time by Council.

- (1) **Contents of Application:** Every application for a development permit or site plan approval shall be signed by the owner of the lot or, with the written authorization of the owner, the agent of the owner of the lot, and shall be accompanied by a plan drawn to an appropriate scale and showing:
 - (a) The true shape and dimension of the lot to be used or upon which it is proposed to construct or locate any building or structure; and

- (b) The proposed use, location, height, and dimensions of any building or structure for which the permit is applied and the information shall include measurements of the lot frontage, front, side and rear yards; and
- (c) The location of every building or structure already constructed, or partly constructed on such lot and the location of every building or structure existing upon abutting lots; and
- (d) The proposed location and dimensions of parking areas, parking spaces, loading spaces, driveways, curbs, landscaping; and
- (e) Other such information as may be necessary to determine whether the proposed development conforms with the requirements of this Bylaw.
- (f) The Development Officer may waive the requirement to supply the above noted information if he/she deems it is not pertinent to the application.

- (2) **Survey of Lands:** Where the Development Officer is unable to determine whether the proposed development conforms to this Bylaw and other Bylaws and Regulations in force which affects the proposed development, the Development Officer may require that the plans submitted under *Section 2.3* be based upon an actual survey by a licensed Nova Scotia Land Surveyor.



2.5 APPLICATION FEES

Every application for a development permit, subdivision, variance, site plan approval, Municipal Planning Strategy and/or Land Use Bylaw amendment(s) and development agreement shall be subject to the fees as established by Council from time to time, and in accordance with Policy 140-015, Municipal Fees.

2.6 RIGHT OF ENTRY

The Council, or any of its duly authorized employees, shall have the right to enter at all reasonable times into or upon any property within the area to which this Land Use Bylaw applies for the purpose of any inspection necessary in connection with the administration of the Land Use Bylaw.

2.7 AS-OF-RIGHT & CONDITIONAL PROCESS

The Municipal Government Act (MGA) enables as-of-right and conditional approval processes. A generalized as-of-right and conditional approval process is summarized in *Figure 2.1 and Figure 2.2*.

2.8 SITE PLAN APPROVAL PROCESS

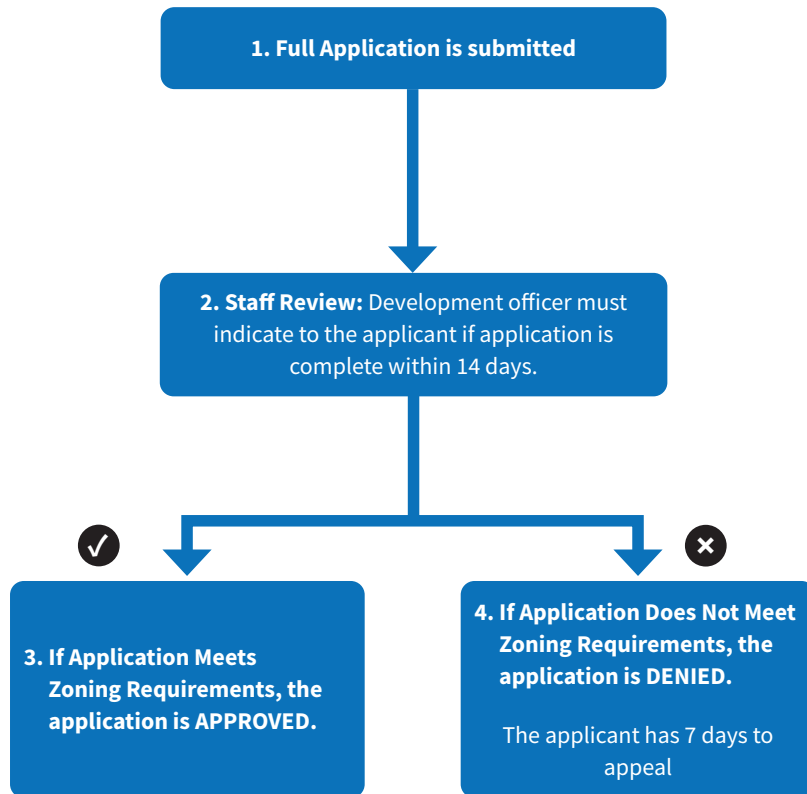
The Municipal Government Act (MGA) enables site plan approval as a streamlined approval process. A generalized Site Plan Approval process is summarized in *Figure 2.3*.

2.9 DEVELOPMENT AGREEMENT PROCESS

The Municipal Government Act (MGA) enables Development Agreements as a council approval process. A generalized Development Agreement process is summarized in *Figure 2.4*.

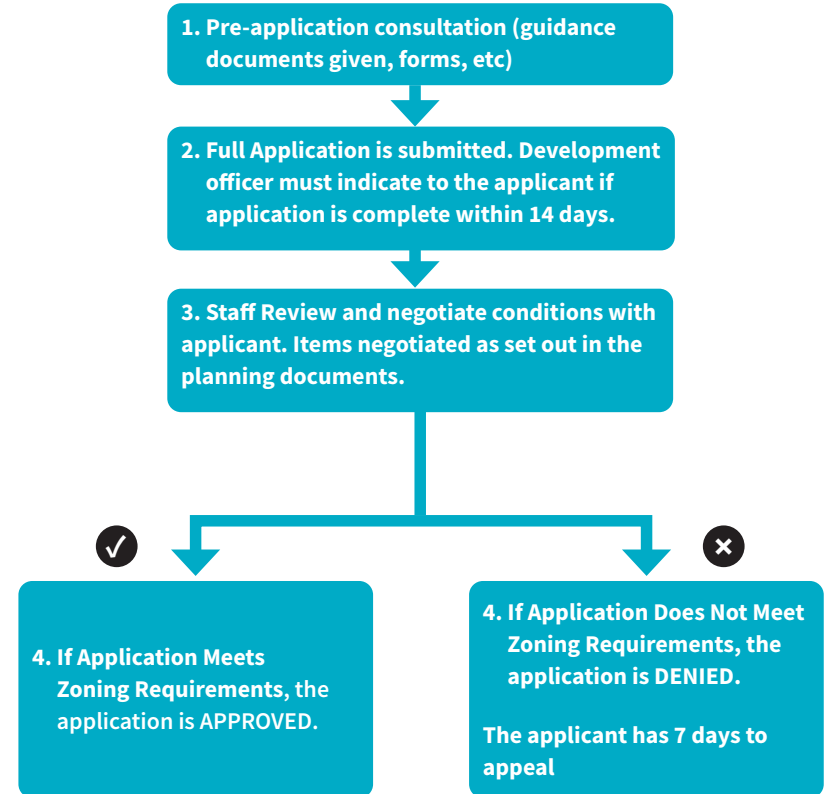
Simple Projects

Fig. 2.1
Permitted Approval Process

E.g. Single-detached dwellings, semi-detached dwellings, duplex dwellings, townhomes, additions to existing dwellings.

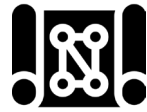
Fig. 2.2 Permitted with Conditions Approval Process

E.g. internal and detached accessory dwelling units, garden clusters, etc.

More Complex Projects

**Fig. 2.3
Site Plan Approval Process**



1. Pre-application consultation (guidance documents given, forms, etc)

2. Full Site plan Application (Section 2.10) is submitted.

3. Staff Review and negotiate a site plan with applicant. Items negotiated as set out in the planning documents.

4. Design Review (If necessary)



5. If Application Meets Zoning Requirements, the application is **APPROVED**.

Approval notification is posted to properties within 30m.,

14-day Appeal period is observed



6. If Application does not meet Zoning Requirements:

Design Review Working Group may be asked to review the specific requirement against the Design Guidelines for approval; or

The proposal may require a Development Agreement process; or

The application may be denied

The applicant has 7 days to appeal

E.g. multi-unit developments, university developments, commercial developments,

**Fig. 2.4 Development
Agreement Approval Process**



1. Pre-application consultation (guidance documents given, forms, etc)

2. Full Application (Section 2.10) is submitted with notification as per the Town's public participation program policy.

3. Public Information Meeting

4. Design Review (If necessary)

5. PAC Report & Review

6. Committee of the Whole

7. Public Hearing



8. If DA is Supported, the application is **APPROVED** subject to appeal period.



9. If DA is not supported: The application is denied

E.g. master planned developments, developments that require more major variances, etc.

2.10 SITE PLAN & DEVELOPMENT AGREEMENT APPROVAL APPLICATION

All applications for site plan approval or development agreement approval shall be accompanied by:

- (1) A Town of Wolfville **Site Plan Application Form** or **Development Agreement Application Form** and associated fee.
- (2) A recent (no more than 5 years old) topographic survey by a surveying engineer. The survey must reflect existing topographic and boundary conditions.
- (3) A site plan and conceptual grading plan drawn to scale and stamped by a landscape architect or engineer showing all buildings, entries, driveways, parking areas, walkways, stormwater features, fences, walls, landscape beds, trees and other important site information and noting conformance with the zone standards in each zone.
- (4) Stormwater Management Plan by a civil engineer in accordance with the *Town of Wolfville Stormwater Management Design Guidelines*.
- (5) Architectural plans prepared by a licensed Architect for applications located in a Design Guidelines Area (as shown on Schedule C) or plans prepared by a relevant professional or technician in areas outside a Design Guidelines Area, indicating compliance with the specific design requirements of the zones in this Bylaw including;
 - (a) Height, streetwall height and setbacks pursuant to each zone;
 - (b) Building elevations and articulation on all sides including signage and lighting.
 - (c) Notes on building material intentions
 - (d) Floor plans for every level
 - (e) A perspective rendering of the building showing context and/or a 3D model.
- (6) A servicing schematic prepared by a civil engineer to demonstrate that every building can be properly serviced.
- (7) For buildings over 8 units or 1,200 square metres, a Traffic Impact Statement (TIS) by a transportation engineer.
- (8) Any other information the Development Officer may require to determine compliance with the zone requirements.
- (9) The Development Officer may waive the requirement to supply the above noted information if he/she deems it is not pertinent to the application.

Submission Requirements

FIGURE 2.5 Site Plan & Development Agreement Checklist

- ☒ Application Form
- ☒ Application Fee
- ☒ Topographic Survey
- ☒ Site/Grading/Landscape Plan
- ☒ Stormwater Management Plan
- ☒ Architectural Plans
- ☒ Civil Servicing Schematic
- ☒ Traffic Impact Statement (if required)
- ☒ Any additional material Development Officer or building official may require

2.11 VARIANCES

- (1) A variance from the minimum requirements of this Bylaw may be granted by the Development Officer, in accordance with the Municipal Government Act, for any of the following requirements provided they meet the intent of the MPS:
 - (a) The lot frontage or lot area, or both;
 - (b) Size or other requirements relating to yards;
 - (c) Number of parking spaces and loading spaces required;
 - (d) Ground area and height of a structure;
 - (e) Floor area occupied by a home-based business; and
 - (f) Height or area of a sign.
- (2) For site plan approval, where the Design Review Working Group is involved, the Development Officer may vary any of the prescriptive dimensional requirements of this Bylaw by up to 10 percent of the requirements to allow some flexibility to accommodate physical anomalies of a site, so long as the intent of the particular requirement is not compromised.

2.12 COST OF NOTICE OF VARIANCE

Where a variance from the requirements of the Bylaw has been granted or refused, the Development Officer shall give notice to the persons entitled, and in the manner prescribed by Section 236 of the Municipal Government Act. This notice

is to be served by ordinary mail and the Town of Wolfville may recover from the applicant the cost of giving notice.

2.13 VIOLATIONS

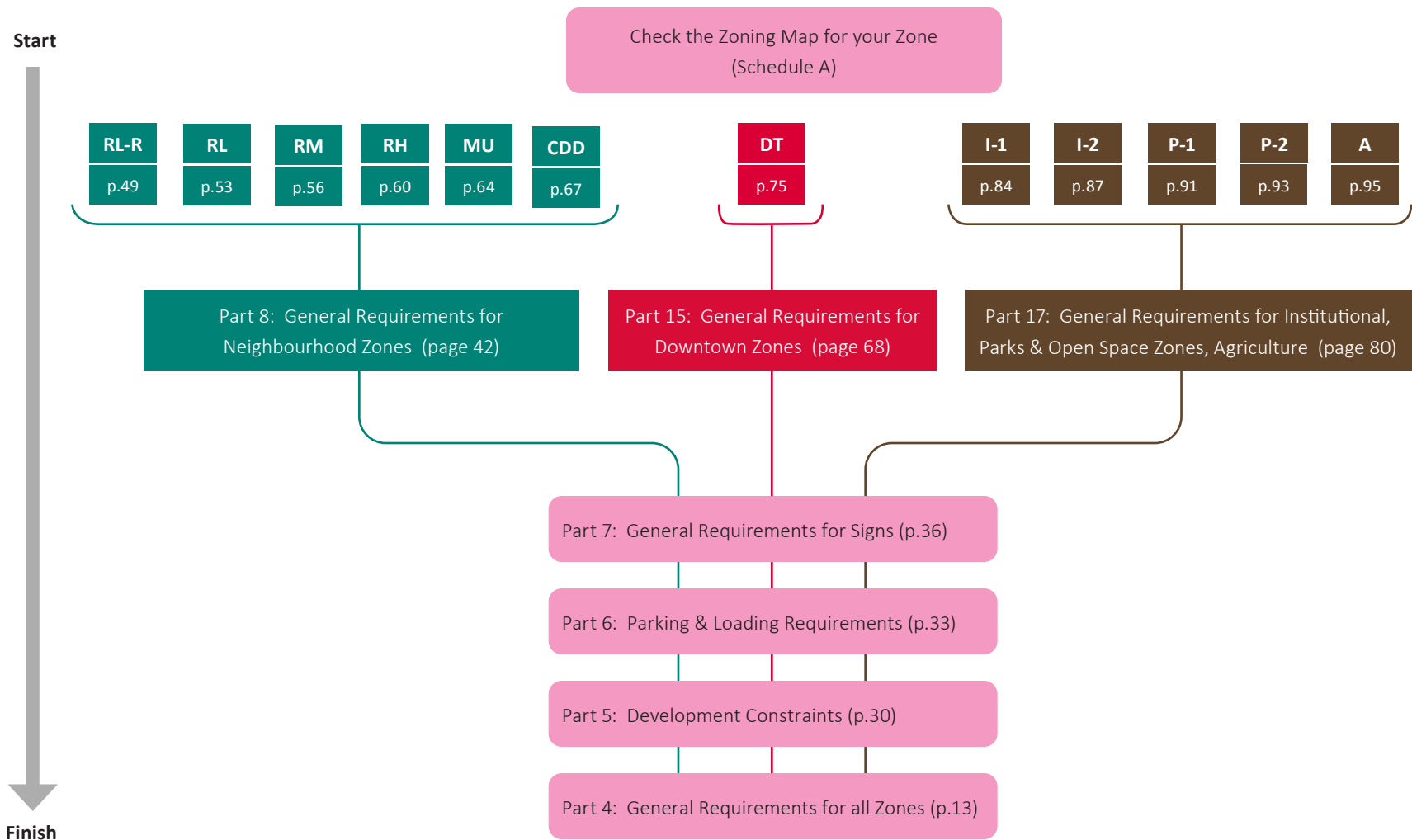
In the event of any alleged contravention of the provisions of this Land Use Bylaw, the Town of Wolfville may take action as outlined in Sections 266 and 267 of the Municipal Government Act.

2.14 NOTICE OF AMENDMENT OR DEVELOPMENT AGREEMENT

Where the Council has given notice of its intention to adopt an amendment to Schedule A, the Zoning Map, which is not general in scope, but which is in direct response to a site specific development proposal, or has given notice of its intention either to enter into a development agreement or amend a development agreement, the Council shall post a sign on the property describing the proposed amendment or development agreement and serve notice upon affected property owners whose property lies within 100 metres of the property which is the subject of the proposed amendment or agreement.

Such notice shall:

- (1) Set forth a synopsis of the proposed amendment or agreement; and
- (2) Set the date, time and place for the public hearing on the amendment or agreement; and
- (3) Be served by ordinary mail at least 14 days before the date of the public hearing.



2.15 HOW TO USE THIS LAND USE BYLAW

The following steps will confirm that your proposed development meets the requirements of this land use bylaw:

- (1) All development shall be generally consistent with the intent of the Town's Municipal Planning Strategy
- (2) Confirm your zone using the zoning map (see Schedule A)
- (3) Confirm your proposed development meets the zone requirements for the zone that the property falls within.
- (4) Confirm your proposed development meets the General Requirements for each zone.
- (5) Confirm your proposed development meets the
 - (a) General Sign Requirements (page 36);
 - (b) Parking & Loading Requirements (page 33);
 - (c) Development Constraints Requirements (page 30); and
 - (d) General Requirements (page 13) for all zones.
- (6) If the development is located in a Design Review Area (Schedule C), the Downtown & Neighbourhood Design Guidelines (Schedule F) apply.

PART 3 Interpretation of Maps and Text

3.1 ZONES

For the purpose of this Bylaw, the Town of Wolfville is divided into the following zones, the boundaries of which are shown on the attached Schedule A. Schedule A should be interpreted using both Schedule B (Development Constraints) and Schedule C (Design Guidelines Areas). The zones on Schedule A are referred to as follows:

Residential Zones

RL-R	Low Density Residential - Restricted
RL	Low Density Residential
RM	Medium Density Residential
RH	High Density Residential
MU	Mixed-Use
CDD	Comprehensive Development District

Commercial Zones

DT	Downtown
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Institutional Zones

I-1	Institutional General
I-2	Institutional University

Green Space Zones

P-1	Park & Open Space
P-2	Park & Open Space - University
A	Agriculture
AT	Active Transportation Corridor

3.2 ZONING MAP

Schedule A, attached hereto, is titled the “Zoning Map” and forms part of this Bylaw. Schedule A should be interpreted with both Schedules B and C.

3.3 ZONING BOUNDARIES

The extent and boundaries of all zones are shown on Schedule A as attached hereto.

3.4 INTERPRETATION OF ZONE BOUNDARIES

Boundaries between zones, as shown on Schedule A, shall be determined as follows:

- (1) Where a zone boundary is indicated as approximately following lot line, the boundary shall follow such lot lines; and
- (2) Where a street, highway, railroad or rail right-of-way, electrical transmission line right-of-way, watercourse or other linear feature is included on Schedule A, it shall, unless otherwise indicated, be included in the zone in which it occurs; and
- (3) Where a street, highway, railroad or rail right-of-way, electrical transmission line right-of-way, watercourse or other linear feature is included on Schedule A and serves as a boundary between two (2) or more zones, a line midway on such right-of-way, watercourse or other linear feature, and extending in the general direction of the long division thereof, shall be considered the boundary

between zones unless specifically indicated otherwise; and

- (4) Where the zone boundary is indicated as following the shoreline of a river or bay, the boundary shall follow the actual shoreline, including wharves and piers; and
- (5) Where none of the above provisions apply, and where appropriate, the zone boundary shall be scaled from the attached Schedule A.

3.5 ZONES NOT ON ZONING MAP

Schedule A of this Bylaw may be amended in conformity with the Municipal Planning Strategy to utilize any zone in this Bylaw, regardless of whether or not such zone had previously appeared on Schedule A.

3.6 CERTAIN WORDS

In this Bylaw, words used in the present tense include the future; words in the singular include the plural; words in the plural include the singular; and the word “used” includes “arranged”, “designed or intended to be used”; the word “shall” is mandatory and not permissive.

All other words carry their customary meaning except for those defined in Part 28: Definitions.

3.7 STANDARDS OF MEASUREMENT

The metric system of measurement is used throughout this Bylaw and in all cases represents the required standard. Any

reference to imperial measurements is approximate and for convenience only.

3.8 ILLUSTRATIVE GRAPHICS

This bylaw includes graphics to illustrate requirements, concepts, definitions and other content. Additional graphical content may be added over time to enhance the accessibility of this document for all users.

3.9 OVERLAYS

Important regulatory overlays are also included as Schedule B (Development Constraints) and Schedule C (Downtown and Neighbourhood Design Guidelines).

- (1) Schedule B – Development Constraints
- (2) Schedule C – Downtown and Neighbourhood Design Guidelines

3.10 OTHER SCHEDULES

Other Schedules form part of this bylaw and are used in certain circumstances, as indicated in the Municipal Planning Strategy or Land Use Bylaw. Other Schedules include:

Schedule D - Comprehensive Development District (CDD) Zone Checklist
Schedule E - Environmental Study Requirements
Schedule F – Downtown and Neighbourhood Design Guidelines

PART 4 General Requirements For All Zones

4.1 ACCESSORY BUILDINGS (GARAGES AND SHEDS) AND USES

- (1) Accessory uses and accessory buildings and structures are permitted in any zone within the Town of Wolfville but shall not:
 - (a) Be used for human habitation except where a dwelling is a permitted accessory use;
 - (b) Be located within the front yard or the flankage yard of a lot;
 - (c) Have a side yard less than that required for the main building (does not apply in DT zone);
 - (d) Be built closer to the rear lot line than 2 metres; an accessory building less than 16 square metres in building area may have a minimum rear or side yard of 1 metre (does not apply in DT zone);
 - (e) Be built closer than 4.5 metres to the rear streetline of a through lot;
 - (f) Be considered an accessory building if it is attached to the main building;
 - (g) Be considered an accessory building if located completely underground;
 - (h) Be greater than 8 metres in height;
 - (i) Be larger than the main building in volume.
- (2) For detached garages, the building shall:
 - (a) Be designed to be compatible with the architectural style of the main building(s)
 - (b) Be set back no less than 2 metres from the main building facade facing a street

- (3) Accessory buildings located within the Design Guidelines Area and greater than 50 square metres in floor area shall have a public facade compatible with the architectural style of the main building on the lot with respect to criteria listed in part 4.7(5).
- (4) Where this Bylaw provides that any land may be used or a building or structure may be erected or used for a purpose, the purpose includes any use Accessory thereof.
- (5) Attached garages are not permitted to extend beyond the front building facade and must be set back no less than 60 centimetres from either the front facade, bays or veranda of the building. This includes buildings with flankage yards.

4.2 ACCESSORY DWELLING UNITS - DETACHED

- (1) Notwithstanding 4.1.1(a), a detached accessory dwelling (AUD) shall be permitted in any neighbourhood zone in conjunction with an existing dwelling unit, provided:
 - (a) The total floor area of the detached accessory dwelling unit does not exceed 50% of the total floor area of the existing dwelling;
 - (b) The detached accessory dwelling unit contains no more than two bedrooms;
 - (c) A minimum separation distance of 2 metres is maintained between the detached accessory dwelling unit and the existing dwelling;

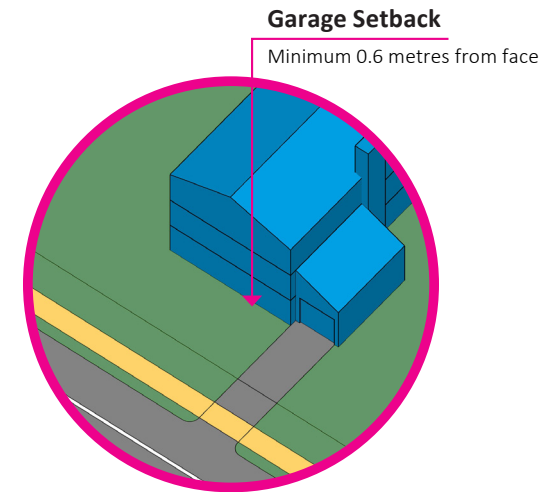


FIGURE 4.1 Attached Garage setback from building face

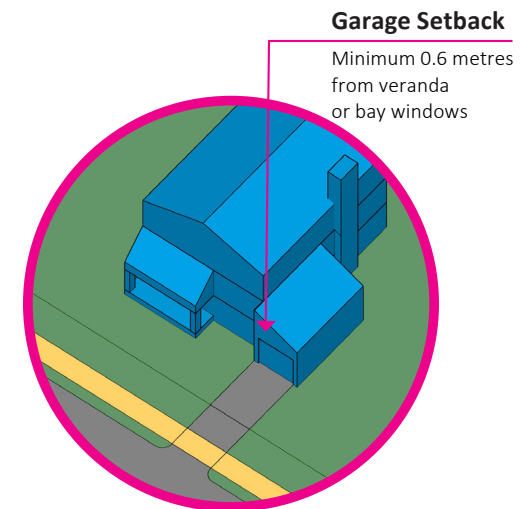


FIGURE 4.2 Attached Garage setback from veranda

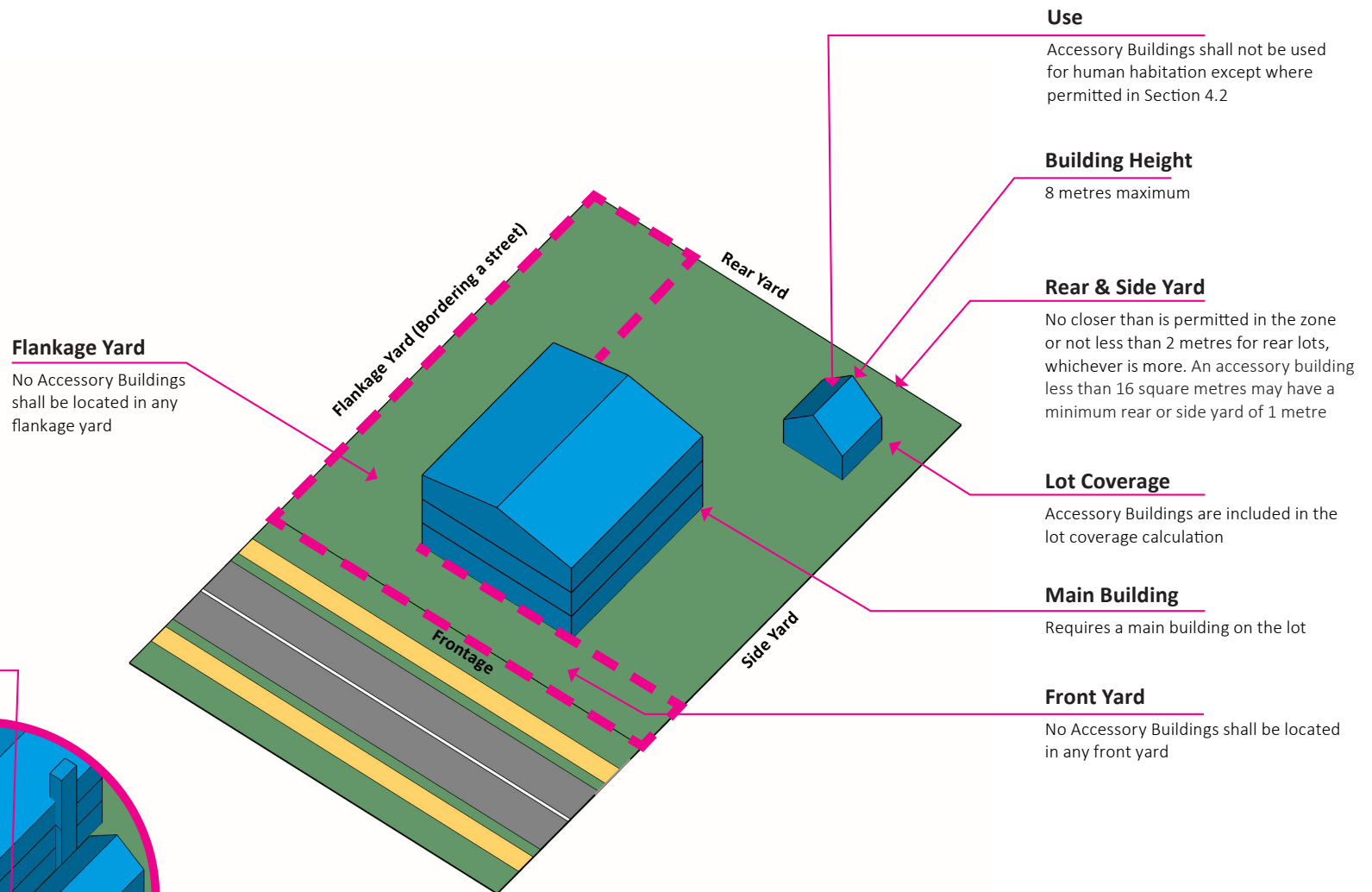


FIGURE 4.3 Accessory Buildings

Detached Garage Setback

2 metres from main building

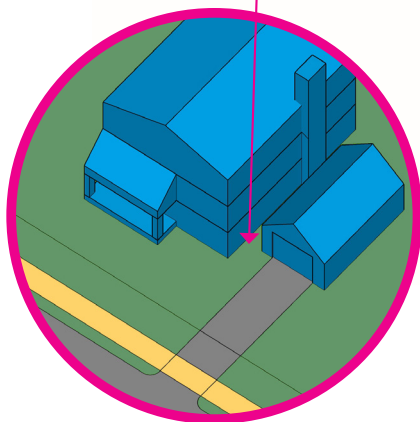


FIGURE 4.4 Detached Garage setback from main building

- (d) The detached accessory dwelling unit meets other relevant zone standards (hard surface coverage, minimum side yard setbacks, maximum building height, etc).

4.3 BUILDING OR STRUCTURE TO BE MOVED

No person shall move a building or structure within or into the area covered by this Bylaw without obtaining a development permit from the Development Officer.

4.4 COMMUNITY GARDEN

Where permitted, a community garden must comply with the following provisions:

- (1) It may not be a commercial garden;
- (2) Produce grown is for the personal use and consumption of the individual(s) working the community garden; and
- (3) No vehicle, equipment, building or structure of any sort, including an arbour or other such supporting structure is permitted within 1.5 metres of a public street.

4.5 CORNER VISION TRIANGLE

- (1) On corner lots, except in the Downtown Zone, a fence, sign, hedge, shrub, bush, or any other structure or vegetation shall not be erected or permitted to grow to a height greater than 50 centimetres above the grade of the streets that abut the lot within the corner vision triangle area included within the street lines for a distance of 5 metres from their point of intersection. See Figure 4.5.

4.6 DESIGN GUIDELINES AREAS – DOWNTOWN AND NEIGHBOURHOOD

- (1) Development located within the Downtown or Neighbourhood Design Guideline Areas, as shown in Figure 2.1 and Schedule C of the Land Use Bylaw, shall be subject to the design requirements as specified in the various zones of this Land Use Bylaw.
- (2) For development proposals considered by Site Plan or Development Agreement, the Development Officer may request a submission from the applicant on how the application is responding to the Downtown or Neighbourhood Design Guidelines.
- (3) For Development Agreement Applications, the Design Review Working Group and relevant design guidelines shall be utilized.
- (4) For Site Plan Applications, the Design Review Working Group and relevant design guidelines may be utilized.
- (5) Additions and alterations which constitute less than 25% of the total floor area of any main building or 10% of the total floor area of registered heritage properties; or accessory buildings greater than 50 square metres located within the Downtown and Neighbourhood Design Guideline Areas as shown on Schedule C of the Land Use Bylaw, shall have a public façade similar to the existing building with respect to the following at the discretion of the Development Officer:
 - (a) Architectural style;
 - (b) Building length to width ratio;
 - (c) Height;

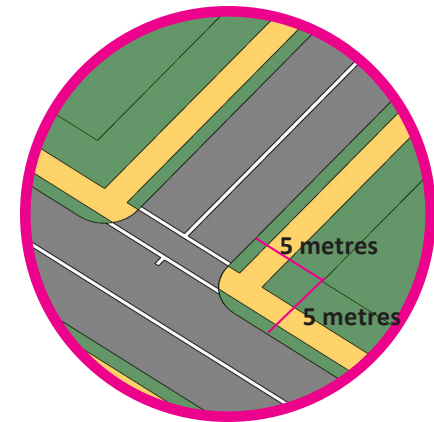


FIGURE 4.5 Corner Vision Triangle

- (d) Roof shape;
- (e) Appearance of exterior cladding and roof materials;
- (f) Architectural details;
- (g) Shape and size of doors and windows;
- (h) Window area to wall area ratio.

4.7 DRIVE THROUGH FACILITIES

Drive through facilities shall be prohibited in all zones.

4.8 EXISTING BUILDINGS WITH NON-CONFORMING YARDS

Where a building has been erected on or before the effective date of the Bylaw on a lot which has less than the minimum yard or yards required by this Bylaw, the building may be enlarged, reconstructed, repaired, or renovated provided that:

- (1) The enlargement, reconstruction, repair, or renovation does not further reduce any yard that does not comply to this Bylaw.
- (2) All other applicable provisions of the Bylaw are satisfied.

4.9 EXISTING UNDERSIZED LOTS

- (1) Notwithstanding anything else in this Bylaw, an undersized lot described in a deed on or before the effective date of this Bylaw, having less than the minimum frontage or area required by this Bylaw, may be used for a purpose permitted in the zone in which the lot is located and a building may be erected provided that all other applicable provisions in this Bylaw are satisfied.

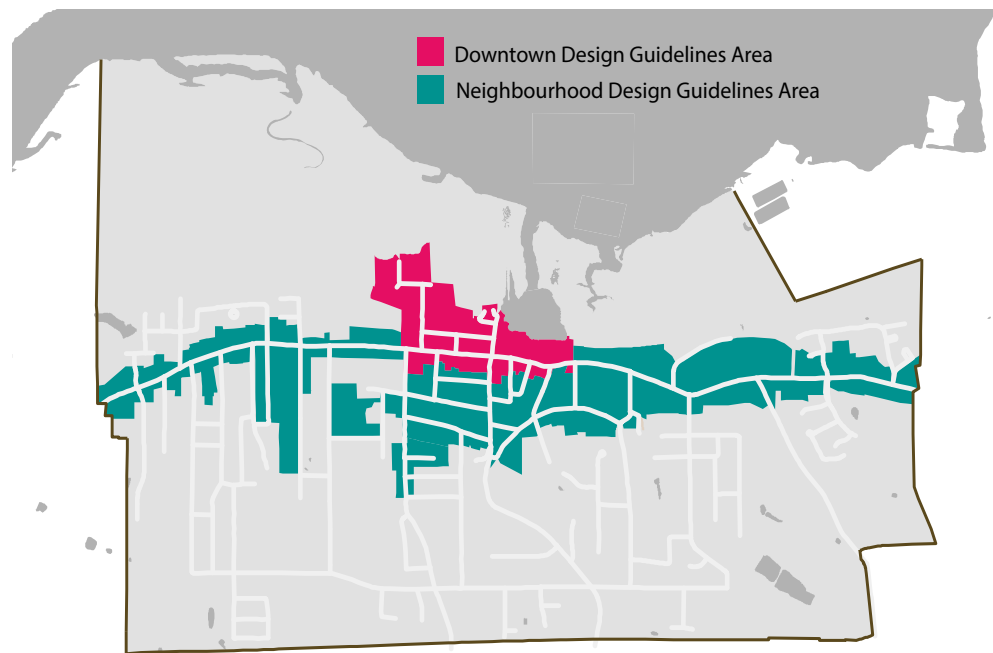


FIGURE 4.6 Wolfville Design Review Areas

- (2) For the purpose of this section, an undersized lot which has had its frontage or area increased yet still remains undersized shall be considered an existing undersized lot.

4.10 EXTERNAL STAIRCASES

External staircases shall not be permitted between the building and any street in any zone unless required as a result of site, life-safety or structural limitations.

4.11 FLAG LOTS

Flag lots will be permitted under the following conditions:

- (1) After subdivision, each lot meets the minimum lot area.
- (2) The flag lot has a minimum frontage of 6 metres.

- (3) All other provisions of this Bylaw can be met.

4.12 FRONTAGE ON A STREET

- (1) No development permit shall be issued for a lot unless the lot abuts and fronts upon a public street or a municipally approved private laneway or access easement.
- (2) A lot that has access to a street over a private right-of-way or private easement shall be deemed not to abut a street. A development permit may be issued where an access easement to an existing property without lot frontage on a street can be provided by a public authority.
- (3) Notwithstanding Section 4.12(1), a development permit may be issued for a lot abutting a future

TABLE 4.7 Permitted Projections & Encroachments

Structure	Projection Permitted In	Maximum Encroachment
Sills, belt courses, cornices, eaves, gutters, chimneys, pilasters, canopies or similar building elements	Any yard	0.6 metres (2 ft)
Window bays	Front, rear and flankage yard only	1 metre (3.28 ft) and a maximum width of 3 metres (9.8 ft)
Fire escapes and exterior staircases	Rear and side yards only	1.5 metres (5 ft)
Balconies – one or more stories above grade	Front, rear and flankage yards	2.5 metres (8.2 ft)
Decks	Front, rear and flankage yards only	2 metres (6.56 ft)
Open, roofed porches not exceeding one storey in height, uncovered terraces	Front, rear and flankage yards only	2 metres (6.56 ft) including eaves and cornices

street provided an approved tentative subdivision plan and servicing agreement is in place and all other applicable parts of this Bylaw are met.

- (4) Notwithstanding Section 4.12(1), an existing building on an existing lot lacking public road frontage may be altered, added to, renovated, replaced with a new structure, changed to another use permitted in the zone, or any or all of these, provided that all other applicable provisions of this Bylaw are satisfied.
- (5) All lots shall require a minimum width at the Street Line of 6 metres.
- (6) Section 4.12(1) shall not apply to lands designated as "Area C" on Schedule A1.

4.13 HABITATION OF VEHICLE BODIES

No automobile, truck, bus, coach or car body, recreational vehicle, with or without wheels, shall be used for human habitation within the area regulated by this Bylaw.

4.14 HEIGHT REQUIREMENTS

- (1) A 'storey' may not exceed 4.5 metres on the ground floor and 4 metres on any upper storeys except in certain conditions where exemptions may be considered by development agreement.
- (2) In calculating building height, basements are not counted as a storey provided they are below the

streetline grade.

- (3) Notwithstanding 4.14(1), on sloping streetline conditions, no more than 1 metres high of basement foundation may be exposed provided that accessible groundfloor entries are provided from the streetline.
- (4) If a development fronts on two streets, the lower streetline grade of the two streets will be used for the average grade calculation.
- (5) Height Exemptions: Certain roof features are exempt from the maximum height requirements to provide for greater architectural variety and address the needs of flat roof buildings which support amenity space or green roofs. Exceptions are shown in table 4.8. No additional habitable space can be created from any exemption.

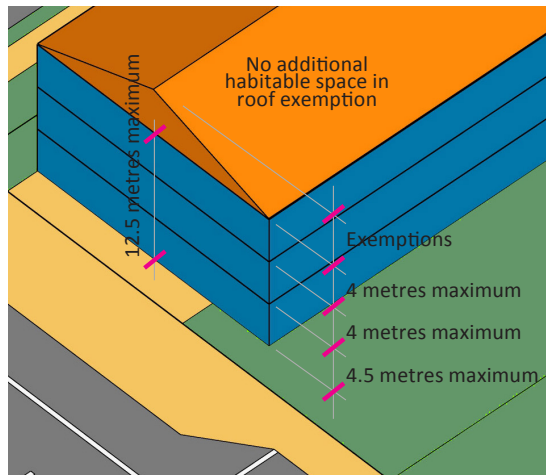


FIGURE 4.8 Building Height Maximums

TABLE 4.9 Height Exemptions

Feature	Exceedance Above Height	30% Coverage Restriction	Minimum Setback from the Street Frontage
Chimney	4 metres	Yes	-
Railing system	2 metres	Yes	-
Clock tower or bell tower	5 metres	Yes	-
Elevator enclosure	5.5 metres	Yes	3 metres
Misc minor structures (e.g. flagpoles)	Unlimited	Yes	-
Mechanical equipment enclosure	4.5 metres	Yes	3 metres
Landscaping including pergolas	3 metres	Yes	-
Parapet	2 metres	Yes	-
Cupola	4 metres	Yes	-
Solar collector	4.5 metres	No	-
Stair enclosure	4.5 metres	Yes	2 metres
Roof peaks that provide no additional habitable space	4.5 metres	No	-



FIGURE 4.10 Average Grade Considerations on sloping sites

4.15 HOME OCCUPATION - RESTRICTED

Nothing in this Bylaw shall prevent the use of any dwelling unit or building accessory to a dwelling unit in any zone for a limited home occupation (e.g. personal office, music and art instruction, tutoring, professional or personal services) as outlined in Table 8.1, subject to the following:

- (1) One appointment may be on the premises at any given time.
- (2) No signage shall be permitted.
- (3) No Employees, other than the resident are employed.
- (4) No more than 50 square metres is devoted to the non-residential use.

4.16 HOME BASED BUSINESS

In any zone that permits residential uses, except the Low Density Residential - Restricted (RL-R) zone, any dwelling unit or a building accessory to a dwelling unit may be used for a home based business provided that all of the following requirements are met:

- (1) No more than 3 people, including the owner(s) are employed by the business.
- (2) The business shall be located wholly within the dwelling unit or a building accessory to a dwelling unit.
- (3) The business use shall fall within one of the types of uses as outlined in Table 8.1 to 8.2.

- (4) Where a business use is not listed in Table 8.1 to 8.2, and the business is similar in nature, the permission of the use shall be at the discretion of the Development Officer, provided all other provisions of this Bylaw are met.
- (5) The dwelling is occupied as a residence by the home occupation owner and the external appearance of the dwelling is not changed by the non-residential use.
- (6) No more than 80 square metres is devoted to the non-residential use.
- (7) The use does not emit noise, smoke, odour, dust, toxic fumes, or light that would be a nuisance or is uncouth in a residential neighbourhood.
- (8) Not more than one additional off-street parking space (i.e. for customers or employees), other than those required for the dwelling, is provided for the home occupation use.
- (9) No advertising other than a business identification plate or sign which has a maximum sign area of 0.8 square metres, and which is a facial wall sign, projecting sign or ground sign.

- (10) In addition to the signage permitted in 4.16(9) one off premise sign shall be permitted for any Bed and Breakfast no located on Main Street, on the Town right-of-way such that every part of the sign is:
 - (a) A minimum of 0.3 metres distance from the back edge of the Town sidewalk, or
 - (b) In the absence of a Town sidewalk a minimum distance of 3 metres from the edge of the pavement,
 - (c) Placement of the sign must be approved by the Traffic Authority.
- (11) No open storage or outdoor display;
- (12) Not more than one commercial vehicle related to the home occupation use may be kept upon or operated from the lot.

4.17 HOME BASED COMMERCIAL

For all properties with frontage on Connector Streets (MPS Map 2) commercial uses are permitted up to a commercial floor area of 120 square metres and if the following requirements are met:

- (1) A minimum 1.5 metres wide hard surface pathway linking the front entry of the commercial use with the public right-of-way.
- (2) May require a landscape buffer and screening from abutting residential uses.

4.18 LICENSES, PERMITS AND COMPLIANCE WITH OTHER BYLAWS

- (1) Nothing in this Bylaw shall exempt any person from complying with the requirements of other Bylaws or regulations in force within the Town of Wolfville or from obtaining any license, permission, permit, authority or approval required thereunder.
- (2) Where the provisions in this Bylaw conflict with those of any other municipal or provincial requirements, the higher or more stringent provisions shall prevail.

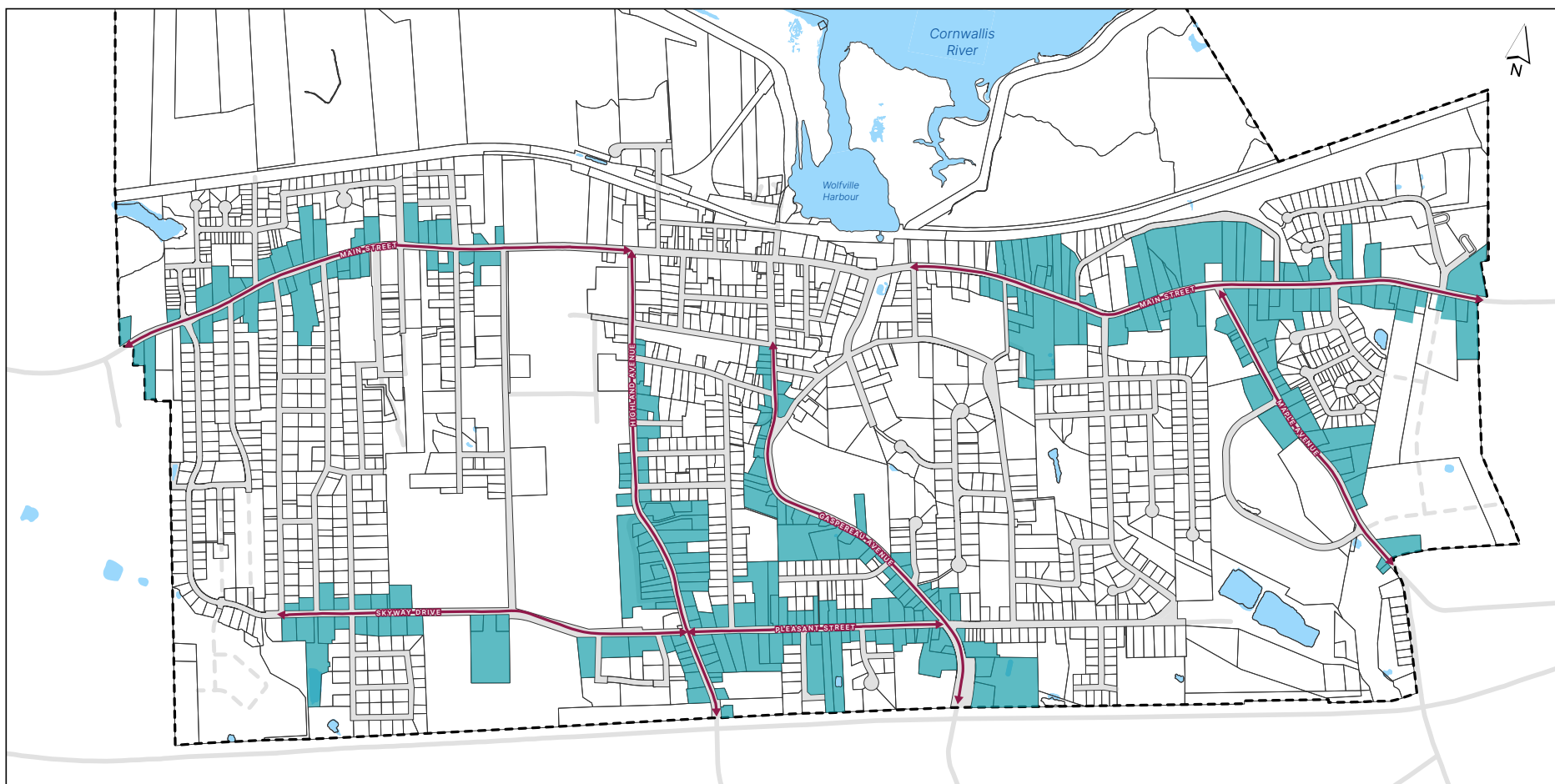
4.19 MULTIPLE USES

Where any lot or building is used for more than one main use, all provisions of this Bylaw relating to each use shall be satisfied. Where there is conflict, such as in the case of lot size or lot frontage, the most stringent provisions shall prevail.

4.20 NON-CONFORMING USES & STRUCTURES

Non-conforming uses shall be subject to **Sections 238-242** of the Municipal Government Act with the following exceptions:

- (1) a non-conforming use of land or a non-conforming use in a structure shall not be recommenced after it has been discontinued for continuous period of one year.
- (2) Expansion, alteration or extension of a non-conforming use or structure may be considered by

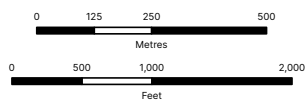


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The Town of Wolfville

Schedule A3

Neighbourhood Commercial Corridors



- Neighbourhood Commercial Corridors
- Neighbourhood Commercial Enabled
- Property Boundaries
- Town Boundary
- Water Bodies
- Streets

development agreement.

4.21 ONE MAIN BUILDING ON A LOT

No person shall erect or use more than one main building in the RL-R zone. More than one main building on a lot may be considered in all other zones provided all general requirements and zone specific requirements are met in the application.

4.22 OUTDOOR LIGHTING

Any outdoor lighting associated with a development shall minimize the impact on properties in the surrounding area by using techniques such as hooded streetlights or Dark Sky compliant standards.

4.23 OUTDOOR WATERSTOVES AND OUTDOOR WOOD FURNACES

Outdoor waterstoves or outdoor wood furnaces shall be prohibited in all zones.

4.24 PARKS AND PLAYGROUNDS

Public and private parks and public playgrounds shall be permitted in any zone.

4.25 PUBLIC USES

Municipal buildings, facilities and infrastructure shall be permitted in any zone and do not need to conform to zone requirements.

4.26 REAR YARD FOR A THROUGH LOT

Notwithstanding anything else in this Bylaw, the minimum rear yard for a through lot shall be not less than 7 metres, except in the DT zone (see Figure 4.4).

4.27 REGISTERED HERITAGE PROPERTIES

As per policy 4.9, any external alterations to a registered heritage property shall demonstrate consideration of the Nova Scotia Heritage Property Act under the guidance, direction and approval of the Town's Heritage Advisory Committee (HAC). The HAC will only be consulted

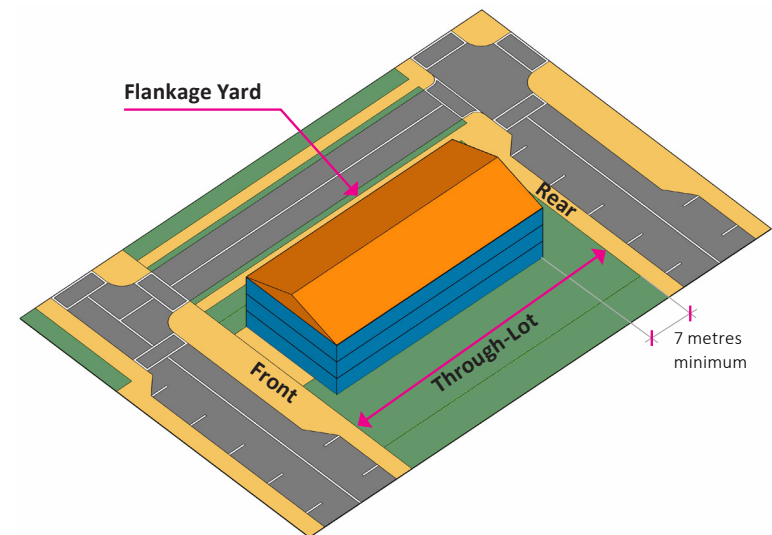


FIGURE 4.11 Through Lots

if the alterations are a change beyond the character defining elements of the designation.

As per Policy 3.2(5) of the Municipal Planning Strategy, any rezoning of a registered Heritage property, to any zone, shall be considered by Council in an effort to preserve the Town's built heritage.

4.28 RESTORATION TO A SAFE CONDITION

Nothing in this Bylaw shall prevent the strengthening or restoring to a safe condition of any building or structure, provided in the case of a non-conforming building or structure, the provisions of Sections of 238-242 of the Municipal Government Act shall prevail, subject to the exceptions noted in section 4.13.

4.29 SERVICE AND UTILITY RIGHT-OF-WAYS

Service and utility rights-of-way for the establishment and maintenance of overhead and underground utilities shall be permitted in any zone.

4.30 SHORT TERM RENTALS

The following special provisions shall apply to Short Term Rentals:

- (1) Short-term rentals shall only be permitted where proof of primary residence is provided to the Development Officer.
- (2) No more than three rental rooms are permitted.
- (3) Notwithstanding 4.29(2), a whole house may be

rented not more than 150 days per year.

- (4) In the case of a rental situation, written confirmation from the Landlord shall be provided.
- (5) A Fire and Life Safety Inspection shall be conducted on the premises before any Development Permit is issued.
- (6) Proof of any required licensing from the Province, or other regulatory bodies shall be provided to the Development Officer.
- (7) Proof of insurance shall be provided to the Development Officer.
- (8) 4.30 (1) through (7) do not apply to DT and MU zone. In these zones this use is considered a hotel use.
- (9) A fee shall be provided as per the Fees Policy.
- (10) Short term rentals (e.g. Bed and Breakfast) shall not be subject to the size restrictions specified in section 4.17.6.

4.31 SIDE YARD WAIVER

Notwithstanding anything else in this Bylaw, where buildings on adjacent lots share a common wall, the applicable side yard requirement will be zero along the common lot line.

4.32 SINGLE ROOM OCCUPANCY

- (1) Single Room Occupancies are a housing type where



FIGURE 4.38 Heritage Plaque

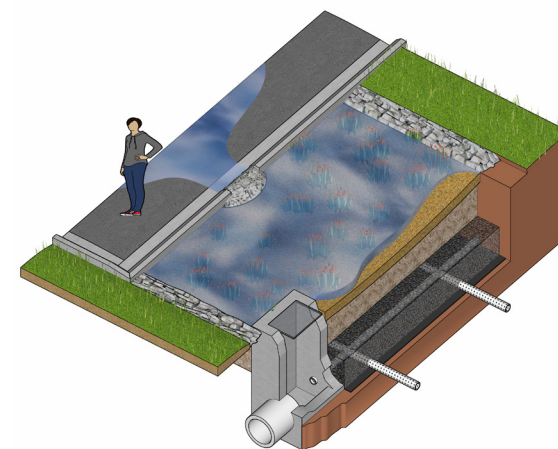


FIGURE 4.12 Typical Stormwater BMP for parking lot

one or two people are housed in single rooms (four or more), where tenants share bathrooms and a kitchen in a dwelling unit.

- (2) Single Room Occupancies do not include Short Term Rentals or single unit dwellings with up to 3 rental rooms.
- (3) Dwellings with four or more rooms used for single room occupancies are permitted in the RL, RM, RH, and MU zones, subject to the following:
 - (a) A Fire and Life Safety Inspection shall be conducted on the premises before any Development Permit is issued;
 - (b) The maximum number of single room occupancies shall correspond to the underlying zone:
 - i) RL - maximum 4 bedrooms
 - ii) RM - maximum 6 bedrooms
 - iii) RH - maximum 8 bedrooms
 - iv) MU - maximum 6 bedrooms
 - (c) Fee shall be provided as per the Town's **Fees Policy**;
 - (d) Existing single room occupancies may be subject to some or all of the requirements of this section, subject to context and the development history of the property.

4.33 STORMWATER MANAGEMENT

- (1) An application for a Development Permit for a new development that is not regulated by an existing

Development Agreement and Servicing Agreement or a renovation that changes the footprint of the building or lot drainage patterns shall include a Lot Grading Plan and Stormwater Management Plan in accordance with the *Town of Wolfville Stormwater Management Design Guidelines*.

- (2) The Lot Grading Plan and Stormwater Management Plan are to be prepared by a Professional Engineer.
- (3) No person shall occupy a building until Lot Grading record drawings confirming that the lot has been constructed in accordance with the Lot Grading Plan and Stormwater Management Plan have been submitted and accepted by the Town.
- (4) Additional hard surface beyond the zone requirements may be permitted (up to 10%) where low impact development measures are included.
- (5) Where Low Impact Development (LID) measures are included with the Stormwater Management Plan, the Owner shall provide a maintenance plan to the Town and an undertaking to perform maintenance activities in accordance with this maintenance plan. The Town reserves the right to audit performance and enforce maintenance as necessary to ensure the continuous function of the LID measures.
- (6) Notwithstanding Section 4.33.4, a building may be occupied prior to acceptance of Lot Grading record

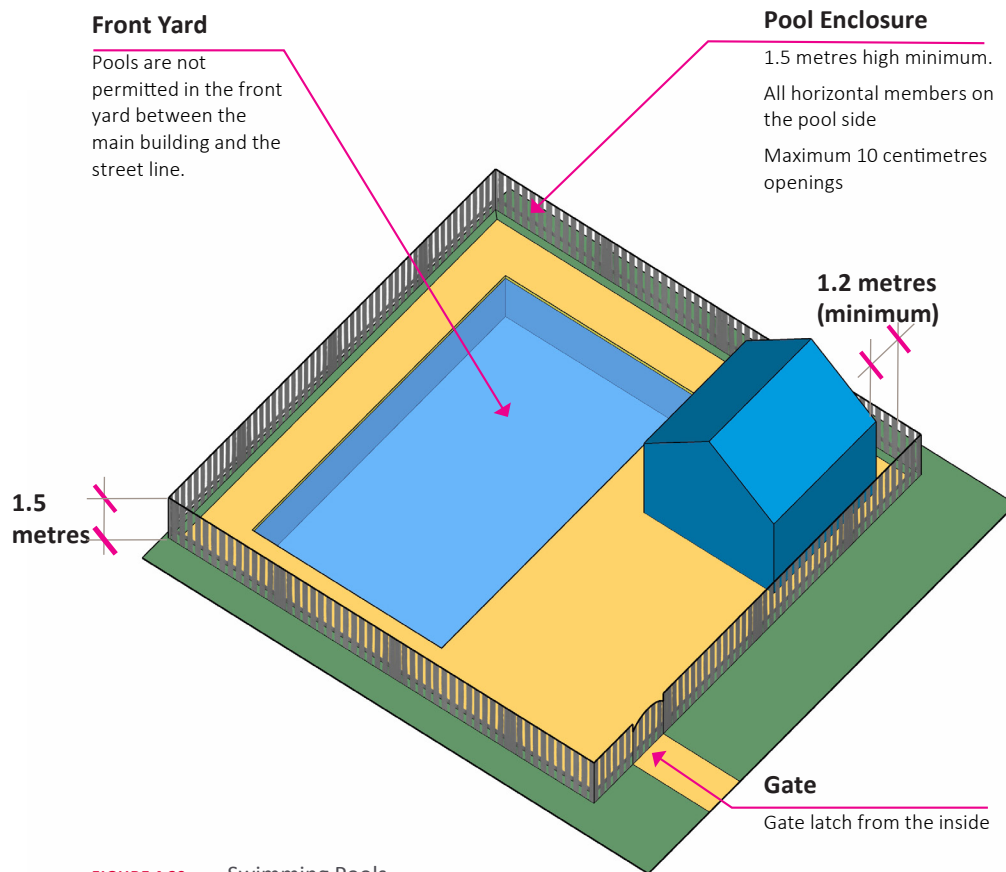


FIGURE 4.39 Swimming Pools

drawings by the Town where the following has been submitted to the Town:

- (a) A deficiency report prepared by a Nova Scotia Land Surveyor, a Landscape Architect or Professional Engineer, registered to practice in Nova Scotia, in a form acceptable to the Town, setting forth details of the work to be completed, and
- (b) An undertaking by the Owner stating that:
 - i) The uncompleted work required by the deficiency report shall be completed within a time frame acceptable to the Town, and
 - ii) The Lot Grading record drawings shall be submitted within a period of nine (9) months.
- (7) No stormwater drains, roof drains, surface drainage, sump pumps or foundation drains shall be permitted to discharge to the sanitary sewer collection system.
- (8) Infiltration practices, including soakaways, trenches, and bioretention cells, shall be designed for a maximum acceptable drain down time of 72 hours.

4.34 SWIMMING POOLS

Swimming pools shall be permitted in all zones subject to the following requirements:

- (1) No portion of a swimming pool, pumps, filters or pool water disinfection equipment shall be located closer to any street line than the main building on the lot;
- (2) No portion of a swimming pool, pumps, filters or pool water disinfection equipment shall be constructed closer than 1.2 metres from any side or rear property line;

- (3) No portion of a swimming pool, pumps, filters or pool water disinfection equipment shall be located closer to any watercourse than the distance applicable to a main building or accessory building, whichever is less;
- (4) All outdoor swimming pools, or the yard in which a swimming pool is located, shall be completely enclosed so as to prevent uncontrolled access;
- (5) All enclosures shall meet the following requirements:
 - (a) Access shall be controlled by a 1.5 metres minimum fence or a combination of fencing, buildings, deck or similar structure or by a fence alone;
 - (b) An elevated deck surrounding an above ground pool may form part of an enclosure, provided:
 - iii) The deck abuts the sides of the above ground pool, and
 - iv) The combined height of the deck and fence on top of the deck is a minimum of 1.5 metres (5ft) above grade.
 - (c) All gates and doors accessing the enclosure shall be equipped with a self closing, self-latching lockable devices placed not less than 1.5 metres (5ft) in height from the surface or grade, and on the inside of the gate, except where access is provided through the dwelling unit;
 - (d) An enclosure or other structure, including gates and doors, intended to control access to a swimming pool, shall be a minimum of 1.5 metres (5ft) in height and shall be so constructed as to prevent easy access through, under or over it, and in the case of an above-ground type

of swimming pool, shall include a construction providing access to the swimming pool;

- (e) An enclosure or other structure, including gates and doors, intended to control access to a swimming pool should have a minimum distance of 1.2 metres (4ft) between horizontal members, a maximum of 10 centimetres (4in) size openings throughout the structure, and all horizontal members located on the pool side of the structure; and
- (f) No person shall permit any structures or material to be placed or to remain adjacent to any swimming pool enclosure which would provide a means of access over the top of the enclosure.

4.35 TEMPORARY USES PERMITTED

- (1) Nothing in this Bylaw shall prevent the use of land or the use or erection of temporary buildings or structures incidental to construction, including but not limited to: a construction camp, tool shed, scaffold, or a sales or rental office. No development permit for such temporary uses, buildings or structures shall be required, provided that a development permit for the main buildings has been issued. Such temporary use shall be terminated no more than 60 days after the completion of the construction of the main structure.
- (2) Nothing in this Bylaw shall prevent the use of land or the erection of temporary buildings, structures or signs for special occasions, events and holidays and no development permit shall be required for such temporary uses, provided that such use of buildings, structure or sign remains in place no more

than 21 consecutive days.

4.36 UTILITY EQUIPMENT BUILDINGS

- (1) Utility equipment buildings that are 20 square metres or less in size and are 2.5 metres or less in height, shall be permitted in all zones and be:
 - a) Considered as accessory buildings and subject to the setback requirements within the zone in which they are located on properties where there is a main building such as a house.
 - b) Considered as a main building and subject to the setback requirements within the zone in which they are located on properties where there is no main building.
- (2) Utility Equipment Buildings that do not meet the conditions in Section 4.35.1 may be considered by Site Plan Approval.

4.37 WASTE BINS

- (1) Waste bins shall not be permitted in a front yard (including the rear yard of a through lot) or flankage yard.
- (2) A waste bin may be permitted in the front yard

where:

- a) The waste bin cannot be located in the side or rear yard of a property due to steep slopes or other topographic features, building locations, or other physical restrictions; or
 - b) The building or complex of buildings under one ownership has 18 or more dwelling units.
- (3) A waste bin permitted by Section 4.35.2 shall be located a minimum distance of 15 metres from the front lot line and be completely screened from view from the street and neighbouring residences by fencing and/or landscaping approved by the Development Officer.

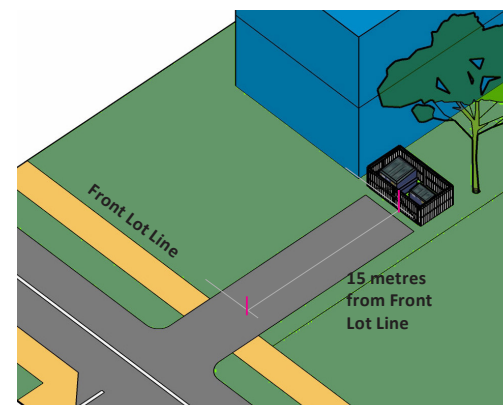


FIGURE 4.40 Fenced Waste Bin Setback

PART 5 Development Constraints

5.1 FLOODPLAIN DEVELOPMENT STANDARDS

- (1) Prohibited uses in the floodplains seen in Schedule B include:
 - (a) Hospitals
 - (b) Seniors care facilities
 - (c) Special care facilities
 - (d) Schools
 - (e) Warehousing or storage of hazardous materials
 - (f) Essential services
- (2) New development in areas within the floodplain as shown on Schedule “B”, are required to sign a *"Floodplain Development Undertaking Form"* (see Schedule E) acknowledging recognition of risks and confirming that:
 - (a) The finished floor elevation is no lower than 8 metres geodetic.

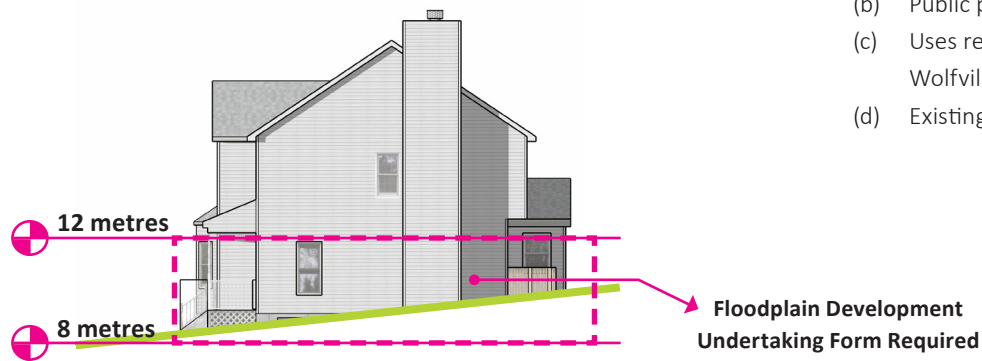


FIGURE 5.1 8 metres - 12 metres floodplain requirements

- (b) Walls and floors below 12 metres geodetic should be constructed to be flood tolerant.
- (c) Consideration be given to placement of mechanical equipment.

5.2 SOURCE WATER PROTECTION

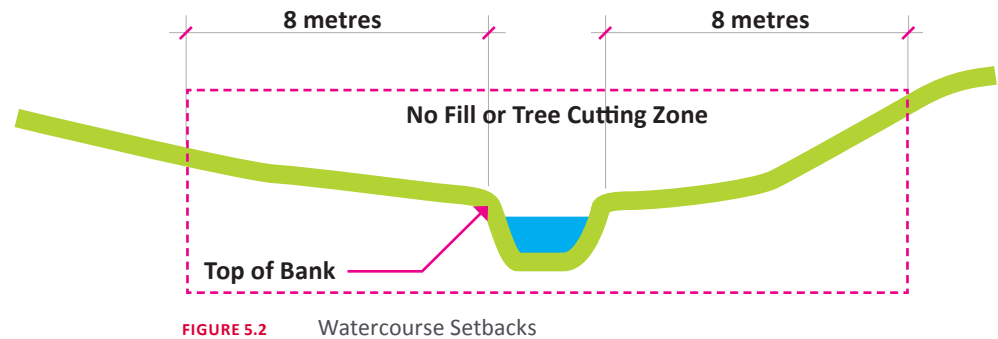
There are three sub-zones in the Source Water Protection area as shown on Schedule “B”, Source Water Protection Area – Wellhead Constraint Area, Source Water Protection Area – Zone A and B, and Source Water Protection Area – Zone C. Development within these areas, as determined by the Development Officer, may require the completion of an environmental impact assessment, secondary containment measures and/or a risk mitigation plan prepared by a qualified professional and shall be subject to the following:

- (1) **Source Water Protection Area - Wellhead Zone:**

Development within the Wellhead Constraint Area as identified on Schedule “B” of the Bylaw is limited to:

 - (a) Existing residential uses
 - (b) Public parkland
 - (c) Uses relating to the operation of the Town of Wolfville’s water supply
 - (d) Existing furnace oil storage

- (2) **Source Water Protection Area – Zone A and B:** Certain land uses which present a significant risk to the groundwater contamination will be prohibited, including:
- (a) Automotive painting, engine and auto body repair shops
 - (b) Bulk chemical storage
 - (c) Bulk storage of salt
 - (d) Commercial nurseries
 - (e) Commercial storage and/or distribution of chlorinated solvents
 - (f) Commercial storage and/or distribution of fertilizers
 - (g) Commercial storage and/or distribution of pesticides and herbicides
 - (h) Commercial storage and/or distribution of petroleum fuel
 - (i) Commercial storage and/or distribution of petroleum solvents
 - (j) Dry cleaners
 - (k) Gas station or accessory gas bars
 - (l) Manure storage facility
 - (m) Scrap metal and salvage yards and/or processing
- (3) **Source Water Protection Area – Zone C:** Similar to 5.2.2, uses that present an identified risk to groundwater contamination will be prohibited, including:
- (a) Automotive painting, engine and auto body repair shops
 - (b) Bulk storage of salt in excess of 100 tonnes
 - (c) Commercial storage and/or distribution of chlorinated solvents
 - (d) Commercial storage and/or distribution of pesticides and herbicides
 - (e) Commercial storage and/or distribution of petroleum fuel
 - (f) Commercial storage and/or distribution of petroleum solvents
 - (g) Dry cleaners
 - (h) Gas station or accessory gas bars



- (i) Scrap metal and salvage yards and/or processing
- (4) Expansion or redevelopment of existing non-conforming uses within the Source Water Protection Area Wellhead zone, zone A and B, and C may be considered by development agreement. See Municipal Planning Strategy, Part 7.3.3(8).
- (5) Development of new private geothermal wells in the Source Water Protection Areas are required to provide a risk mitigation plan prepared by a qualified professional.

5.3 WATERCOURSE, WETLANDS AND STEEP SLOPES

- (1) No structures shall be erected within 8 metres of the top of banks of watercourses as identified on Schedule “B”. An erosion and sedimentation control plan prepared by a qualified professional will be required with any application for development of any lot within 15m of a watercourse.
- (2) Developments proposed on slopes exceeding 20% as shown on schedule B, shall have plans, including mitigation of impacts to neighbouring properties, prepared by a qualified engineer or landscape architect to address stormwater management, erosion control and grade separation.

5.4 VIEW CORRIDORS

- (3) As per Schedule B, views to Blomidon from Reservoir Park, and views to the waterfront from Main Street and Gaspereau Avenue shall be considered for any new development on neighbouring lands zoned CDD (See Figure 5.3)
- (4) For any development within the former CDD boundary of the East End Secondary Plan (Primary Viewplane Area as per figure 5.4), no structure shall be erected, constructed, altered, reconstructed, or located so that it protrudes into a view plane. Under no circumstance shall the maximum allowable building height be exceeded.
- (5) The Development Officer may require a NS Land Surveyor to confirm and stamp that buildings within the primary viewplane area (shown in red on Figure 5.4) do not penetrate the viewplane.
- (6) Where a structure that lawfully existed on the effective date of the Bylaw protrudes into a required view plane, as shown on Figure 5.4 in the "primary and secondary Viewplane protection area", a new structure may be erected, constructed, altered reconstructed or located so that it protrudes into the required view plane if the new structure does not increase the existing protrusion.
- (7) For areas in the "secondary viewplane protection area" as shown on fig 5.4, development may proceed as per the underlying zoning, as per the general height requirements of the Land Use Bylaw.

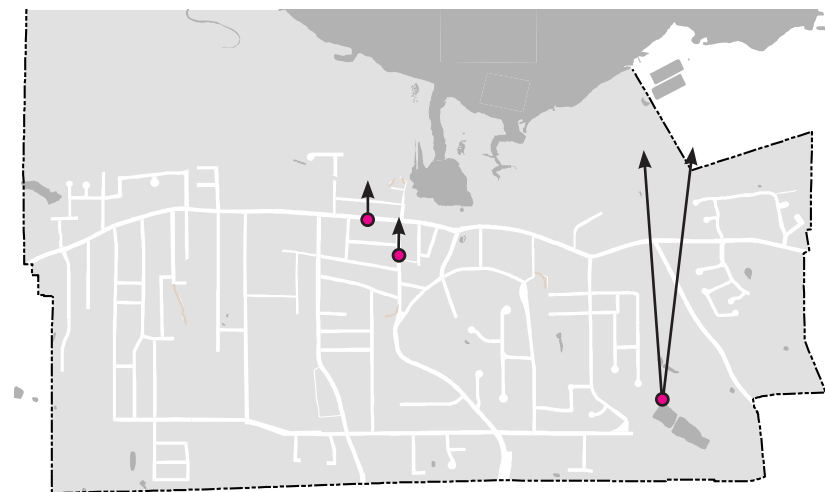


FIGURE 5.3 View Corridors



FIGURE 5.4 Primary (red) and Secondary (green) Viewplane protection areas

PART 6 Parking & Loading Requirements

6.1 NO MINIMUM PARKING REQUIREMENT

- (1) There are no minimum parking requirements for most development. Where parking requirements exist they are included in the zone requirements.
- (2) Where parking is included as part of any development the standards of Part 6 shall apply.

6.2 PARKING IN RESIDENTIAL ZONES

- (1) In all residential zones, parking in the front yard shall not take up more than:
 - (a) 70% of the front yard for townhouse dwellings; and
 - (b) 40% of the front yard for all other unit types.
- (2) No vehicle shall be parked on any area of a residential lot other than a parking area.
- (3) Vehicle access width to the street for any residential lot in a residential zone shall not exceed 7.0 metres.
- (4) No commercially registered motor vehicle having more than two axles or over 7.5 metres in length shall be permitted overnight on any lot in any residential zone except for recreational motor vehicles owned by the resident.
- (5) Where a side or rear yard of a multi-unit dwelling abuts any RL-R zone, parking spaces in the side or rear yard shall be located a minimum distance of 3

metres from the property line.

6.3 STANDARDS FOR PARKING AREAS

- (1) All parking spaces shall have an unobstructed access on a public street or unobstructed access to a driveway or aisle that leads to a public street.
- (2) The parking area shall be maintained with a stable surface sufficient to support a vehicle without undue deformation or damage of the surface, such as rutting, and does not allow the raising of dust or loose particles. Acceptable stable parking surfaces include but are not limited to asphalt or concrete paving (pervious or impervious), brick pavers, compacted granular surfaces, and structural landscape systems such as driveable grass or grass grid.
- (3) Any lights used for illumination of the parking area shall be arranged as to divert the light away from streets, and adjacent lots.
- (4) For any parking area containing more than 6 parking spaces, any ingress or egress driveways shall not be closer than 10 metres from the nearest boundary of any street intersection.

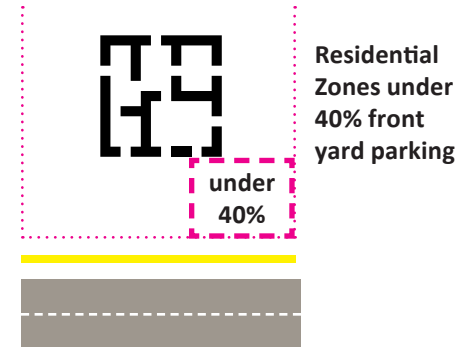


FIGURE 6.1 % Parking in Front yard

- (5) The maximum length of a parking row shall be twelve (12) stalls and a landscaped island shall be located at both ends of each parking row and be designed in accordance with:
 - (a) Be the full length of the parking row (5.8 metres minimum) and be a minimum of 1.8 metres wide.
 - (b) Contain a minimum of one deciduous tree (50 millimetres caliper) and eight shrubs (minimum 60 centimetres high) or to meet the requirements of a stormwater island.
- (6) Parking rows may exceed twelve (12) parking stalls where a landscaped island is incorporated in accordance with policy 6.2.5.
- (7) Where parking lots with more than two (2) cars cannot be located to the rear or side of a building, away from the public right-of-way, a landscaped strip shall be provided consisting of:
 - (a) A minimum width of 1.5 metres between any lane or parking spot fronting on a public right-of-way (excluding the access lane which shall not exceed 7 metres).

- (b) Contains a minimum of one tree (50 millimetres minimum caliper) for every 3m of parking or lane frontage length.
 - (c) Contains shrubs spaced no less than 1m apart and no less than 600 millimetres of height.
 - (d) Incorporates additional vegetation as may be required by the Development Officer to screen the parked cars without obstructing views.
 - (e) May incorporate stormwater measures
- (8) Parking stalls shall be no less than 2.8 metres x 5.8 metres.
- (9) Parking lanes shall be no less than 6 metres wide.
- (10) No more than 1 drive lane access and curb cut is permitted per lot on each road frontage unless spaced more than 60 metres apart.
- (11) Parking in the Downtown and Neighbourhood Design Guidelines areas shall be reviewed with the relevant Design Guidelines.
- (12) For existing uses the Development Officer may encourage the relocation of parking on a site through easements, hard surface abatements of other means.

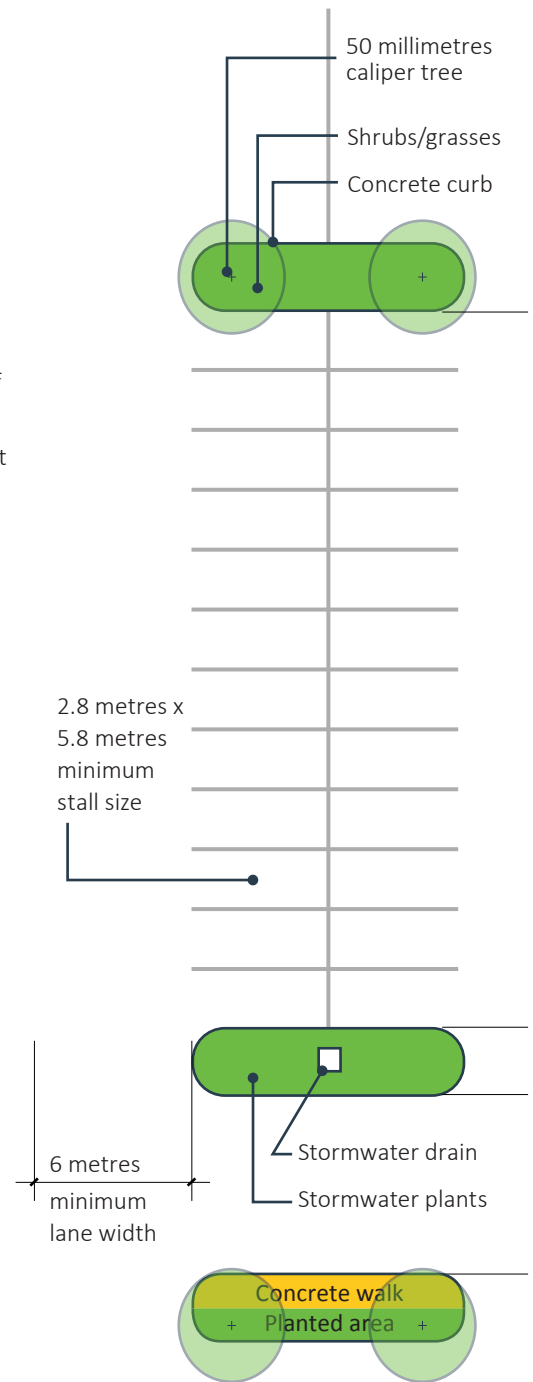


FIGURE 6.6 Parking Requirements

6.4 BICYCLE PARKING REQUIREMENTS

Bicycle parking shall adhere to the following;

- (1) Minimum required bicycle parking spaces quantified in Table 6.1.
- (2) Class A bicycle parking spaces shall:
 - (a) Be located indoors;
 - (b) Have a clear width of 0.6 metres from the outside to the bicycle parking;
- (3) Class B bicycle parking shall:
 - (a) Be located a maximum of 10 metres from an entrance when uncovered;
 - (b) Be located 30 metres from an entrance when covered;
 - (c) Be located at ground level;
 - (d) Have direction signage when not visible from the streetline;
 - (e) Have a minimum overhead clearance of 2 metres.
- (4) Where it is not possible to provide on-site Class B bicycle parking, cash in lieu of parking will be required.
- (5) Required bicycle parking calculations shall be rounded up to the nearest whole number.

6.5 LOADING ZONES

- (1) Any uses requiring a Loading Zone shall provide

details to be considered in a Development Permit, Site Plan or Development Agreement application.

- (2) Any required loading zones in public right-of-ways must be coordinated with the Town's Traffic Authority at the time of the application.

TABLE 6.1 Bicycle Parking Requirements

Type of Use	Minimum Bicycle Parking Requirement
Multiple Unit Dwelling (greater than 30 units)	0.25 spaces per dwelling unit 80-100% Class A, 20% Class B
Multiple Unit Dwelling (less than 30 units)	0.25 bike parking spaces per dwelling
Lots with Parking lots greater than 30 spaces	1 bike parking space per 10 vehicle parking spaces

General Requirements For Signs

7.1 GENERAL

- (1) Where this part is inconsistent with any statute of the Province of Nova Scotia or regulations made there under respecting advertising signs on or near public highways, the more restrictive regulations shall apply.
- (2) No person shall erect a sign without first obtaining a permit from the Development Officer except in the case of signs listed in Section 7.2 and no permit to erect a sign shall be issued unless all the sign provisions of the Bylaw are satisfied.
- (3) Signage for business premises shall be for the purpose of identifying the name of the business and the general product or service provided. Signage shall not be used for the purpose of advertising and/or promotions.
- (4) Every sign and all parts thereof including framework, supports, background, anchors and wiring systems shall be constructed with adequate structural support to secure the sign. The Development Officer may request a certification from a Professional Engineer confirming the structural integrity of any sign prior to issuance of a permit.
- (5) All signs and all parts thereof, including structural support, shall be kept in a good state of repair and maintenance.

- (6) Signage for business premises located in a design guidelines are required to meet the design guidelines for the corresponding area.

7.2 SIGNS PERMITTED IN ALL ZONES

The following additional signs are permitted in all zones and do not require a permit pursuant to this Part.

- (1) Signs identifying name and address of resident and of not more than 0.2 square metres in sign area.
- (2) “No Trespassing” signs or other such signs regulating the use of a property and of not more than 0.2 square metres in sign area.
- (3) Real estate signs not exceeding 0.5 square metres in sign area in a Residential Zone and 3 square metres in other zones or on a residential property with 12 or more dwelling units, which advertise the sale, rental or lease of the premises.
- (4) Signs regulating or denoting on-premises traffic, parking or other signs denoting the direction or function of various parts of a building or premise provided that such signs are less than 0.5 square metres in sign area.
- (5) Signs erected by a public authority or under the direction of such authority and signs located on public streets as permitted under the Town of Wolfville Streets Bylaw.

- (6) Memorial signs or tablets and signs denoting the date of erection of a structure.
- (7) The flag, pennant or insignia of any public authority, or of any religious, charitable, or fraternal organization.
- (8) A sign having a sign area of not more than 5 square metres incidental to construction and located on the construction site.
- (9) Electoral signs.

7.3 SIGNS PROHIBITED IN ALL ZONES

The following signs shall not be permitted in any zone:

- (1) Signs which incorporate, in any manner, any flashing or moving illumination which varies in intensity or any illumination which varies in colour.
- (2) Signs which have any visible moving part, visible revolving parts, visible mechanical movement of any description or other apparent visible movement achieved by electrical pulsations or by actions of normal wind currents.
- (3) Signs located on the roof of a building.
- (4) Signs or sign structure which constitutes a hazard to public safety or health.
- (5) Signs which by reason of size, location, content, colouring, or manner of illumination obstruct the vision of drivers either when leaving a roadway or

driveway; or obstruct or detract from the visibility or effectiveness of any traffic sign; or control device on public streets and roads.

- (6) Signs which obstruct free ingress to or egress from a fire escape door, window or other required exit way.
- (7) Signs not erected by a public authority which make use of words such as 'STOP', 'LOOK', 'ONE WAY', 'DANGER', 'YIELD' or any similar words, phrases, symbols, lights, or characters typically used by a public authority to regulate vehicles or pedestrians.
- (8) Signs on public property or public right-of-way, unless:
 - (a) Erected by a public authority; or
 - (b) A uniform Bed & Breakfast sign for which a permit to erect has been issued under this Bylaw; or
- (9) Signs painted on, attached to, or supported by a tree, stone, cliff or other natural object.
- (10) Signs not related to any business premise or use located on the lot or premises except one off premise sign shall be permitted on a building or lot provided the maximum size of the sign shall be 0.7 square metres, and the sign complies with all other applicable requirements of this Bylaw.
- (11) Pennants, spinners, banners, and streamers except those expressly permitted by this or any other Bylaw.

- (12) Illuminated signs.
- (13) Reader board signs.
- (14) Ground signs with more than 2 sign faces

7.4 SIGNS IN RESIDENTIAL ZONES

Signs shall not be permitted in residential zones except as follows:

- (1) As permitted in Section 7.2 Signs Permitted in all Zones.
- (2) As permitted in Section 4.17 Home Based Business.
- (3) A residential building or a group of residential buildings under common ownership with 16 or more dwelling units shall be permitted to have one ground sign which identifies the name of the residential building or group of buildings. The sign may contain ancillary information such as the date the development was established but may not include advertising messages. The maximum sign area of the sign shall be 3 square metres and the maximum height of the sign shall be 2.5 metres.

7.5 SIGNS IN COMMERCIAL ZONES

(1) Limit on Number of Signs

- (a) Each business premise is permitted to erect a maximum of two (2) signs, including combined signs on the building or property occupied by the business premise. The

maximum number of projecting signs shall be one (1) per business premise per street front.

- (b) Notwithstanding Section 7.5.1(a), a business premise with more than one (1) public entrance is permitted one (1) additional sign for each public entrance in excess of one.
- (c) Notwithstanding Sections 7.5(a) and 7.5(b), one Portable sign per business premise shall be permitted in accordance with Section 7.7.3.
- (d) A ground sign with each side used to identify the same business premise shall count as a single sign.
- (e) In the case of a corner lot or through lot, one (1) additional sign will be permitted in addition to all other permitted signs.
- (f) A combined sign counts as one (1) sign for each business premise whose name, symbol, logo, or other identifying feature is included on the combined sign.
- (g) On a building with more than one (1) side visible to public view, two (2) identical facial wall signs placed on different sides of the building count as one (1) sign.
- (h) In addition to all other signs permitted under this section, one (1) sign which identifies the name of the building or development name is permitted, if it complies with the size requirements of Section 7.5.2.

FIGURE 7.1 Sign Types

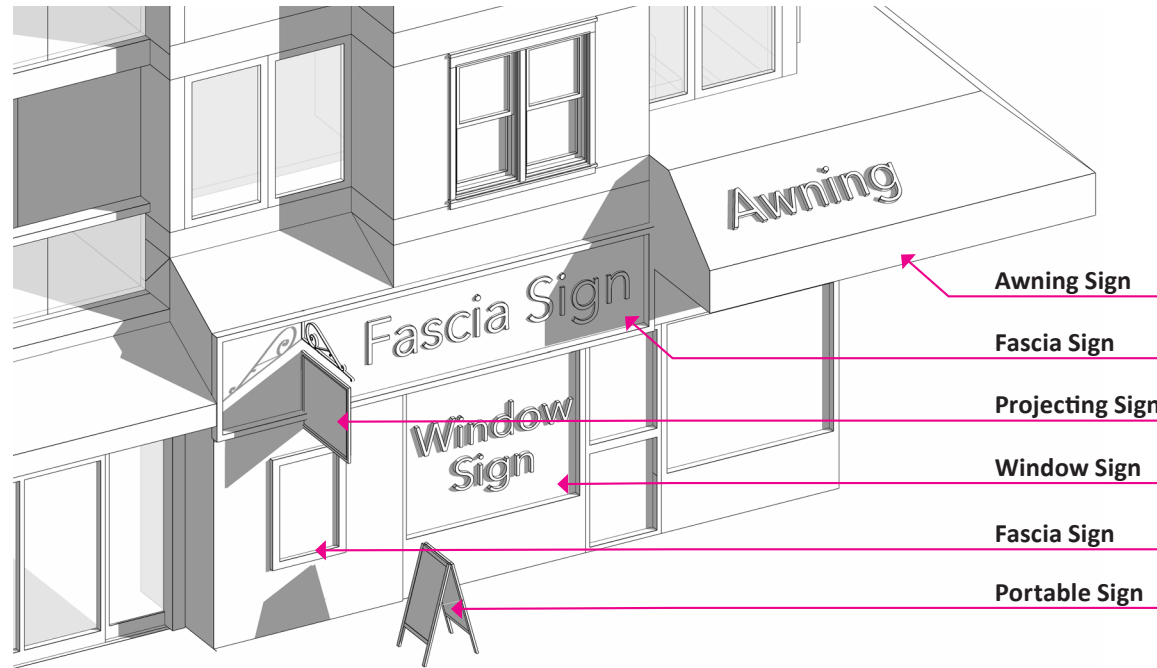
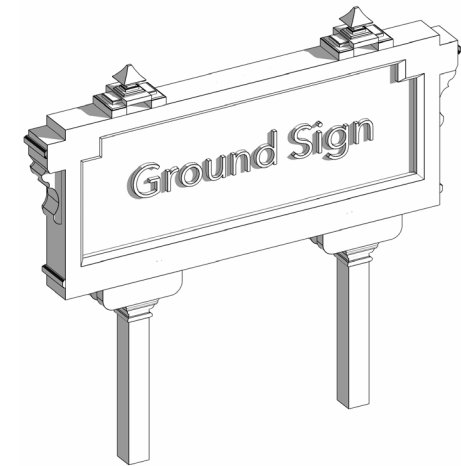


FIGURE 7.2 Ground Sign



7.6 SIZE AND LOCATION OF SIGNS

(1) Fascia Wall Signs

- (a) No combined sign area for all facial signs on any side of a building shall exceed 0.3 square metres per linear metre of building width of the side on which the signs are to be placed. In no case shall the total sign area of facial wall signs on 1 side of the building exceed 10 square metres.
- (b) No Facial wall signs shall:
 - i) Extend above the top of the wall upon which it is attached.
 - ii) Extend beyond the extremities of the wall upon which it is attached.

- iii) Extend over any substantial architectural feature of a building, such as windows, doors, columns, corner boards, frieze bands, fascia boards, etc.

(2) Ground Signs

- (a) No ground sign shall exceed 2.0 square metres in sign area for a single business premise and a combined ground sign shall not exceed 1.5 square metres in background area for each business premise referred to on the sign. Notwithstanding the foregoing, a ground sign for a tourist establishment may have a sign area of 3 square metres.

- (b) No ground sign shall exceed a height of 5 metres from the grade level to the highest part of the sign.
- (c) No ground sign area shall exceed a width of 2.5 metres.
- (d) No ground sign shall extend beyond a property line or project over a public street or road, other adjoining lands, corner vision triangle or any driveway or parking space.

(3) Portable Signs

- (a) Portable signs shall adhere to the following requirements:
 - i) Only 1 portable sign is permitted per business.
 - ii) A Portable Sign shall have a maximum of two (2) sign faces.
 - i) Each sign face shall not exceed 0.56 square metres in area.
 - ii) Sign faces shall not exceed 1m in height.
 - iii) A Portable Sign shall be placed on a sidewalk adjacent to the business premise only when the premise is open to the public or in areas that may be designated by the Town within public parks or the right-of-way.

- iv) A Portable Sign shall not interfere with the flow of pedestrian or vehicular traffic or pose a safety hazard. The sign shall be placed in a manner that leaves a minimum walkway of 1.8 metres on the sidewalk.
- v) A Portable Sign shall not be placed in a public right-of-way when snow or ice are present.

(4) Projecting Signs

- (a) Notwithstanding the Town of Wolfville Streets Bylaw projecting wall signs and canopy/awning signs shall be permitted.
- (b) No projecting sign shall exceed a sign area of 0.7 square metres.
- (c) No projecting sign or any part thereof shall be less than 2.5 metres above established grade.
- (d) No projecting sign shall extend for more than 1.2 metres from the building wall on which it is attached.
- (e) No projecting wall sign shall be located within 7 metres from any other projecting wall sign on the same building.
- (f) Canopy/Awning signs shall not exceed 0.3 square metres per linear metre to a maximum of 10 square metres. Canopies or Awnings shall not project more than 2 metres over the sidewalk or be erected less than 2.2 metres above the sidewalk.

7.7 SIGNS IN INSTITUTIONAL ZONES

- (1) An institutional premise in the Institutional General zone shall be permitted to have signage in accordance with the provisions for signage for a business premise in a commercial zone as per Section 7.5.
- (2) Signage in the Institutional University zone shall be permitted as follows:
 - (a) Ground signs for wayfinding and building identification are permitted with a maximum sign area of 2.0 square metres and a maximum height of 2.5 metres.
 - (b) Facial wall signs for building identification are permitted with a maximum sign area of 5% of the area of the building face upon which they are placed.
- (3) Notwithstanding section 7.3, Digital Reader Board Signs are permitted subject to the following:
 - (a) For Institutional assembly uses fronting on an Arterial Road in the I-2 zone.
 - (b) No scrolling text, flashing, animation, video or graphics are permitted.
 - (c) Text may only change every 2 hours.
 - (d) The nighttime brightness of the sign must not exceed 100 NITS and may not operate between 11:00 pm and 7:00 am.
 - (e) The sign must be equipped with a sensor which automatically lowers light output in accordance with atmospheric conditions and so not to exceed the maximum number of NITS permitted.
 - (f) The sign is only for information related to the use on the property.
 - (g) Sign text shall be white.
 - (h) Digital signs require a default mechanism that will cause them to revert to a black screen during a malfunction.
 - (i) The digital sign shall encompass a maximum of 50% of the sign face and maybe be combined with a ground sign to a maximum of 5 square metres.
 - (j) The applicant shall submit the operational specification at the time of permit application demonstrating compliance with the requirements of the bylaw.
 - (k) The sign shall be located a minimum of 30 metres from a pedestrian crosswalk.

General Requirements for Neighbourhood Zones

8.1 PERMITTED USES

- (1) In this Bylaw any use not listed as a permitted use in a zone is prohibited in that zone unless otherwise indicated.
- (2) Where a permitted use within any zone is defined in this Bylaw the uses permitted in the zone include any similar uses that satisfy such definition except where a definition specifically excludes any similar use.
- (3) Municipal offices, buildings, and infrastructure shall be permitted in any zone.

8.2 DESIGN GUIDELINE AREAS & REGISTERED HERITAGE PROPERTIES

- (1) New buildings in the Design Guideline Areas shall be subject to Site Plan Approval.
- (2) Additions to buildings in the Design Guidelines Areas greater than 25% of the floor area shall be subject to site plan approval
- (3) Additions to registered heritage buildings greater than 10% of the floor area shall be subject to site plan approval

TABLE 8.1 Neighbourhood Land Use Table

APPROVAL PROCESS:

P = Permitted as-of-right,

PC = Permitted with Conditions

SP = Site Plan Approval

DA = Development Agreement

- = Not Permitted

Residential Uses	RL-R	RL	RM	RH	MU
Additions	P	P	P	P	P
Accessory Dwelling Unit – Detached	PC	PC	PC	PC	PC
Accessory Dwelling Unit – Internal	PC	PC	PC	PC	PC
Dwelling, Single	P	P	P	-	P
Dwelling, Two Unit	-	P	P	P	P
Dwelling, Townhouse (maximum 4 units)	-	PC	P	P	P
Dwelling, Multi-unit (maximum units)	-	PC	P	P	P
Townhouse up to 6 consecutive units	-	-	P	P	P
Stacked Townhouse up to 6 consecutive units (12 total units)	-	-	SP	SP	SP
Dwelling, Multi-unit (60 units)	-	-	SP	SP	SP
Dwelling, Multi-unit (120 units)	-	-	-	SP	-
Cluster Housing	-	DA	SP	SP	SP
Existing	P	P	P	P	P
Home Occupation – Restricted (under 50 square metres)	PC	PC	PC	PC	PC
Home Based Business (Arts and Crafts Workshop, Catering Establishments, Daycares up to seven children/persons, Online/Digital Sales, Offices, Studios, Short Term Rentals maximum of 3 rental rooms) under 80 square metres	-	PC	PC	PC	PC
Innovative Housing	-	DA	DA	DA	DA
Parks and Playgrounds, Public Washrooms, Community Gardens, & Historic Sites	P	P	P	P	P

TABLE 8.2 Neighbourhood Land Use Table

APPROVAL PROCESS:

P = Permitted as-of-right,

PC = Permitted with Conditions

SP = Site Plan Approval

DA = Development Agreement

- = Not Permitted

Neighbourhood Commercial Uses	RL-R	RL	RM	RH	MU
Art Galleries & Studios	-	-	-	DA	SP
Bakeries	-	-	-	-	SP
Catering Establishment (over 80 square metres)	-	-	-	-	SP
Commercial Schools	-	DA	-	DA	P
Craft Workshops under 80 square metres	-	-	-	DA	SP
Daycare Facilities	-	PC	PC	PC	PC
Farm Markets	-	-	-	P	P
Home Based Commercial (under 120 square metres)	-	PC	PC	P	SP
Hostels	-	-	-	-	SP
Hotels	-	-	-	-	SP
Institutional Uses	-	-	-	P	SP
Laundromats	-	-	-	P	SP
Medical Clinics	-	-	-	P	SP
Nano-Brewery (See Special Provisions in the MU zone)	-	-	-	DA	SP
Neighbourhood cafe (under 120 square metres)	-	PC	PC	PC	PC
Neighbourhood Commercial up to 40% of Groundfloor of a multi unit building	-	-	-	SP	-
Neighbourhood Commercial up to 100% of Groundfloor of a multi unit building	-	-	-	SP	-
Nursing Homes	-	-	-	P	SP
Offices & Professional Services	-	-	-	P	SP
Personal Services	-	-	-	P	P
Residential Care Facility	-	PC	PC	PC	PC
Short Term Rentals	-	PC	PC	PC	PC
Single Room Occupancy (4 or more rental rooms)	-	PC	PC	PC	PC
Stand Alone Parking Lots and Structures	-	-	-	-	-
Tasting Rooms (See Special Provisions in the MU zone)	-	-	-	DA	SP

TABLE 8.4 Cluster Development Requirements

Requirement	
Minimum front yard Setback	2 metres
Minimum separation distance between buildings in a Cluster Residential Development	2.5 metres
Parking	In addition to providing parking in accordance with Part 6 of this Bylaw, parking within a Cluster Residential Development may be located anywhere within the development, whether or not the development parcels within the Cluster Residential Development are severed. Required visitor parking may be provided as parallel parking on a private street provided the private street has a minimum width of 9.5 metres.
Maximum lot coverage	Where lots in a cluster Residential Development are to be subdivided, the maximum lot coverage on each lot shall be 40% without any shared public open space or 60% if the cluster has designated at least 15% of the gross site area for public open space or private (shared amongst residents) open space.
Landscaping	The cluster shall include: 1. At least one 50 millimetres minimum caliper tree per unit 2. 10 square metres of mulched shrub beds per unit 3. All soft landscape areas are sodded or seeded or left naturalized with native plants. 4. A hard surface pathway no less than 1.5 metres wide connecting the parking area(s) to each unit 5. Where feasible, a shared common open space area fully landscaped for residents.

8.3 CLUSTER HOUSING

- (4) Cluster Housing developments shall be considered on lots larger than 2,000 square metres in the Residential Low-Density (RL) by Development Agreement and in the Residential Medium-Density (RM) and Residential High-Density (RH) Zones by Site Plan Approval. Proposals not able to meet the requirements of Section 8.3 or other underlying zone requirements may be considered by Development Agreement as Innovative Housing.
- (5) The entire Cluster Residential Development shall comply with all applicable sections of this Bylaw and the provisions set out in Table 5.1.

- (6) A Residential use building in a Cluster Residential Development is considered to meet the frontage requirements in Section 4.14 provided the land on which it is to be located after severance has a minimum of 6 metres of frontage abutting a private street that serves as a driveway leading to a public street.
- (7) Parcels within the Cluster Residential Development that are severed need not comply with the dwelling-type specific provisions for the RL, RM, or RH zones.

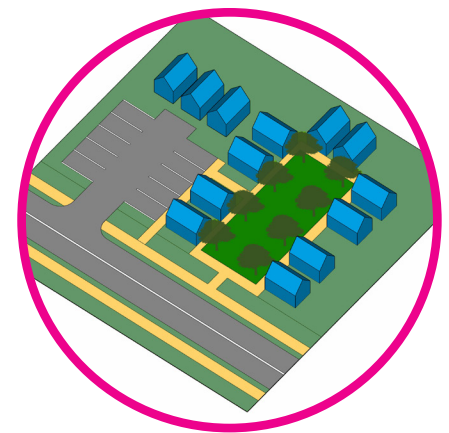


FIGURE 8.3 Cluster Housing Example

8.4 CONFORMITY WITH EXISTING FRONT YARDS

Notwithstanding anything else in this Bylaw, in any residential zone, new structures between existing buildings within 60 metres on the same block may be built with a setback equal to the average setback of the adjacent buildings, but this depth shall not be less than 3 metres from the front lot line and need be no greater than setback regulations prescribed in the zone in which it is situated.

8.5 NEIGHBOURHOOD DESIGN

- (1) Identical buildings may not be repeated more frequently than every fourth (4th) building along the same side of a street.
 - (a) Buildings are not identical if they are varied in at least 2 of the following: roof style; building mass and form; variation in exterior surfaces, colours and materials; fenestration; and main entry and garage placement, subject to the approval of the Development Officer.
- (2) To avoid facades of large blank walls on corner sites (having frontage on two streets), the façades of a principal building abutting the front lot line and the flanking side lot line shall use consistent building materials and architectural features on both frontages, and shall include features such as windows, doors, or porches, subject to the approval of the Development Officer.

8.6 LOT SURFACING AND LANDSCAPING

- (1) **General Requirements:**
 - (a) The maximum hard surface area of any lot in any Residential zone shall be 50% of the lot area, subject to stormwater management allowances.
 - (b) All disturbed areas of the site shall be landscaped with grass well suited for the level of shade in which they will be placed, ground covers, or native meadow mixtures.
 - (c) Wherever possible, trees with a diameter at breast height (DBH) of 30 centimetres or more should be preserved whenever possible. Trees greater than 30 centimetres DBH must be protected within 2 metres of all property lines unless the tree can be shown to compromise the safety of new buildings or additions.
 - (d) Any excavation or work within the dripline of trees greater than 60 centimetres DBH must be inspected by a certified arborist and remedial measures suggested should be implemented to preserve the health of larger trees.
 - (e) For any buildings set back 8m or more from the streetline, one 60 millimetre caliper (minimum) sized tree is required to be planted or retained between the building and the streetline for every 8m of building frontage.

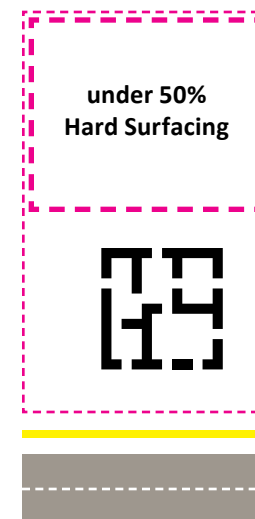


FIGURE 8.5 Hard surfacing coverage

- (f) Mechanical equipment, waste bins, and other obnoxious uses shall not be located in the front yard of any building.
- (g) The landscaping referred to in Section 8.3.2 shall be established within 1 year from the issuance of an occupancy permit.

(2) **Abutting Landscape buffers**

- (a) Where a multi unit building greater than 3 units abuts a yard in any Residential, Institutional, Park or Recreation zone a landscaped buffer is required. The minimum yard requirement for the abutting yard (rear or side) shall be 3 metres and shall include one or more of the following between the building and the abutting property:
 - i) At least one native shrub (that will grow to at least 2m in height) for along the abutting sideyard and one tree (minimum caliper of 50 millimetres) for every 4.5 linear metres of building depth; or
 - ii) Evergreen shrubs (that will grow to at least 2 metres in height) that form an opaque and continuous visual barrier between the proposed building and the sideyard; or
 - iii) An opaque wood fence or masonry wall at least 1.8 metres high continuous along the entire side yard.

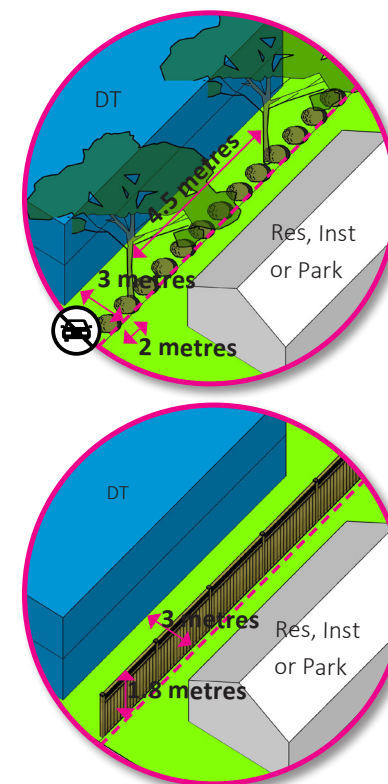
- iv) Or any combination of 1-3.

- (b) No parking space, driveway or travel lane shall be permitted in the 3 metre landscape buffer. This requirement may be reduced to 1.5 metres provided that landscape screening as described in 8.3.2.(A) provides a visual barrier along the full length of the sideyard lot line.

(3) **Lighting**

- (a) Any lit driveways, walkways, parking areas, circulation roads, or service areas shall be designed to reflect light away from adjacent residential properties using cut-offs, strategic placement or other means of reducing light pollution.
- (b) Any lights must be manually operable or on a timer to ensure lights are no left on all night.

FIGURE 8.6 Landscaping



8.7 FARM ANIMALS

No lot in any residential zone or designation shall be used for the rearing or keeping of farm animals except for a maximum of four laying hens contained within the lot.

8.8 RESIDENTIAL CARE FACILITIES

When creating a residential care facility, the following criteria shall apply:

- (1) The use must be contained within the single unit dwelling; and
- (2) No more than four (4) persons are provided with nursing and/or personal care; and
- (3) There shall be a resident manager.

8.9 SITE PLAN APPROVAL

The following Site Plan criteria shall apply to all new development that is permitted through the Site Plan Approval process in the RL Zone, RM Zone and the RH Zones. No development permit shall be issued for development that is inconsistent with these criteria. All development subject to Site Plan Approval must also meet all applicable zone standards and all other requirements of this Bylaw and section 2.9.

- (1) The location of new structures, off-street parking and the location, width and number of driveway access points on the lot shall minimize negative impacts on the surrounding neighbourhood, including dust, fumes, lighting, shadows, or other nuisance or inconvenience to

occupants of nearby residences.

- (2) The type, location, and height of walls, fences, hedges, trees, shrubs, ground cover or other landscaping elements, or a combination thereof, which is necessary to protect and minimize negative land use impact on neighbouring properties.
- (3) Existing vegetation shall be retained where the vegetation is healthy and helps to minimize negative impacts on the surrounding neighbourhood.
- (4) The location of pedestrian walkways, and/or related infrastructure, shall be provided to link public sidewalks and parking areas to entrances of all primary buildings.
- (5) The type and location of outdoor lighting is designed to light the structure, driveways and pedestrian infrastructure, but shall not be directed onto neighbouring properties.
- (6) For maximum separation from residential development and public areas.
- (7) The location of all existing easements shall be identified;
- (8) The grading or alteration in elevation or contour of the land shall minimize undue erosion and/or sedimentation, and other negative impacts on neighbouring properties.
- (9) The management of storm and surface water is addressed, and associated plans are approved by the Town Engineer.

- (10) The type, location number and size of signs or sign structures do not negatively alter the appearance of the streetscape or neighbourhood; and
- (11) All signage shall be designed and constructed according to the signage requirements listed in Part 7.
- (12) If the property is located within the Downtown and Neighbourhood Design Guideline areas (see Figure 4.5), the architecture of the building must be in keeping with the Neighbourhood Design Guidelines. Input from the Design Review Working Group may be required.
- (13) The Development Officer may vary any of the prescriptive dimensional requirements by up to 10 percent of the requirements to allow some flexibility to accommodate physical anomalies of a site, so long as the intent of the particular requirement is not compromised.

8.10 TRAIL CONNECTIVITY

Where a development site in the RM, RH, or MU zone abuts a trail the development shall provide a dedicated pedestrian connection onto the trail system.

PART 9 Low Density Residential Restricted (RL-R) Zone

9.1 INTENT

The RL-R zone is intended to permit low density residential development including single dwelling units with one accessory dwelling unit subject to conditions in 4.17.

9.2 PERMITTED USES

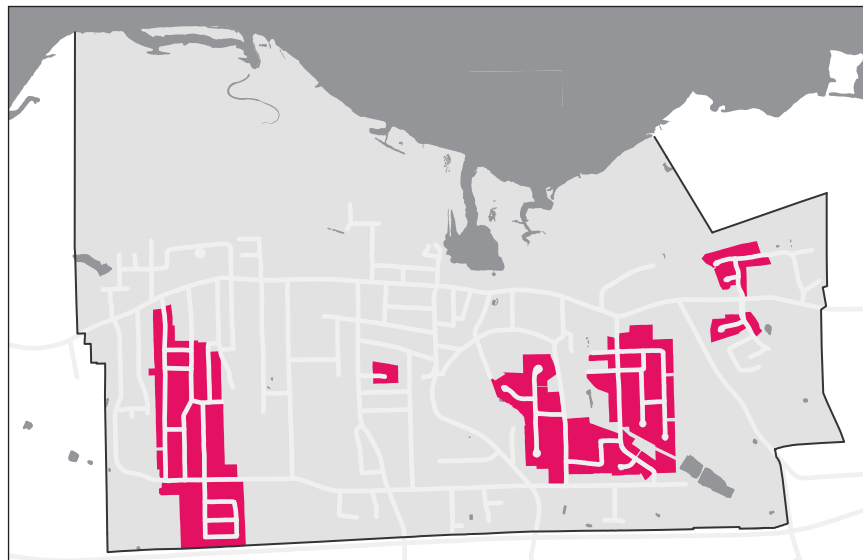
As-of-right, and as-of-right with conditions are shown in Table 8.1.

9.3 RE-ZONING

Rezoning is not permitted.

9.4 BUILT FORM REQUIREMENTS

- (1) Developers are encouraged to use materials and styles consistent with existing neighbourhood character.



Single unit dwellings



Single Unit + ADU



Additions



Home Occupation

Other Provisions Apply

Part 4:

General Requirements for All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 8:

General Requirements for Neighbourhood Zones

9.5 INTERNAL ACCESSORY DWELLING UNITS WITH CONDITIONS

- (1) One (1) internal accessory dwelling unit is permitted in the Restricted RL-R Zone subject to the following conditions:
 - (a) The Zone requirements and Building Code requirements must be met;
 - (b) The internal accessory dwelling unit has no more than two (2) bedrooms;
 - (c) One (1) additional Parking Space shall be provided on the same lot; and
 - (d) No Detached Accessory Dwelling Unit exists on the property.

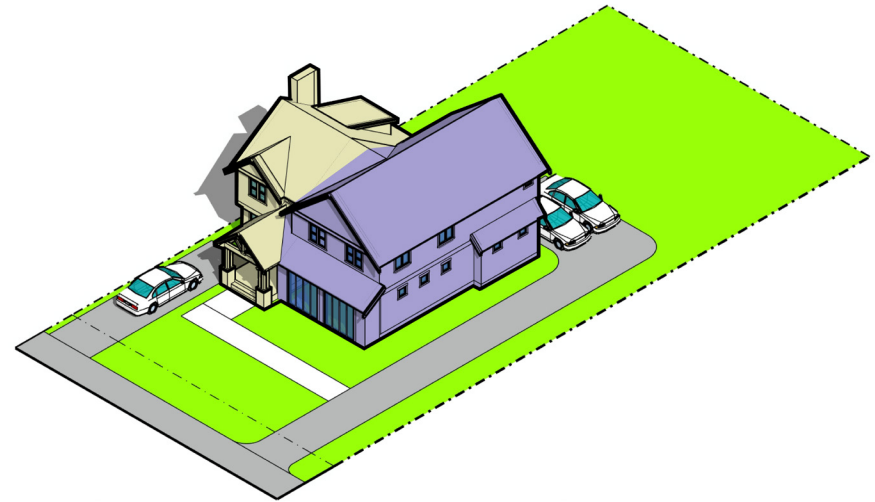


FIGURE 9.1 Principle residence and Internal Accessory Dwelling Unit Example

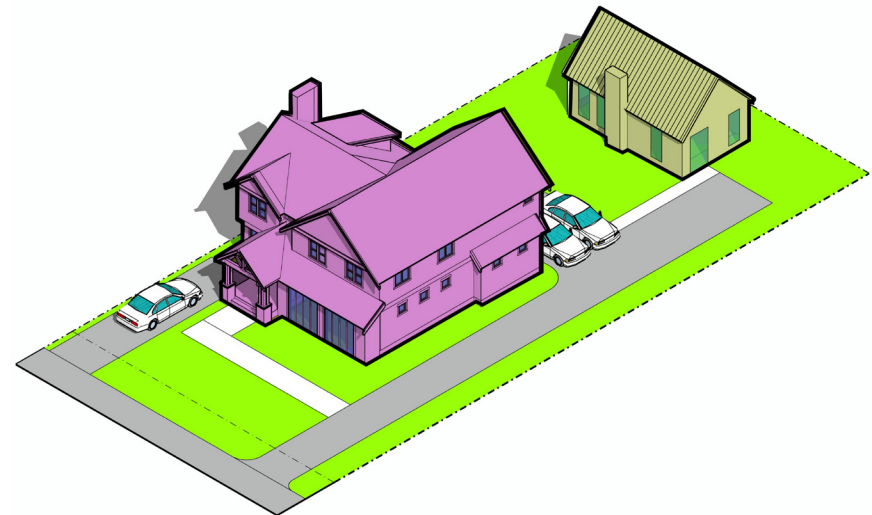


FIGURE 9.2 Principle residence and Detached Accessory Dwelling Unit Example

9.6 BUILT FORM STANDARDS

See Figure 9.3 for all built form standards in the RL-R Zone

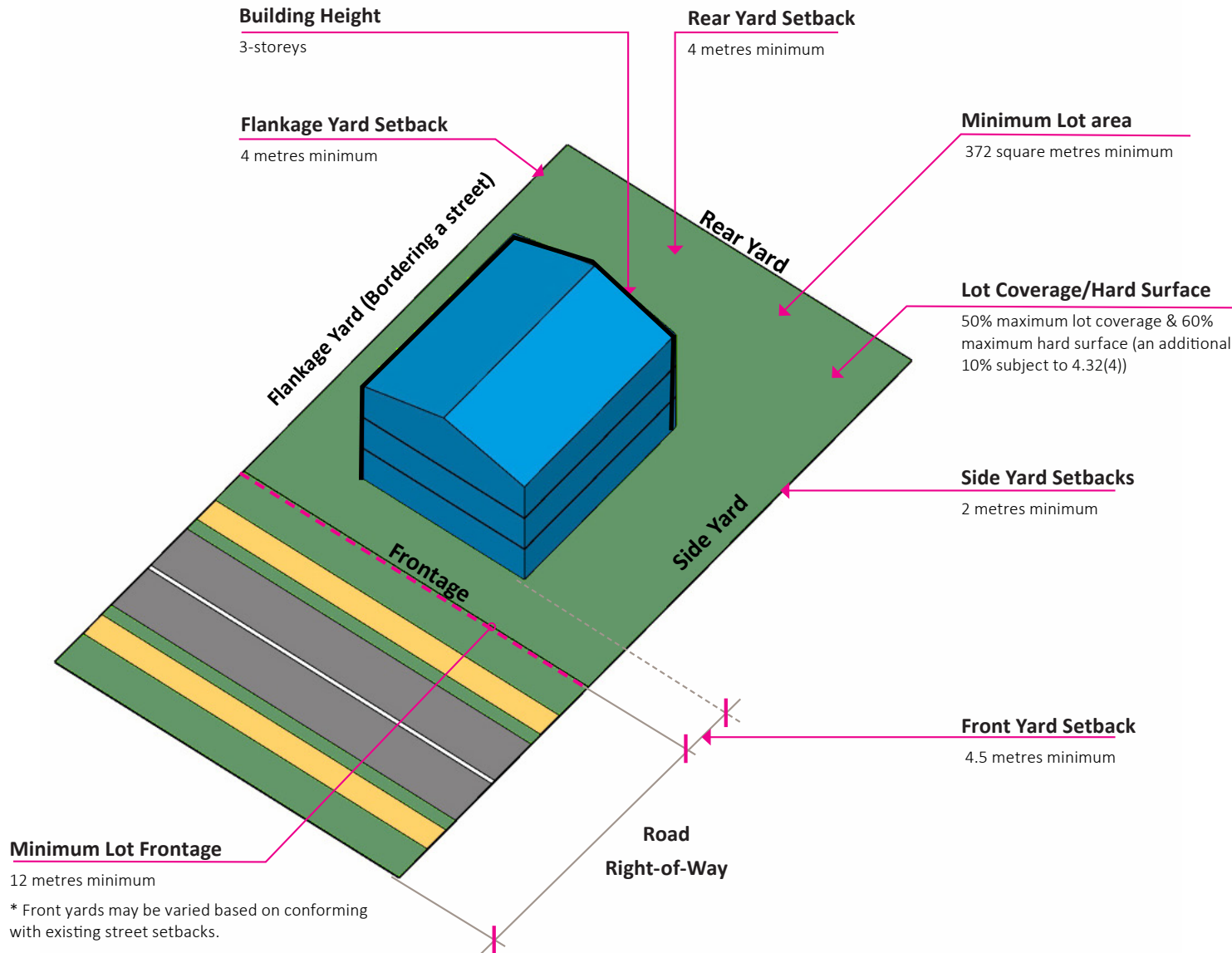


FIGURE 9.3 RL-R ZONE Built Form Standards

9.7 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the RL-R Zone



FIGURE 9.1 RL-R ZONE Single Family with Detached Garage Example



FIGURE 9.2 RL-R ZONE Single Family Example



FIGURE 9.3 RL-R ZONE Single Family Addition Example

PART 10 Low Density Residential (RL) Zone

10.1 INTENT

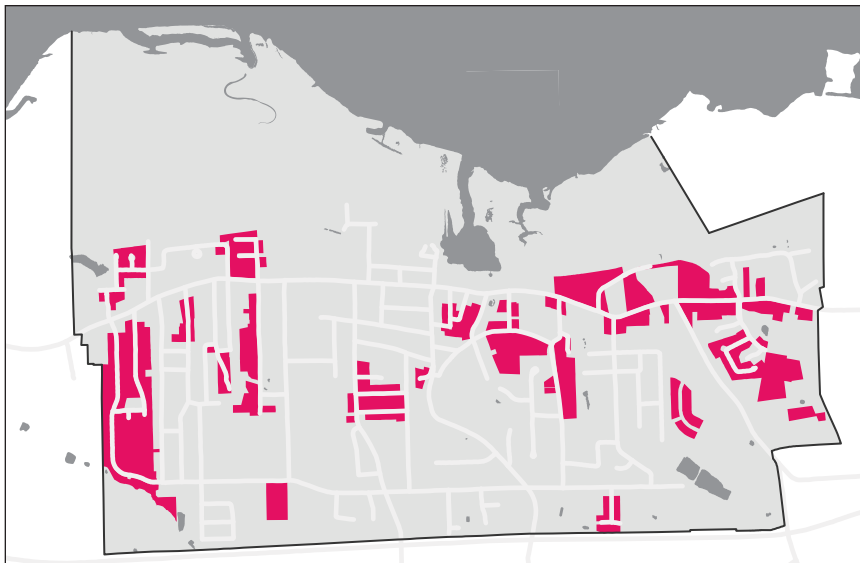
The RL zone is intended to permit a range of low density residential development to a maximum of up to 4 units. Home based businesses and other uses such as short-term rentals in an owner-occupied dwelling unit are permitted subject to conditions in 4.17. Council may consider unique and site-specific developments by development agreement.

10.2 PERMITTED USES

As-of-right, Site Plan and Development Agreement uses are shown in Table 8.1

10.3 RE-ZONING

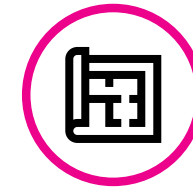
Rezoning to the RM zone will be considered subject to **policy 11.4.3** of the MPS.



Two unit dwellings



3-4 unit dwellings with Conditions



Additions



Accessory Dwelling Unit



Home based business

Other Provisions Apply

Part 4:

General Requirements for All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 8:

General Requirements for Neighbourhood Zones

10.4 BUILT FORM STANDARDS

See Figure 10.1 for all built form standards in the RL Zone

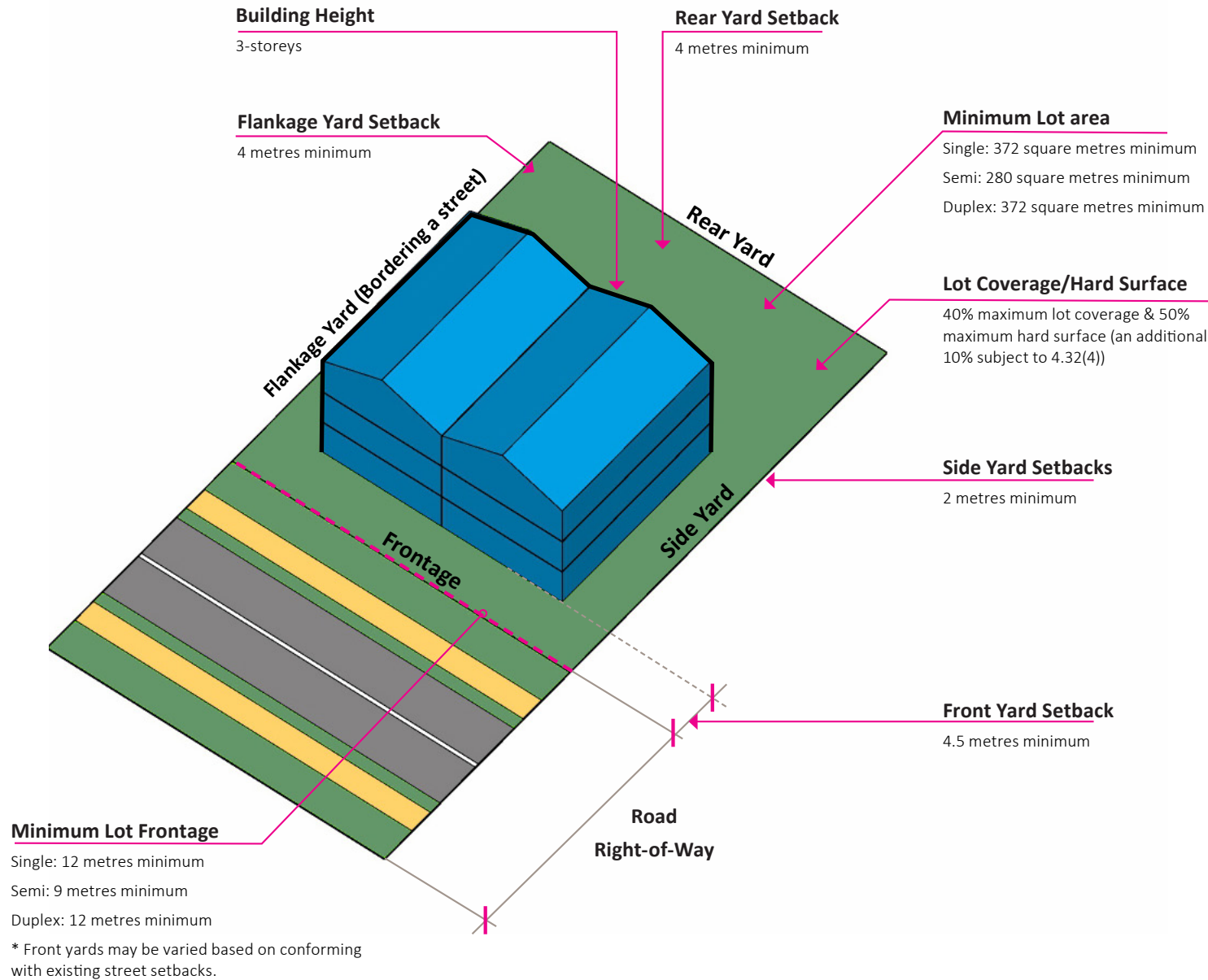


FIGURE 10.1 RL ZONE Built Form Standards

10.5 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the RL Zone

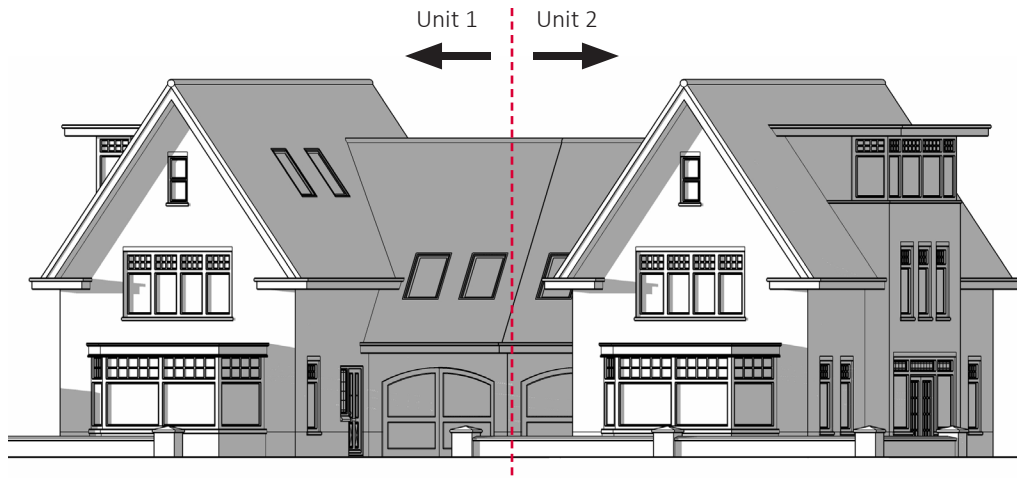


FIGURE 10.1 RL ZONE Semi-Detached Example (units divided horizontally)



FIGURE 10.3 RL ZONE Single Family with Detached Accessory Dwelling Unit Example



FIGURE 10.2 RL ZONE Duplex Example (units divided Vertically)



FIGURE 10.4 RL ZONE Single Family Addition Example

PART 11 Medium Density Residential (RM) Zone

11.1 INTENT

The RM zone is intended to permit a range of medium density residential development to a maximum of 8 dwelling units as-of-right, and additional units by Site Plan Approval up to a maximum height of 4 storeys. Unique and site specific conditions are considered by Development Agreement. Home based businesses and other uses such as short-term rentals in an owner-occupied dwelling unit are permitted subject to conditions of 4.17.

11.2 PERMITTED USES

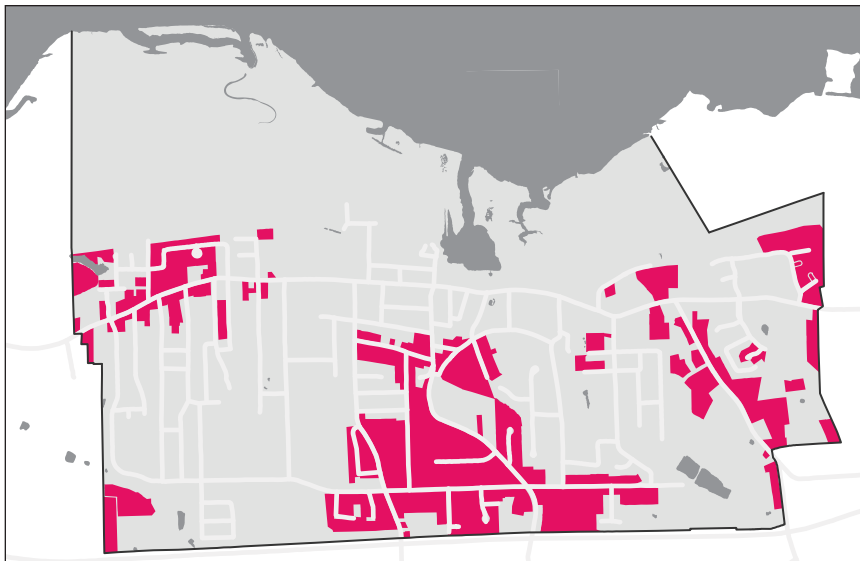
As-of-right, Site Plan and Development Agreement uses are shown in Table 8.1

11.3 RE-ZONING

Rezoning to the RH or MU zone will be considered subject to **policy 11.4.3** of the MPS.

11.4 AMENITY SPACE

- (1) 5 square metres of amenity space shall be provided for each dwelling unit (patio, balcony, rooftop deck, or landscape yard) to ensure resident comfort and privacy.
- (2) For buildings that exceed 24 units, a dedicated amenity room with no less than 110 square metres area shall be provided.



Other Provisions Apply

Part 4:
General Requirements for All Zones

Part 5:
Development Constraints

Part 6:
Parking & Loading Requirements

Part 7:
General Requirements for Signs

Part 8:
General Requirements for Neighbourhood Zones



Townhouse dwellings



Stacked Townhouses



Multi-unit dwellings



Additions



Home based business

11.5 BUILT FORM REQUIREMENTS

- (1) Townhomes shall not exceed 8 consecutive townhomes
- (2) Stacked townhomes shall not exceed 16 units.
- (3) Townhomes may be oriented perpendicular to the street as long as
 - (a) Provides a connected walkway from each door to the street
 - (b) Parking is in the rear or sideyard
 - (c) The building is located on one lot such that units are rental or condominium
- (4) Attached front yard garages are not permitted for townhomes or multi-unit buildings except for one single entry/exit to below-grade parking garage.
- (5) Parking for stacked townhomes shall be located in the rear or side yard of the of the units.
- (6) Port cochere's are permitted in the front yard no more than 6 metre wide and 24 metre long so long as no parking is attached to the through-way.
- (7) Building height shall not exceed 4-storeys by Site Plan Approval
- (8) A 2.5 metre streetwall stepback is required above the 3rd storey of no less than 60% of the facade.
- (9) The maximum building dimension in the RM zone shall not exceed 50m
- (10) Every minimum 12 metres of building length shall include a vertical articulation of no less than 300 millimetres for no less than half the height of the building in order to break up the horizontal scale of the building.

11.6 BUILT FORM STANDARDS

See Figure 11.1 for all built form standards in the RM Zone

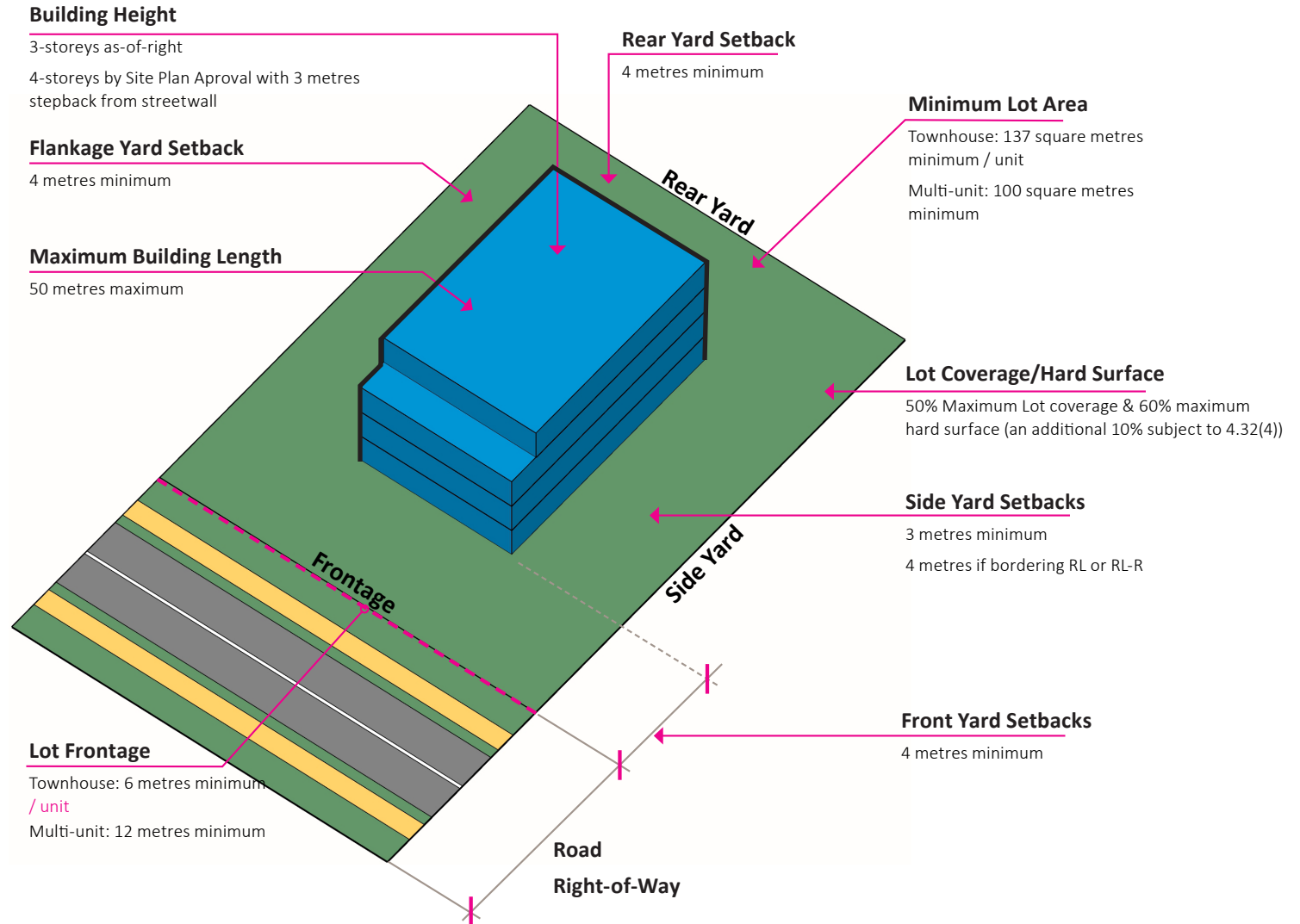


FIGURE 11.1 RM Zone Built Form Standards

11.7 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the RM Zone



FIGURE 11.1 RM ZONE Typical 3-unit Townhome



FIGURE 11.2 RM ZONE 8-unit Stacked Townhouse Example

PART 12 High Density Residential (RH) Zone

12.1 INTENT

The RH zone is intended to permit high density residential development up to 6 storeys by Site Plan Approval. Unique and site specific conditions are considered by Development Agreement. Home based businesses and other uses such as short-term rentals in an owner-occupied dwelling unit are permitted subject to conditions.

12.2 PERMITTED USES

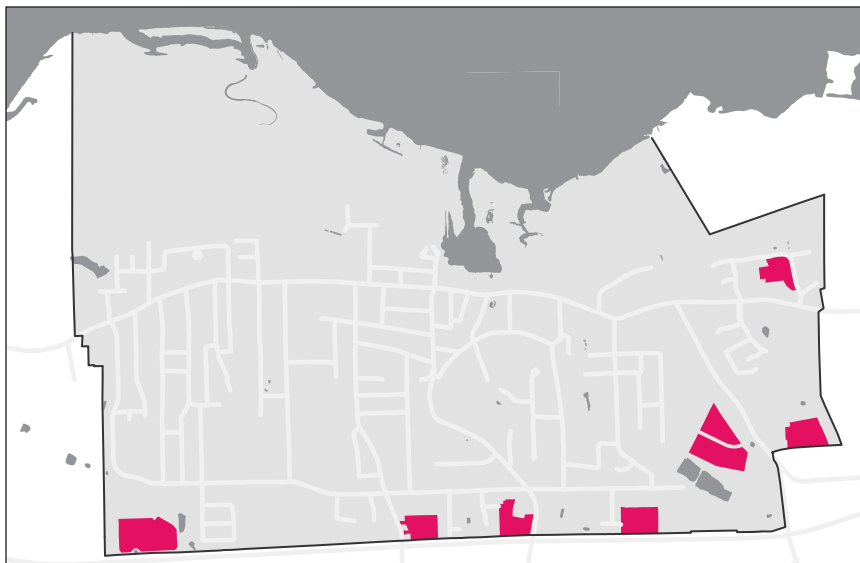
As-of-right, Site Plan and Development Agreement uses are shown in Table 8.1

12.3 RE-ZONING

Rezoning to the MU zone will be considered subject to **policy 11.4.3** of the MPS.

12.4 AMENITY SPACE

- (1) 5 square metres of amenity space shall be provided for each dwelling unit (patio, balcony, rooftop deck, or landscape yard) to ensure resident comfort and privacy.
- (2) For buildings that exceed 24 units, a dedicated amenity room with no less than 110 square metre area shall be provided.



Other Provisions Apply

Part 4:
General Requirements for All Zones

Part 5:
Development Constraints

Part 6:
Parking & Loading Requirements

Part 7:
General Requirements for Signs

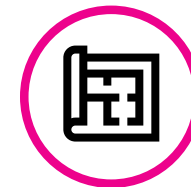
Part 8:
General Requirements for Neighbourhood Zones



Multi-unit dwellings



Home based business



Additions

12.5 BUILT FORM REQUIREMENTS

- (1) Building height shall not exceed 6-storeys by Site Plan Approval.
- (2) Streetwall Height shall not exceed 3-storeys
- (3) A 3 metre streetwall stepback is required above the 3rd storey of no less than 70% of the facade.
- (4) Attached garages shall not front onto the street frontage unless it provides access to an underground parking garage that does not exceed 25% of the main building frontage on any streetline to a maximum width of 6m.
- (5) All street-facing facades shall provide at least one horizontal or vertical facade articulation of no less than 300 millimetres for no less than 30% of the facade area.
- (6) Every minimum 12 metres of building length shall include a vertical articulation for no less than half the height of the building in order to break up the horizontal scale of the building.
- (7) Streetwalls shall stepback no less than 3m above the streetwall for no less than 70% of the streetwall.
- (8) The groundfloor height shall:
 - (a) Not be less than 4m high,
 - (b) Not be less than 3.5m high if no more than 20% of the floor slab is a stepped slab.
- (9) Parking is discouraged between the street and the building.
- (10) The main lobby and elevator core shall exit onto the street-front.
- (11) For multi-unit buildings, each grade-related unit shall include a patio door.
- (12) Parking for multi-unit buildings shall be located in the rear or side yard of the of the building.
- (13) Port cochere's are permitted in the front yard no more than 6 metres wide and 24 metres long so long as no more than 2 parking spaces are attached to the through-way.

12.6 BUILT FORM STANDARDS

See Figure 12.1 for all built form standards in the RH Zone

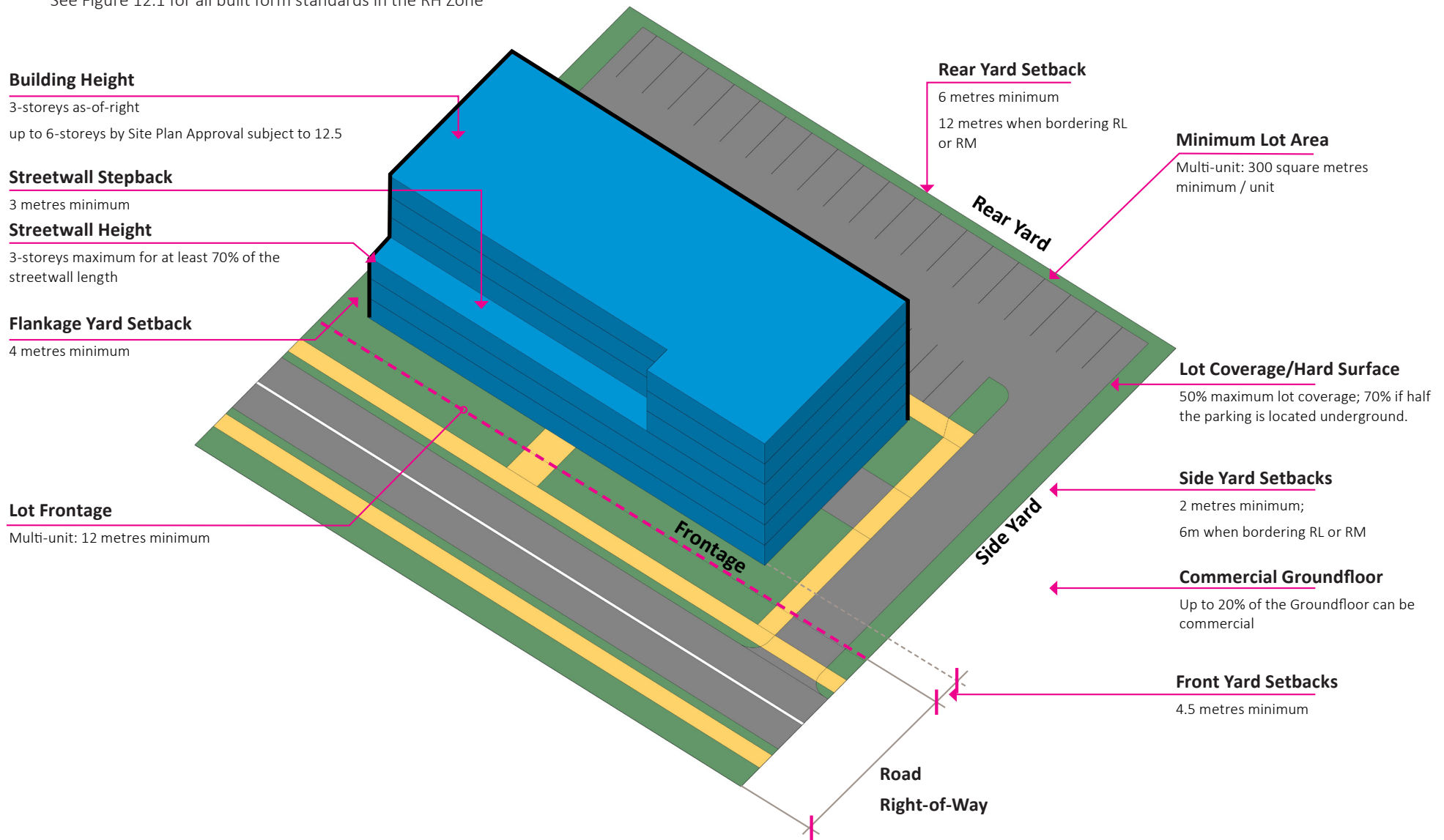


FIGURE 12.1 RH ZONE Built Form Standards

12.7 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the RH Zone



FIGURE 12.1 RH ZONE Multi-Unit Example



FIGURE 12.2 Mixed Use, with portion of groundfloor commercial & multi-units.

PART 13

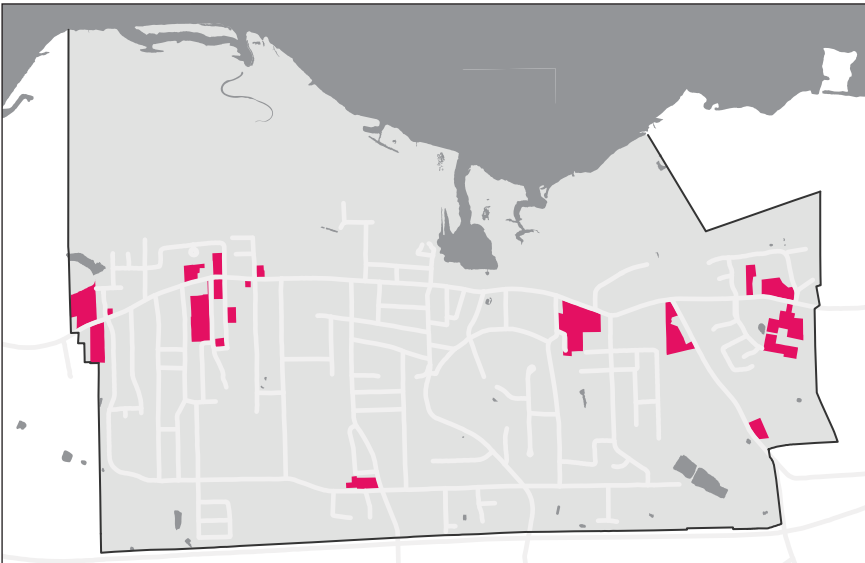
Mixed Use (MU) Zone

13.1 INTENT

The MU zone is intended to support existing uses and expand a range of mixed commercial, office and medium density residential uses up to 3-storeys in height by site plan approval and 4-storeys by Development Agreement. Short-term rentals are permitted subject to conditions. Council may consider unique and site-specific developments by development agreement.

13.2 PERMITTED USES

As-of-right, Site Plan and Development Agreement uses are shown in Table 8.1.



13.3 RE-ZONING

Rezoning is not permitted.

13.4 AMENITY SPACE

- (1) 5 square metres of amenity space shall be provided for each dwelling unit (patio, balcony, rooftop deck, or landscape yard) to ensure resident comfort and privacy.
- (2) For buildings that exceed 24 units, a dedicated amenity room with no less than 110 square metre area shall be provided. Built Form Requirements

Other Provisions Apply

- Part 4: General Requirements for All Zones
- Part 5: Development Constraints
- Part 6: Parking & Loading Requirements
- Part 7: General Requirements for Signs
- Part 8: General Requirements for Neighbourhood Zones



Multi-unit dwellings



Commercial Use



Additions



Accessory Buildings



Home based business

13.5 BUILT FORM REQUIREMENTS

- (1) Building height shall not exceed 4-storeys by Site Plan Approval.
- (2) Streetwall Height shall not exceed 3-storeys.
- (3) A 3 metres streetwall stepback is required above the 4th storey of no less than 70% of the facade.
- (4) Attached garages shall not front onto the street frontage unless it provides access to an underground parking garage that does not exceed 25% of the main building frontage on any streetline to a maximum width of 6 metres.
- (5) All street-facing facades shall provide at least one horizontal or vertical facade articulation of no less than 300 millimetres for no less than 30% of the facade area.
- (6) Every minimum 12 metres of building length shall include a vertical articulation for no less than half the height of the building in order to break up the horizontal scale of the building.
- (7) Streetwalls shall stepback no less than 3 metres above the streetwall for no less than 70% of the streetwall.
- (8) The groundfloor height shall:
 - (a) Not be less than 4 metres high,
 - (b) Not be less than 3.5 metres high if no more than 20% of the floor slab is a stepped slab.
- (9) Parking is not permitted between the street and the building.
- (10) The main lobby and elevator core shall exit onto the street-front.
- (11) For multi-unit buildings, each grade-related unit shall include a patio door.
- (12) Parking for multi-unit buildings shall be located in the rear or side yard of

the of the building.

- (13) Port cochere's are permitted in the front yard no more than 6 metres wide and 24 metres long so long as no more than 2 parking spaces are attached to the through-way.

13.6 BUILT FORM STANDARDS

See Figure 13.1 for all built form standards in the MU Zone

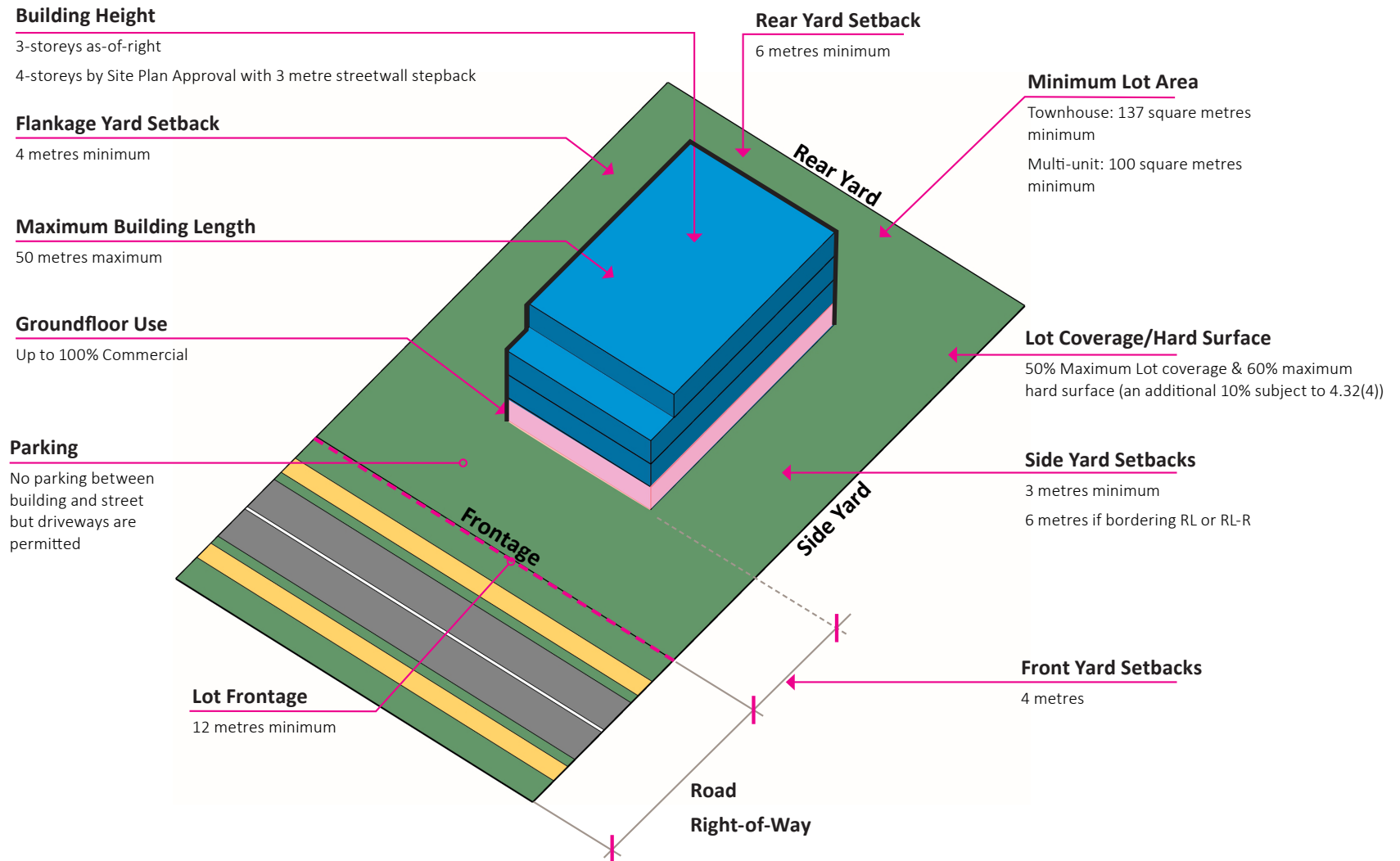


FIGURE 13.1 MU ZONE Built Form Standards

PART 14 Comprehensive Development District (CDD)

14.1 INTENT

The CDD zone is intended to enable the potential future expansion of land or other scenarios where lands may require further planning beyond the policies of the Land Use By-law.

14.2 USES PERMITTED AS-OF-RIGHT

Notwithstanding Table 8.1, no development permit shall be issued for any use in the CDD zone except for one or more of the following uses:

- (a) Existing agricultural uses
- (b) Areas where a Development Agreement has already been approved

14.3 USES PERMITTED BY SECONDARY PLANNING STRATEGY

- (1) Notwithstanding Table 8.1, only after completion of a Secondary Planning Strategy and amendment to the Land Use Bylaw may CDD areas be re-zoned.

- (2) Secondary Planning Strategies shall be subject to Schedule D of this bylaw.

14.4 USES PERMITTED BY DEVELOPMENT AGREEMENT

Once a secondary planning strategy has been completed, new developments shall meet the following requirements:

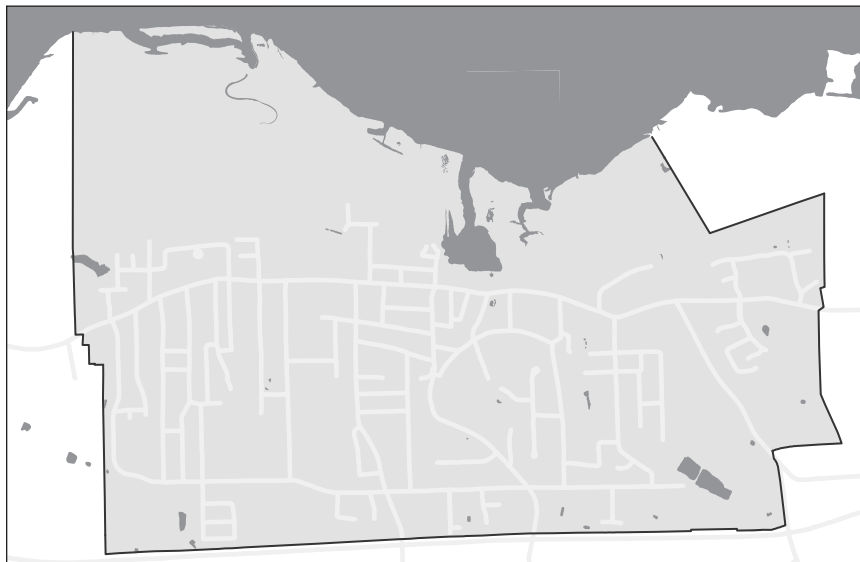
- (1) Residential densities on CDD parcels shall be a minimum of 10 units per net acre.
- (2) 4th Storey Bonus Criteria as per Section 4.15.6 shall be utilized .

14.5 RE-ZONING

Rezoning is not permitted unless a Secondary Planning Strategy is completed.



**Comprehensive
development**



Other Provisions Apply

Part 4:

General Requirements for All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 8:

General Requirements for Neighbourhood Zones

PART 15 General Requirements For Downtown Zones

15.1 PERMITTED USES

- (1) In this Bylaw any use not listed as a permitted use in a zone is prohibited in that zone unless otherwise indicated.
- (2) Where a permitted use within any zone is defined in this Bylaw the uses permitted in the zone include any similar uses that satisfy such definition except where a definition specifically excludes any similar use.
- (3) Municipal offices, buildings, and infrastructure shall be permitted in any zone.

15.2 DOWNTOWN DESIGN REQUIREMENTS

- (1) A development permit will not be issued for any new main building in the Downtown Design Guidelines Area as identified in **Schedule C** unless the proposed structure conforms generally to the urban design requirements set out in the Downtown Design Guidelines (Schedule F).

15.3 COVERED PARKING GARAGES

- (1) Covered parking stalls are not permitted on the frontage of any building except by Development Agreement (Figure 15.3).
- (2) Covered parking is permitted in the rear of the building on the groundfloor so long as the street frontage is active and not blank (Figure 15.4).
- (3) A 6 metre wide (maximum) parking garage entry is permitted on the groundfloor to access underground parking (Figure 15.4)

TABLE 15.1 Downtown Land Use Table

APPROVAL PROCESS:

P = Permitted as-of-right

PC = Permitted with Conditions (refer to general conditions)

SP = Site Plan Approval

DA = Development Agreement

- = Not Permitted

Commercial Zones	DT
<i>Accommodations</i>	
Hostels	P
Hotels	P
Short term Rentals	PC
<i>Educational Facilities</i>	
Daycare Facilities (over 4 persons)	P
Commercial Schools	SP
<i>Food and Beverage</i>	
Bakeries	P
Beverage Room (conversion from existing restaurant)	P
Beverage Room (New)	SP
Cafes	P
Catering Establishment (over 80 square metres)	P
Commercial Brewery or other Craft Beverage Facility (Stand Alone)	SP
Craft Beverage Facility (Accessory Use only under 500 square metres in floor area.)	P
Lounges (conversion from existing restaurant)	P
Lounges (New)	SP
Nano Brewery	P
Restaurants	P
Tasting Rooms	P
Parks and Playgrounds, Public Washrooms, Community Gardens, & Historic Sites	P

TABLE 15.2 Downtown Land Use Table

APPROVAL PROCESS:

P = Permitted as-of-right,
 PC = Permitted with Conditions
 (refer to general conditions)
 SP = Site Plan Approval
 DA = Development Agreement
 - = Not Permitted

Commercial Zones		DT
<i>General Commercial</i>		
Art Galleries		P
Autobody Repair Shop (excluding Schedule A1 - Area A)		DA
Automobile Rental		SP
Automotive Service Centre (excluding Schedule A1 - Area A)		DA
Craft Workshops		P
Clubs – Private and Public		P
Existing Uses		P
Farm Markets		P
Financial Institutions		P
Institutional Uses		P
Laundromats		P
Marinas and Wharves		P
Medical Clinics		P
Nursing Homes		P
Offices & Professional Services		P
Stand Alone Parking Lots and Structures		SP
Personal Services		P
Places of Assembly		P
Retail Stores		P
Services Industries		SP
Studios		P
Taxi Stands		SP
Veterinary Clinics		P
<i>Mixed Use Residential and Residential</i>		
Multi-unit dwellings above the groundfloor		P
Groundfloor Residential Use (Schedule A1 - Area A)		PC
<i>Large Format Commercial (Schedule A1 - Area B Only)</i>		
Big Box Retail		P
Expansion of Existing Uses		P
Light Industrial Uses		SP
Manufacturing, Processing and Assembly (accessory to a main use)		SP
Warehousing or bulk storage (maximum 1,000 square metres)		SP

- (4) Where possible, parking garage entries should exit from the rear or side of a building.
- (5) Parking garage entries which cross a sidewalk must be designed for safe passage of pedestrians.

15.4 DRINKING ESTABLISHMENTS

All establishments licensed under the Nova Scotia Liquor Control Act shall be subject to the following:

- (1) The Lounge component of the establish shall be restricted to a closing hour of 2:00 am.
- (2) Adult entertainment shall be prohibited.
- (3) Parking lots and driveways for the use of patrons shall be subject to the buffering provisions of this by-law.

15.5 LANDSCAPING

- (1) All lot areas in the DT Zone which have not been developed as buildings, parking lots or walkways shall be fully landscaped using species specially selected for the microclimatic conditions of the site including shade or sun tolerant species of grass, groundcover, shrub, vine or tree.
- (2) Screening of fuel tanks, air conditioning systems, storage areas, solid waste areas, loading and parking areas is required. Landscape plans prepared by a qualified professional shall indicate how these areas will be screened and landscaped.
- (3) Any DT zone property that fronts on Main Street or Front Street shall include hard surfacing (concrete, stone, pavers) fully between the building and the sidewalk to reinforce the urban conditions of Main Street.



FIGURE 15.3 Groundfloor frontage parking restrictions

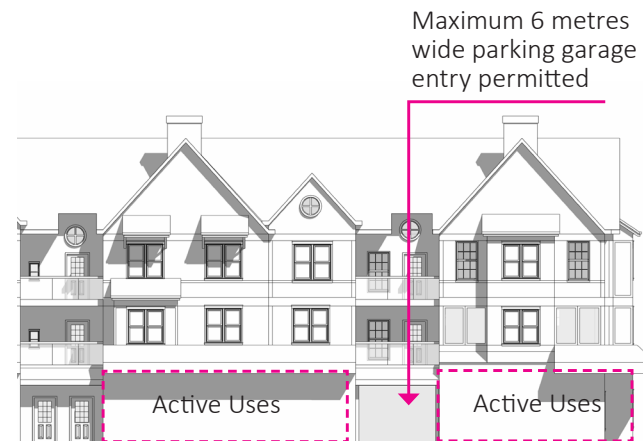


FIGURE 15.4 Parking Garage entry permitted on street frontage

- (4) All landscaping shall be installed within 1 year of the issuance of an occupancy permit or a temporary occupancy permit.

15.6 SITE PLAN APPROVAL CRITERIA

The following Site Plan criteria apply to all new development that is permitted through the Site Plan Approval process in the Downtown (DT) Zone. No development permit shall be issued for development that is inconsistent with these criteria. All development subject to Site Plan Approval must also meet all applicable zone standards and all other requirements of this Bylaw.

- (1) The location of new structures, driveways, or parking and loading facilities on the lot shall minimize negative impacts on any abutting residential zones, including noise, dust, fumes, lighting, shadows, or other nuisance or inconvenience to neighbouring properties;
- (2) The type, location, and height of walls, fences, hedges, trees, shrubs, ground cover or other landscaping elements which is necessary to protect and minimize negative land use impact on abutting residential zoned properties;
- (3) The location of pedestrian walkways, and/or related infrastructure, shall be provided to link public sidewalks and parking areas to entrances of all primary buildings;
- (4) The type and location of outdoor lighting is designed to light the structure, driveways and pedestrian infrastructure, but shall not be directed onto neighbouring residential properties;
- (5) The location of facilities for the storage of solid waste provides for maximum separation from abutting residential and park zones;
- (6) The location of all existing easements shall be identified;
- (7) The grading or alteration in elevation or contour of the land

shall minimize drainage and/or sedimentation on neighbouring properties;

- (8) The management of storm and surface water is addressed, and associated plans are approved by the Town Engineer;
- (9) The type, location number and size of signs or sign structures do not negatively alter the appearance of the streetscape or neighbourhood;
- (10) All signage shall be designed and constructed according to the signage requirements listed in Part 7;
- (11) The Development Officer may vary any of the prescriptive dimensional requirements by up to 10 percent of the requirements to allow some flexibility to accommodate physical anomalies of a site, so long as the intent of the particular requirement is not compromised.

15.7 SIDEWALK CAFÉ BYLAW REQUIREMENTS

(1) Permitted Zone and Uses

- (a) Sidewalk cafes are only permitted in the Downtown (DT) zone
- (b) Sidewalk cafes are only permitted as an accessory use of a food and beverage establishment such as a restaurant, cafe or pub.
- (c) Sidewalk cafes may be used for restaurant use (including licensed restaurants) and may be used for lounge use to the extent permitted by other relevant requirements.

(2) Hours of Operation

- (a) Sidewalk cafes may not be installed before May 1.
- (b) Sidewalk cafes must be removed by November 7.

- (c) A sidewalk cafe shall be operated for no longer than the operating hours of the principal use to which it is an accessory.
- (d) Once the Sidewalk Cafe is removed for the season the street, sidewalk and municipal property must be restored to its original condition (to the satisfaction Development Officer).
- (3) **Cafe Dimensions and Design**
- (a) Sidewalk cafes shall not extend onto the frontage of abutting property owners, except that an extension of a maximum 3.7 metres (12 feet) may be granted to each side of a sidewalk cafe, provided written permission has been provided by the adjacent property owner. In no case shall the total street length of a sidewalk cafe exceed 22 metres (72 feet).
- (b) Sidewalk cafes shall not extend beyond the edge of the existing sidewalk (inside edge of curb).
- (c) All objects must be contained within the area approved for the sidewalk cafe.
- (d) Any lighting of the sidewalk cafe shall be of a temporary nature and shall not project light onto adjacent properties nor conflict with traffic control indicators.
- (e) Landscaping of the sidewalk cafe is encouraged however this should be of a temporary nature, unless otherwise approved by the Development Officer. Plant material must be contained within the designated sidewalk cafe area.
- (f) All umbrellas shall be located entirely within the approved sidewalk cafe area.
- (g) The elements of the sidewalk cafe, including: temporary sidewalks, landscaping, fencing and furnishings, shall be designed and finished to a standard that is consistent with general appearance of the existing downtown streetscapes and buildings.
- (h) All elements shall be maintained in good repair and must be free of hazards.
- (i) Wood treated with Chromated Copper Arsenate (CCA - and commonly referred to as pressure treated lumber) shall not be used in the construction of the temporary sidewalk or any other elements of the sidewalk cafe construction, except that CCA wood that formed part of the sidewalk cafe materials approved under a previous sidewalk cafe permit may be reused.
- (j) The temporary sidewalk shall be constructed in such a way as not to block or otherwise interfere with the street drainage system.

(4) **Temporary sidewalks**

- (a) The Owner must provide a temporary sidewalk in the parking area adjacent to the sidewalk cafe to preserve free flow of pedestrian traffic.
- (b) The temporary sidewalk is a requirement of the sidewalk cafe application and must conform with the specifications outlined below:
 - i) The temporary sidewalk must be no less than 1.8 metres (6 feet) wide and no more than 2.1 metres (7 feet) wide.
 - ii) The base of the temporary sidewalk must provide a smooth, level transition between the existing sidewalk and the temporary sidewalk surface.
 - iii) When wooden decks are used to create the temporary sidewalk a non-slip surface must be applied.
 - iv) Street traffic shall be blocked at each edge of the temporary sidewalk in both directions by triangular shaped planters with a minimum weight of 1000kg and minimum height of 60 centimetres. The planters shall be maintained with healthy plant materials at all times through the duration of the sidewalk cafe.
 - v) A fence or barrier must be installed along the street side of the temporary sidewalk to delineate the edge of the structure and prevent pedestrians from entering the street.
 - 1) The fence or barrier shall be between 107 centimetres (3.5 feet) and 120 centimetres (4 feet) in height.
 - 2) Fencing may provide partial visual screen but shall not be opaque

- vi) When a temporary sidewalk is proposed, the Traffic Authority will review the proposal to ensure that vehicles and pedestrians may continue to move safely and that barrier free access is provided.
- vii) Where there are sidewalk cafes adjacent to one

- (5) Another the required temporary sidewalk shall be connected in such a way to provide safe barrier free access as approved by the traffic authority.

(6) **Waste, Storage and Maintenance**

- (a) The sidewalk cafe Owner shall maintain the sidewalk cafe area, the temporary sidewalk area, and the immediately adjacent area, in a clean and safe condition, free of hazards at all times.
- (b) Waste receptacles and work-stations should be located along the building wall .
- (c) The Owner shall ensure any refuse originating in the cafe is removed from the street right-of-way.

(7) **Building Access**

The Owner shall maintain a minimum width of 120 centimetres (4 feet) of unobstructed (barrier free) access to the entrance of the building.

(8) **Utility Access**

- (a) The Town of Wolfville and public utility agencies retain the right of entry to the approved Sidewalk Cafe area and the temporary sidewalk for the installation, maintenance and repair of pipes, cables, wires, poles, hydrants and other elements as necessary.
- (b) In the case of emergency repairs of utility infrastructure noted in clause "a" above, entry may be made without notice. For scheduled work, a minimum notice of 48 hours will be given.

- (c) When access is required by the Town or a Public Utility for a purpose such as those noted in "a" above, the owner shall remove the sidewalk cafe improvements and reinstalls them at the Owner's expense.

(9) **Emergency Access**

The Town of Wolfville retains the right of entry to the approved sidewalk cafe for emergency vehicle access.

(10) **Application and Approval Process**

No Owner shall establish or maintain a sidewalk cafe without a permit granted under this Bylaw.

(11) **Permit Fees**

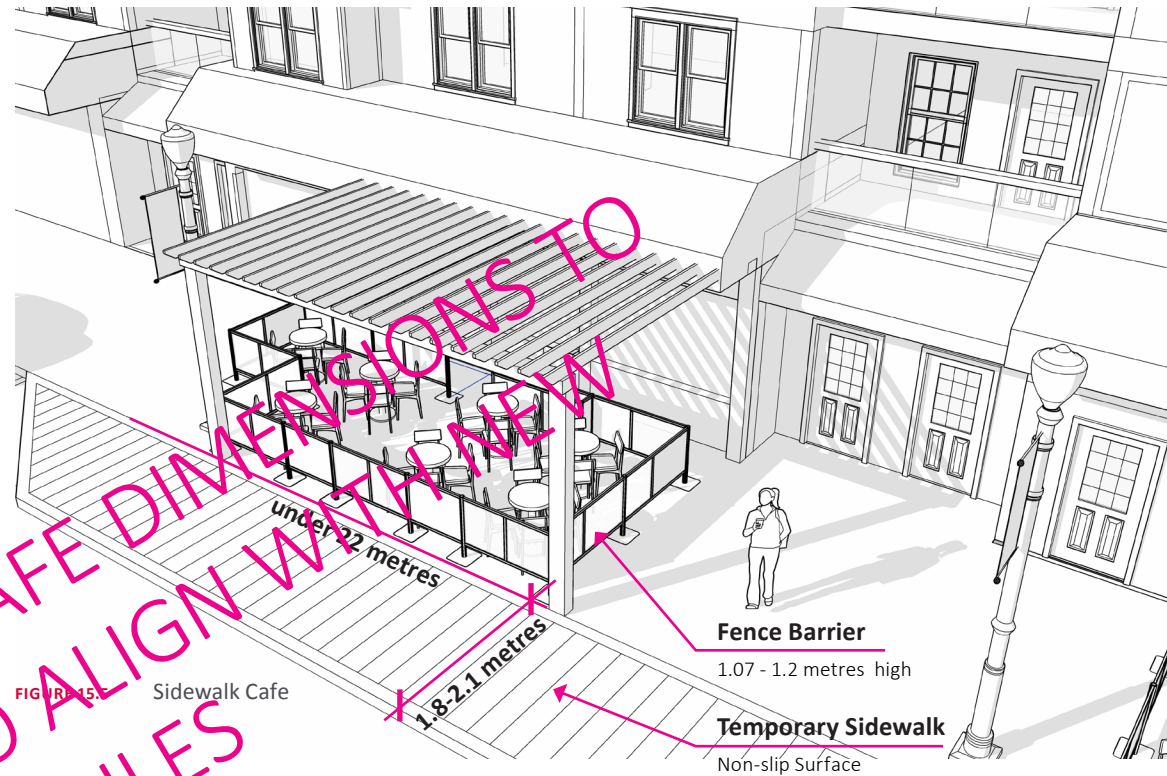
The Owner of a Sidewalk Cafe shall pay an annual permit fee as per the Fees Policy of Council.

(12) **Liability**

- (a) The Owner of a sidewalk cafe must carry a minimum of \$2,000,000 liability insurance for the operation of the sidewalk cafe and name the Town as co-insured and must indemnify the Town of Wolfville and save it harmless from any and all claims of injury to persons or damage to property attributable in whole or in part to the existence, location and operation of a sidewalk cafe in the public right-of-way.
- (b) The Town of Wolfville shall be notified 30 days before the cancellation of this insurance.

(13) **Minor Variations**

The Development Officer may vary any of the prescriptive dimensional sidewalk cafe requirements by up to 10 percent of the requirements to allow some flexibility to accommodate physical anomalies of a site, so long as the intent of the particular requirement is not compromised.



PART 16 Downtown Zone (DT)

16.1 INTENT

The DT zone is intended to permit a wide range of downtown commercial uses as well as residential mixed-uses and residential-only uses. Council may consider unique and site-specific developments by development agreement.

16.2 PERMITTED USES

As-of-right, Site Plan and Development Agreement uses are shown in Table 15.1 and 15.2.

16.3 RE-ZONING

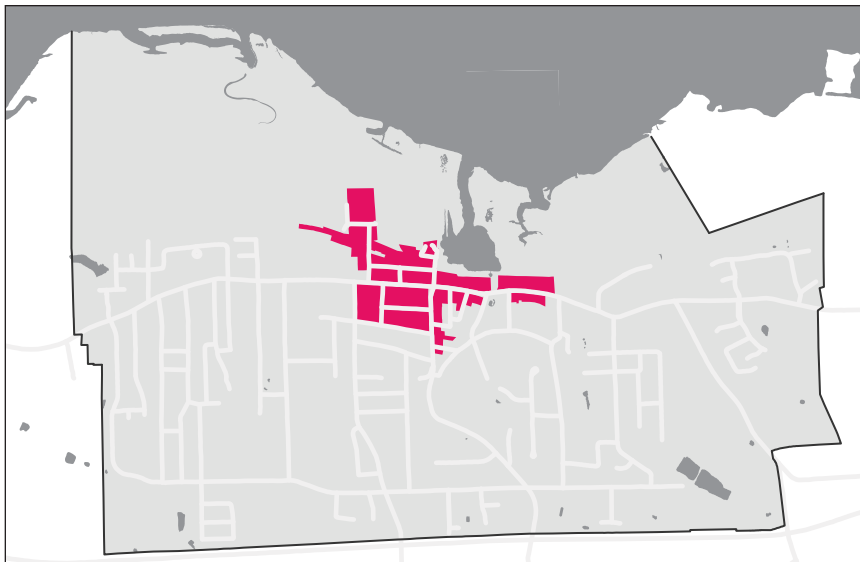
Rezoning is not permitted in the DT Zone

16.4 DOWNTOWN DESIGN REQUIREMENTS

- (1) A development permit will not be issued for any new main building in the Downtown Design Guidelines Area as identified in **Schedule C** unless the proposed structure conforms generally to the urban design requirements set out in the Downtown Design Guidelines (Schedule F).

16.5 CONDITIONAL USES (SCHEDULE A1)

- (1) In the **Schedule A1 - Area A** (See Figure 15.12) of the DT Zone, the following conditions are permitted



Other Provisions Apply

Part 4:
General Requirements for All Zones

Part 5:
Development Constraints

Part 6:
Parking & Loading Requirements

Part 7:
General Requirements for Signs

Part 14:
General Requirements for Commercial Zones



Downtown Commercial



Accommodations



Food & beverage



Detached Residential with Conditions



Office & professional

- (a) Residential only or residential groundfloor uses are permitted in Area A.
 - (b) No more than one driveway and 2 parking spaces are permitted between the building and the street.
 - (c) Nano breweries, tasting rooms, and other alcohol related uses are prohibited on Acadia Street.
 - (d) The Development Officer may use the Design Review Working Group for any aspect of an application.
- (2) In the Schedule A1 - Area B (See Fig 15.12) of the DT Zone, the following conditions are permitted
 - (a) Parking is permitted between the street and the building
 - (3) In the Schedule A1 - Area C (See Fig 15.12) of the DT Zone, the following conditions are permitted
 - (a) Development is permitted by Site Plan in Area C where development is generally consistent with the Rail Line Secondary Plan.
 - (4) In the Schedule A1 - Area D (See Fig 15.12) of the DT Zone, the following conditions are permitted
 - (a) Development is permitted by Development Agreement, with attention to Flood Control and ongoing adaptation work.

16.6 BUILT FORM REQUIREMENTS

- (1) The first storey facade facing a street must feature a minimum of 60% windows and doors in the DT Zone and 30% windows and doors in the **Schedule A1 - Area A** area.
- (2) Buildings on corner lots may reduce the groundfloor window and door requirement to 30% along sidestreets.
- (3) Buildings in the **Schedule A1 - Area A** Area are not subject to 15.5(1) and

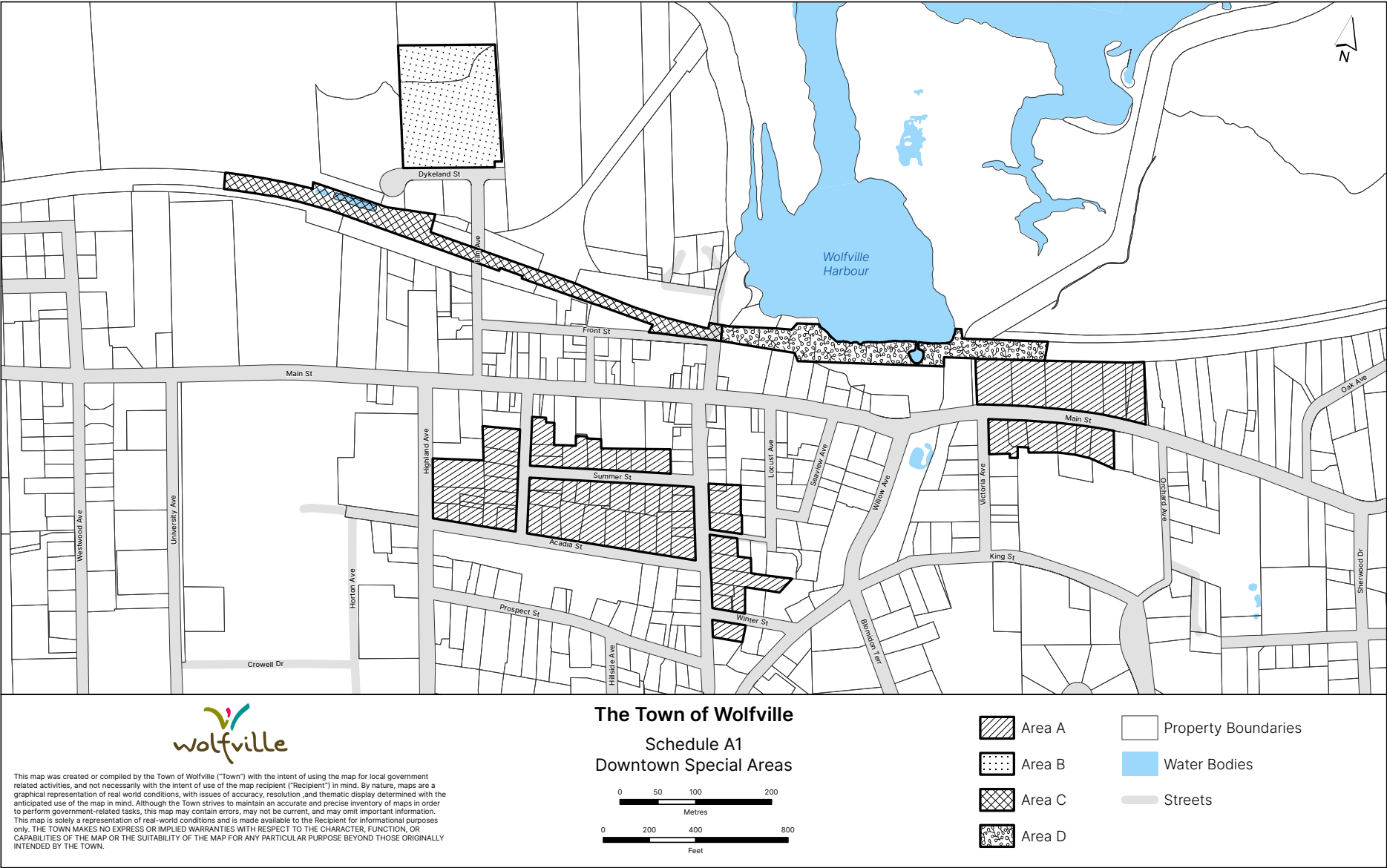
15.5(2) unless more than 50% of the groundfloor is designated a commercial use.

- (4) a 2.5 metre streetwall stepback is required for no less than 70% of the streetwall building frontage above the 3rd storey.
- (5) Buildings with frontages that exceed 15 metres shall demonstrate vertical and horizontal articulations of no less than 150 millimetres for no less than 25% of the frontage to reinforce the intent of the architectural style being proposed. The intended architectural style must be defensible by description and by design to the Development Officer.
- (6) All street fronting doors shall be accessible.
- (7) The maximum Building Dimension in the DT Zone is 50 metres; excepting **Schedule A1 - Area B**
- (8) The maximum permitted building height in the DT zone shall be 4 storeys.
- (9) Notwithstanding Subection 15.9(8), a 5th storey may be considered by development agreement in the DT zone, subject to the bonusing criteria in clause 15.9(9)(a).
 - (a) The bonus storey will be considered by Development Agreement if a suitable public benefit can be granted. For a public benefit to be considered suitable, a minimum of 50% of all units in the development must be owner or operated by a non-market housing provider.
 - (b) Suitable public benefits include non-market housing.

16.7 BUILT FORM STANDARDS

See Figure 16.1 for all built form standards in the DT Zone

FIGURE 16.10 Schedule A1



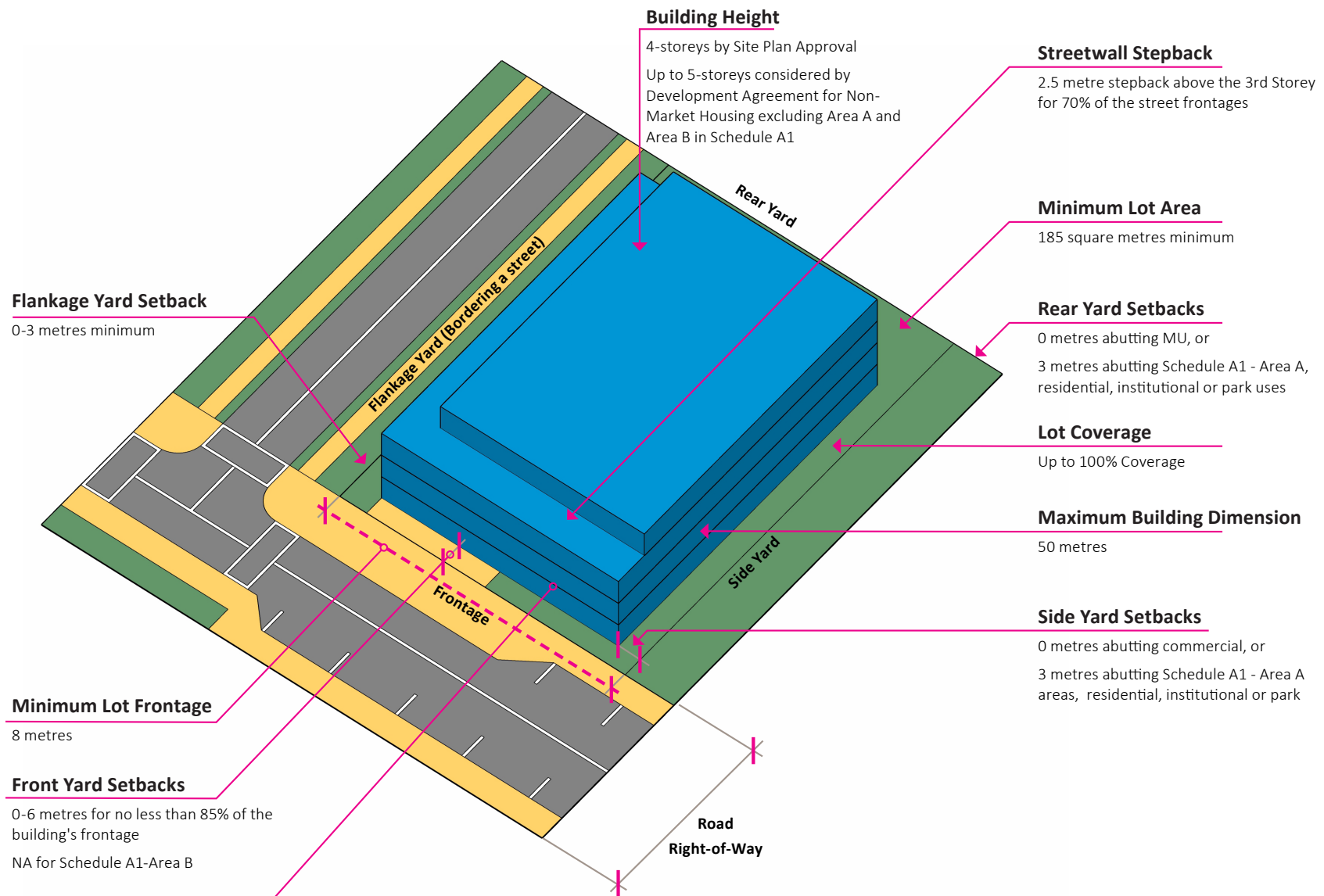


FIGURE 16.1 DT ZONE Built Form Standards

16.8 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the DT Zone



FIGURE 16.2 DT Modern Example



FIGURE 16.3 DT Traditional Corner Lot Example



FIGURE 16.5 Area A - Neighbourhood Commercial Mixed-Use Infill Example



FIGURE 16.4 Area A - Traditional Home Conversion (live-work) Example

PART 17 General Requirements For Institutional, Parks & Open Space, and Agricultural Zones

17.1 PERMITTED USES

- (1) In this Bylaw any use not listed as a permitted use in a zone is prohibited in that zone unless otherwise indicated.
- (2) Where a permitted use within any zone is defined in this Bylaw the uses permitted in the zone include any similar uses that satisfy such definition except where a definition specifically excludes any similar use.
- (3) Municipal offices, buildings, and infrastructure shall be permitted in any zone.

17.2 DESIGN GUIDELINE AREAS & REGISTERED HERITAGE PROPERTIES

- (1) New buildings in the Design Guideline Areas shall be subject to Site Plan Approval.
- (2) Additions to buildings in the Design Guidelines Areas greater than 25% of the floor area shall be subject to site plan approval
- (3) Additions to registered heritage buildings greater than 10% of the floor area shall be subject to site plan approval

17.3 ABUTTING YARD REQUIREMENTS

Where an Institutional zone, Agricultural zone, Parks & Open Space zone abuts a residential zone, the following restrictions shall apply to the abutting yard:

- (1) In the case of a side yard, the minimum yard requirement shall be 4 metres.
- (2) In the case of a rear yard, the minimum requirements shall be 6 metres.

17.4 ABUTTING PARKING REQUIREMENTS

Where an Institutional zone abuts a residential zone, no parking shall be permitted within 3 metres of the lot line. This requirement may be reduced to 1.5 metres provided a double sided opaque wooden fence or landscape screening which provides a similar visual barrier a minimum of 1.5 metres in height is provided along the lot line.

17.5 SITE PLAN APPROVAL

The following Site Plan criteria apply to all new development that is permitted through the Site Plan Approval process in the Institutional (I-1 and I-2) Zones. No development permit shall be issued for development that is inconsistent with these criteria. All development subject to Site Plan Approval must also meet all applicable zone standards and all other requirements of this Bylaw.

- (1) The location of new structures on the lot shall minimize negative impacts on the surrounding neighbourhood, including noise, dust, fumes, lighting, shadows, or other nuisance or inconvenience to neighbouring properties;

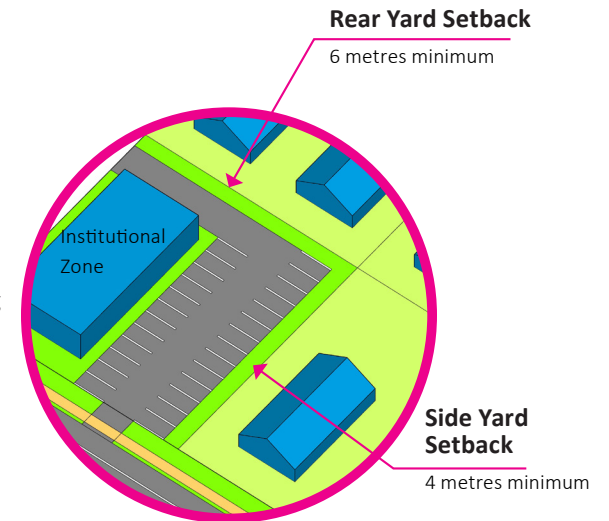


FIGURE 17.1 Institutional Sideyards and rear yards abutting residential

TABLE 17.2 Institutional, Agricultural, Parks and Open Space Land Use Table

APPROVAL PROCESS:

P = Permitted as-of-right,

PC = Permitted with Conditions

(refer to general conditions)

SP = Site Plan Approval

DA = Development Agreement

Institutional	I-1	I-2	P-1	P-2	A
Community Centres	P				
Cultural Facilities	P				
Educational Facilities (Public)	P				
Emergency Services	P				
Existing Uses	P	P	P	P	P
Health Centres	P				
Homeless Shelters	P				
Hospitals	P				
Libraries	P				
Museums	P				
Places of Assembly	P	P			
Places of Worship	P				
Residential Uses Accessory to Places of Worship	P				

Institutional University	I-1	I-2	P-1	P-2	A
University Buildings (under 5-storeys)		SP			
University Buildings (5 or more storeys)		DA			
Classrooms, Laboratories and Similar Educational and Research Facilities		P			
Community Gardens		P	P	P	
Convention Facilities (accessory to other facilities)		P			
Cultural Facilities	P	P			
Existing Uses	P	P	P	P	P

Agriculture	I-1	I-2	P-1	P-2	A
Agricultural Uses					P
Existing Uses	P	P	P	P	P
Structures accessory to a main agricultural use in excess of 20 square metres or 4.5 metres in height					DA
Renewable energy (excluding wind turbines)					DA

Institutional University	I-1	I-2	P-1	P-2	A
Food service Facilities (for the primary use of students)		P			
Places of Assembly	P	P			
Place of Worship	P	P			
Residential Uses (for the primary use of housing students and faculty)		P			
Retail Shops (for the primary use of students)		P			
Sport and Recreation Facilities		P	P	P	
Student Services Buildings		P			
University Offices		P			

Parks and Open Space	I-1	I-2	P-1	P-2	A
Active Transportation Uses (walking trails and bicycle facilities)			P	P	P
Community Gardens	P	P	P	P	P
Community Ovens or similar use			P	P	
Existing Uses	P	P	P	P	P
Historic Sites	P	P	P	P	P
Interpretation Centres	P		P	P	
Linear Parks			P	P	
Museums	P		P		
Non-recreational Open Spaces			P	P	
Occasional, incidental, temporary outdoor or indoor markets, bake sales, flea markets and vegetable and produce markets and similar uses with appropriate permissions, licensing and permits			P	P	
Parks and Playgrounds	P	P	P	P	P
Pavilions and Bandstands			P		
Public Washrooms	P	P	P	P	P
Recreational Facilities			P	P	
Retail – Associated with Municipal Facilities	P		P		
Sports Fields		P	P	P	

- (2) The location of off-street parking and loading facilities shall minimize negative impacts on the surrounding neighbourhood, including traffic, noise, dust, fumes, lighting, or other nuisance or inconvenience to neighbouring properties;
- (3) The location, number and width of driveways are designed to prevent traffic, noise, dust, fumes, congestion, or other nuisance and inconvenience in the area and minimize negative impacts on the surrounding neighbourhood;
- (4) The type, location, and height of walls, fences, hedges, trees, shrubs, ground cover or other landscaping elements which is necessary to protect and minimize negative land use impact on neighbouring properties;
- (5) Existing vegetation shall be retained where the vegetation is healthy and helps to minimize negative impacts on the surrounding neighbourhood;
- (6) The location of pedestrian walkways, and/or related infrastructure, shall be provided to link public sidewalks and parking areas to entrances of all primary buildings;
- (7) The type and location of outdoor lighting is designed to light the structure, driveways and pedestrian infrastructure, but shall not be directed onto neighbouring properties;
- (8) The location of facilities for the storage of solid waste provides for maximum separation from residential development and public areas;
- (9) The location of all existing easements shall be identified;
- (10) The grading or alteration in elevation or contour of the land shall minimize undue erosion and/or sedimentation, and other negative impacts on neighbouring properties;
- (11) The management of storm and surface water is addressed, and associated plans are approved by the Town Engineer;
- (12) The type, location number and size of signs or sign structures do not negatively alter the appearance of the streetscape or neighbourhood;
- (13) All signage shall be designed and constructed according to the signage requirements listed in Part 7;
- (14) Developments shall have regard to the Design Guidelines Schedules F and G, where applicable.
- (15) The Development Officer may vary any of the prescriptive dimensional requirements by up to 10 percent of the requirements to allow some flexibility to accommodate physical anomalies of a site, so long as the intent of the particular requirement is not compromised.

PART 18 Institutional General (I-1) Zone

18.1 INTENT

The I-1 zone is intended to permit a range of institutional uses such as churches and public government facilities. Council may consider unique and site-specific developments by development agreement.

18.2 PERMITTED USES

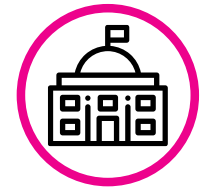
As-of-right, Site Plan and Development Agreement uses are shown in Table 19.1

18.3 RE-ZONING

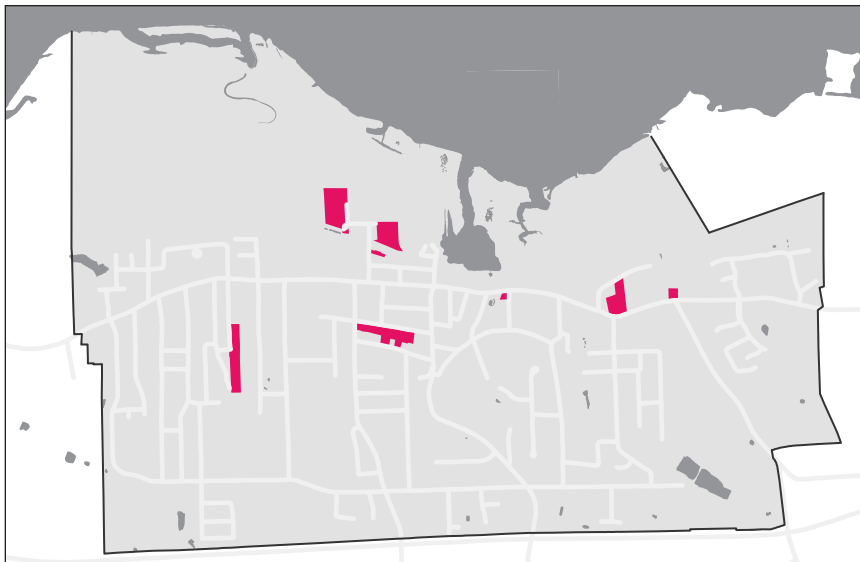
Rezoning to any other zone shall be considered subject to **policy 11.4.3** of the MPS.



Churches



Government facilities



Other Provisions Apply

Part 4:

General Requirements for All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 19:

General Requirements for Institutional Zones

18.5 BUILT FORM STANDARDS

See Figure 20.1 for all built form standards in the I-1 Zone.

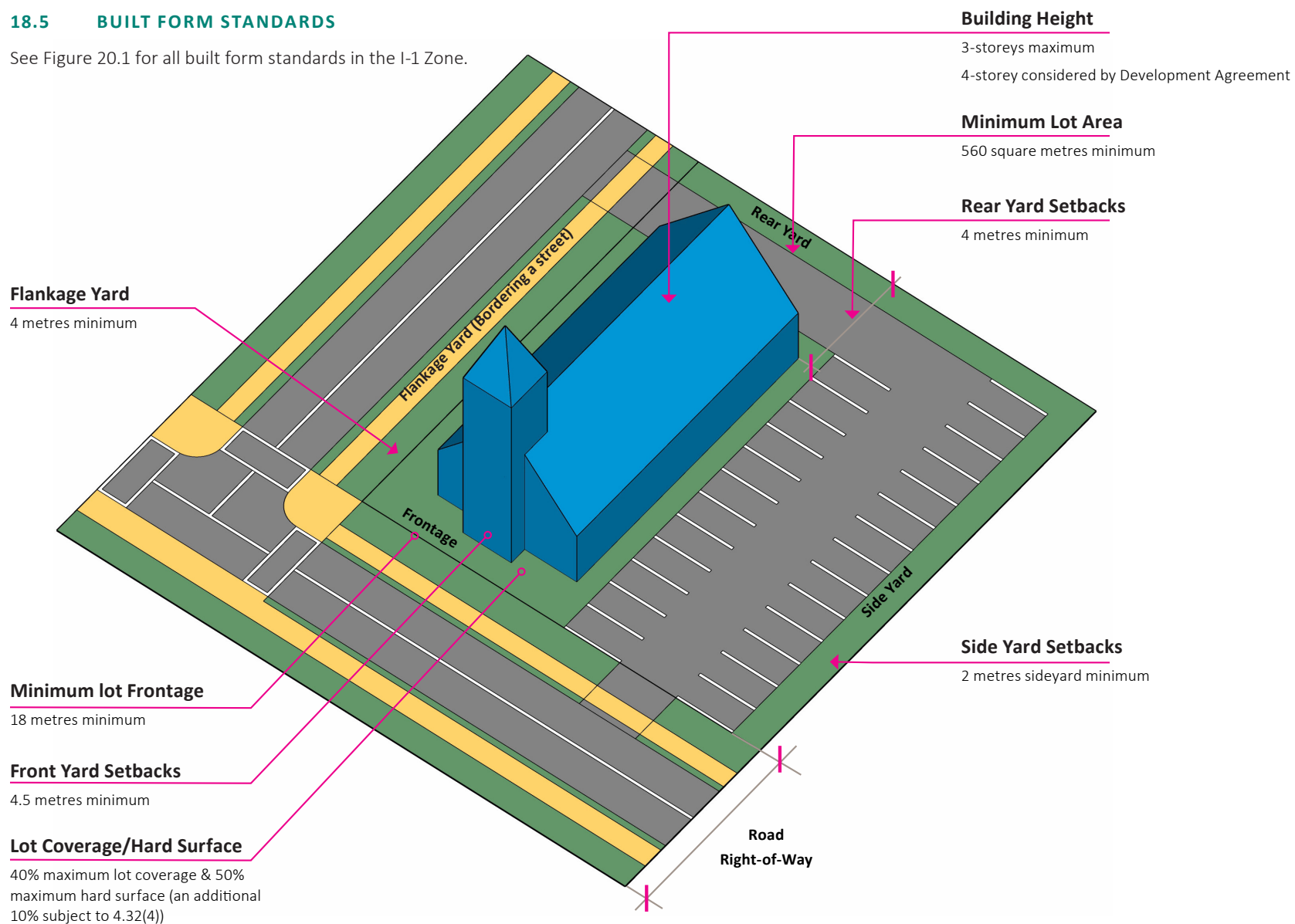


FIGURE 18.1 I-1 ZONE Built Form Standards

18.4 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the I-1 Zone.

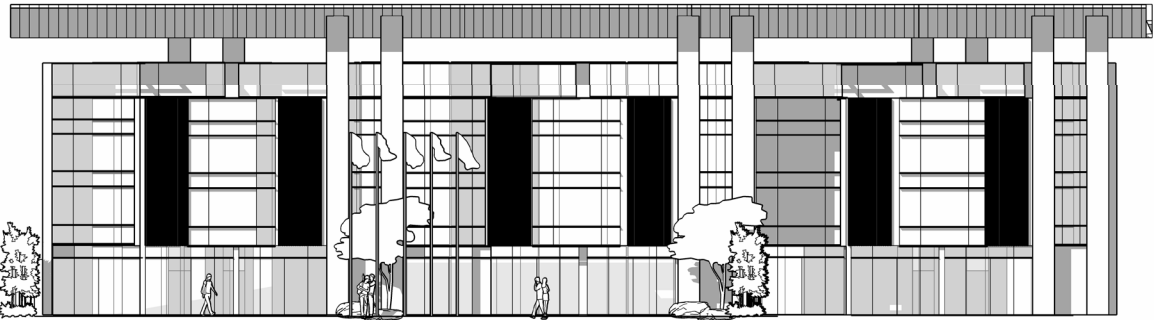


FIGURE 18.2 I-1 Government Building Example

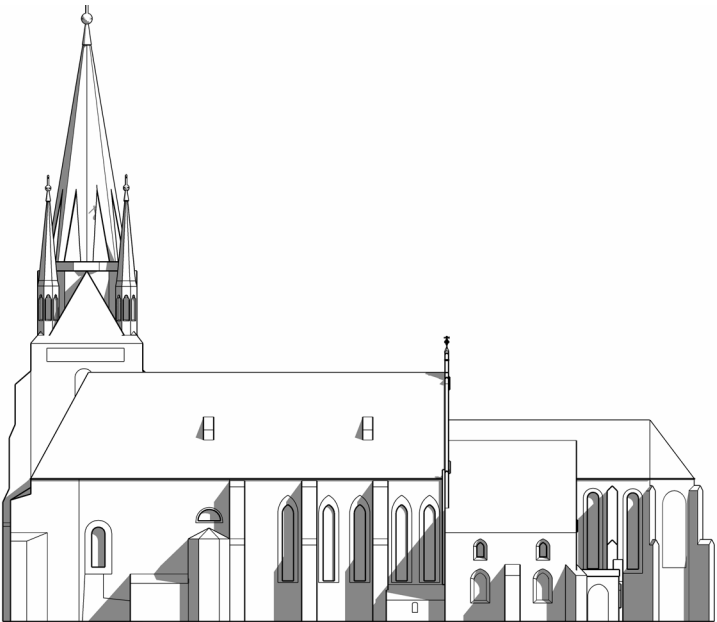


FIGURE 18.3 I-1 Church Example

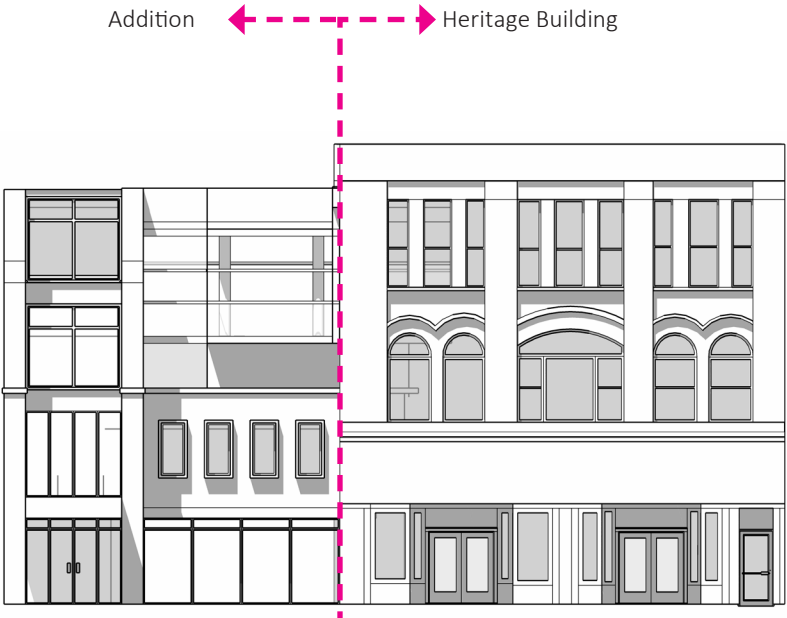


FIGURE 18.4 New Addition to Heritage Building

PART 19 Institutional University (I-2) Zone

19.1 INTENT

The I-2 zone is intended to permit a range of university uses such as classrooms, places of assembly, sports facilities, residential buildings for the use of students, etc. Council may consider unique and site-specific developments by development agreement.

19.2 PERMITTED USES

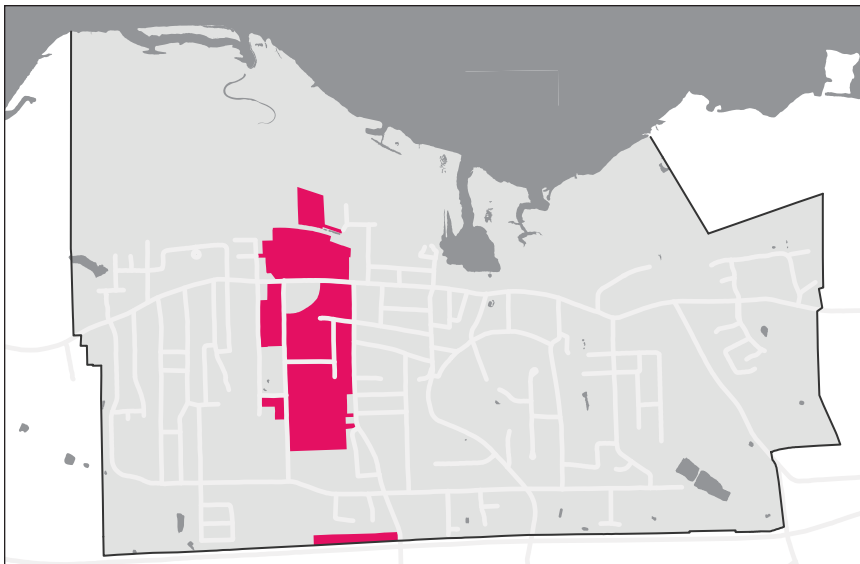
As-of-right, Site Plan and Development Agreement uses are shown in Table 19.1

19.3 RE-ZONING

Rezoning to any other zone shall be considered subject to **policy 11.4.3** of the MPS.

19.4 HEIGHT

Up to 6-storeys are permitted by site plan approval. Up to 8 Storeys may be considered in the area in Schedule 2.



University uses



Sports uses



Places of
assembly



Classrooms

Other Provisions Apply

Part 4:

General Requirements for
All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

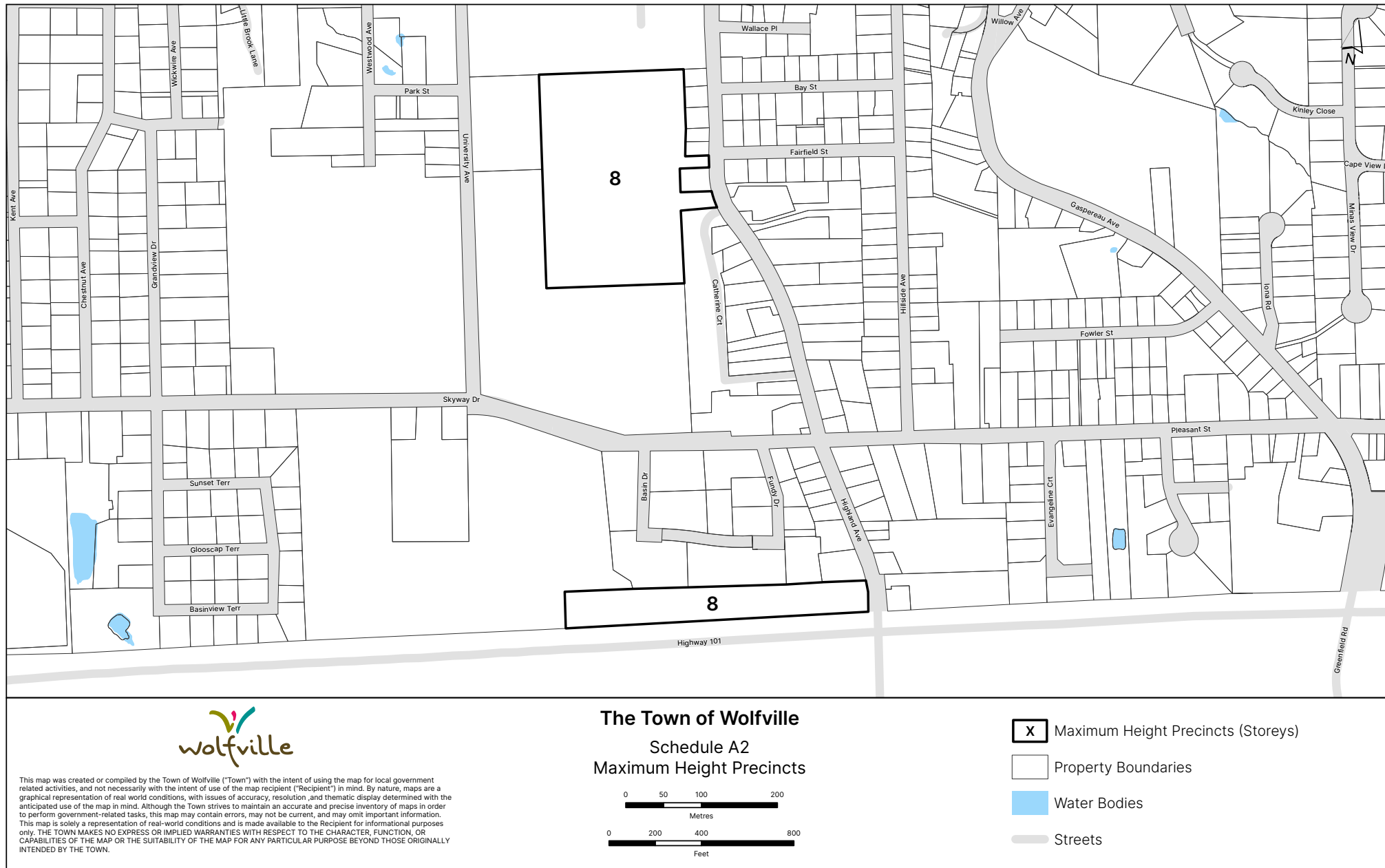
Part 7:

General Requirements for Signs

Part 19:

General Requirements for
Institutional Zones

FIGURE 19.7 Schedule A2 - U-2 Height Precincts



19.5 BUILT FORM STANDARDS

See Figure 21.1 for all built form standards in the I-2 Zone.

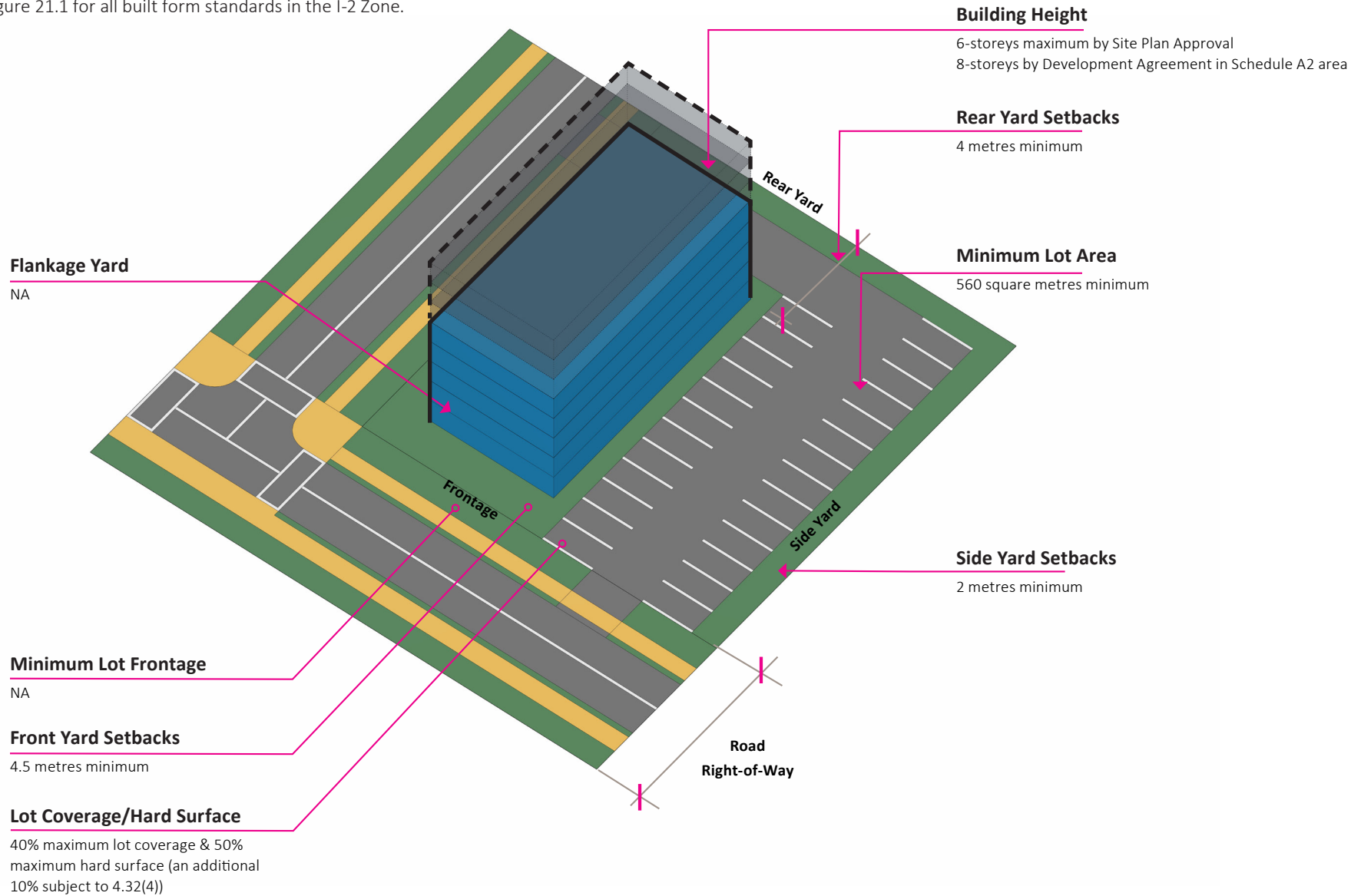


FIGURE 19.1 I-2 ZONE Built Form Standards

19.6 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the I-2 Zone.



FIGURE 19.2 I-2 Institutional Example

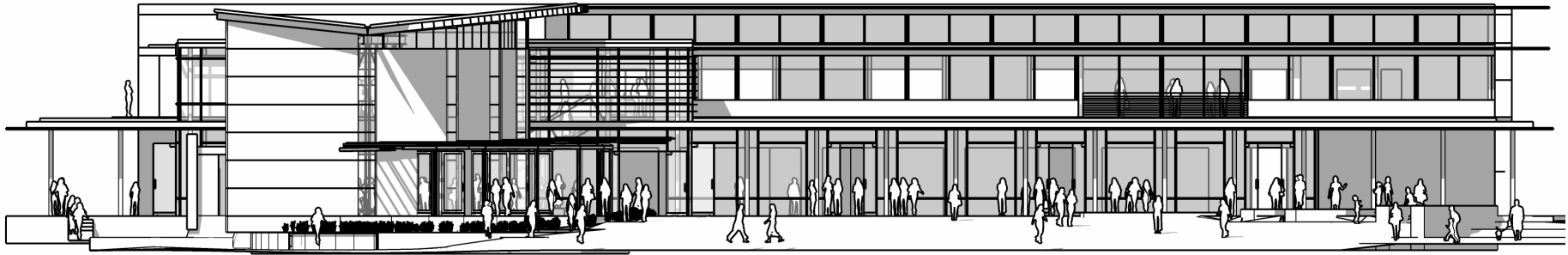


FIGURE 19.3 I-2 Institutional Example

PART 20 Park and Open Space (P-1) Zone

20.1 INTENT

The P-1 zone is intended to permit a range of park and open space uses such as active transportation, cultural facilities, parks and playgrounds, etc.

20.2 PERMITTED USES

As-of-right, Site Plan and Development Agreement uses are shown in Table 19.1

20.3 RE-ZONING

Rezoning to P-1 shall be considered subject to **policy 11.4.3** of the MPS.



Gardens & parks



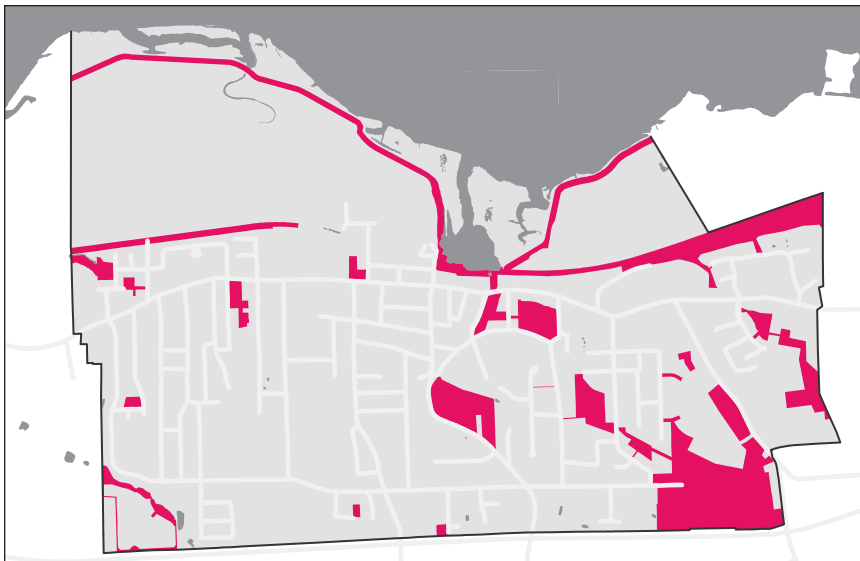
Playgrounds



Trails



Natural areas



Other Provisions Apply

Part 4:

General Requirements for All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 19:

General Requirements for Institutional Zones

20.4 BUILT FORM STANDARDS

See Figure 22.1 for all built form standards in the P-1 Zone

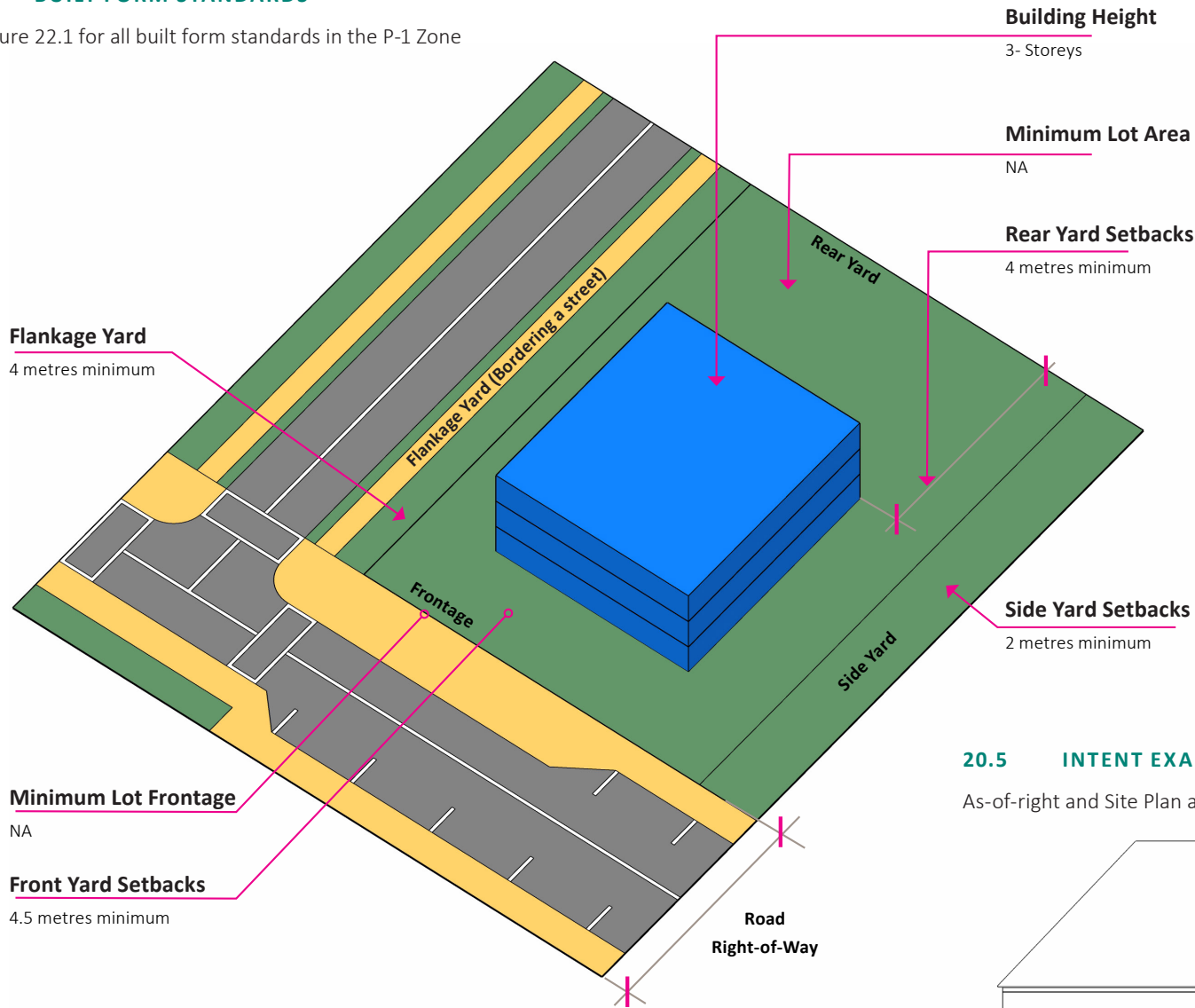


FIGURE 20.1 P-1 ZONE Built Form Standards

20.5 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the P-1 Zone



FIGURE 20.2 P-1 Park and Open Space Example

PART 21 Parks And Open Space - University (P-2) Zone

21.1 INTENT

The P-2 zone is intended to permit a range of park and open space uses on green space areas owned by the University such as community gardens, sports fields, etc.

21.2 PERMITTED USES

As-of-right, Site Plan and Development Agreement uses are shown in Table 19.1

21.3 RE-ZONING

Rezoning to P-2 shall be considered subject to **policy 11.4.3** of the MPS.



Gardens & parks



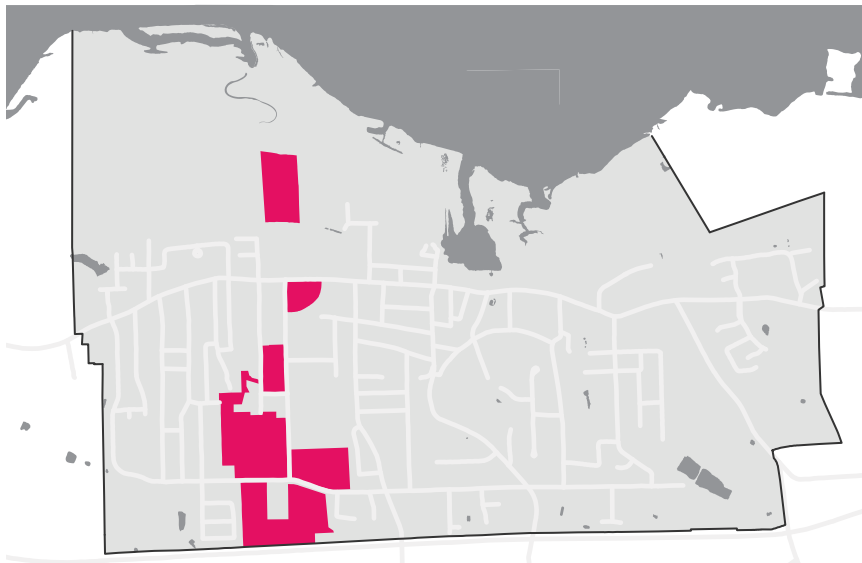
Sports fields



Trails



Natural areas



Other Provisions Apply

Part 4:

General Requirements for All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 19:

General Requirements for Institutional Zones

21.4 BUILT FORM STANDARDS

See Figure 23.1 for all built form standards in the P-2 Zone

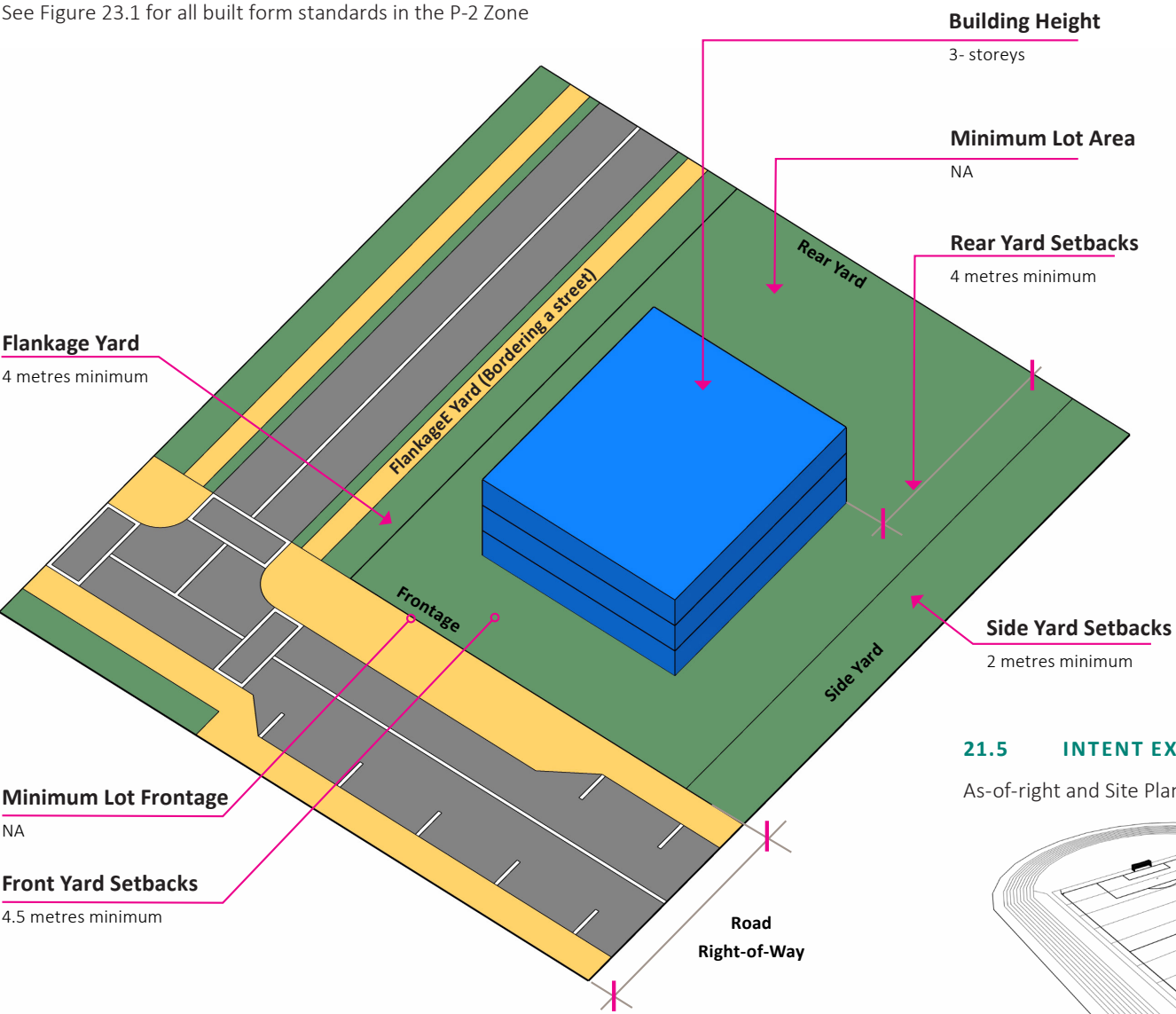
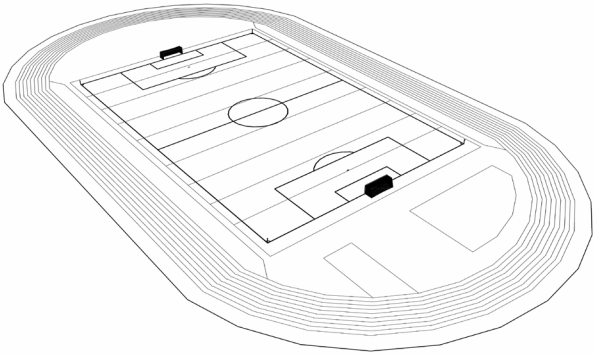


FIGURE 21.1 P-2 ZONE Built Form Standards

21.5 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the P-2 Zone



PART 22 Agriculture (A) Zone

22.1 INTENT

The A zone is intended to permit agricultural uses only. Small scale accessory buildings may also be considered.

22.2 PERMITTED USES

As-of-right, Site Plan and Development Agreement uses are shown in Table 19.1

22.3 RE-ZONING

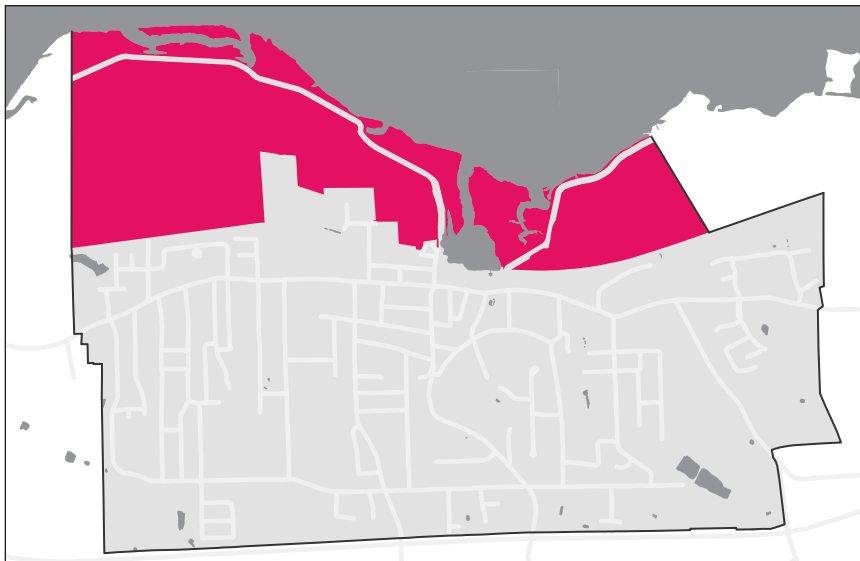
Rezoning is not permitted.

22.4 SPECIAL REQUIREMENTS

- (1) Development within the Agricultural designation also requires the permission of either the Bishop/Beckwith Marsh Body or the Grand Pre Marsh Body whichever marshbody has jurisdiction according to the Marsh Body Act.
- (2) No livestock operations shall be located within 300 metres of an abutting zone.



Agriculture uses



Other Provisions Apply

Part 4:

General Requirements for All Zones

Part 5:

Development Constraints

Part 6:

Parking & Loading Requirements

Part 7:

General Requirements for Signs

Part 19:

General Requirements for Institutional Zones

22.5 BUILT FORM STANDARDS

See Figure 24.1 for all built form standards in the A Zone

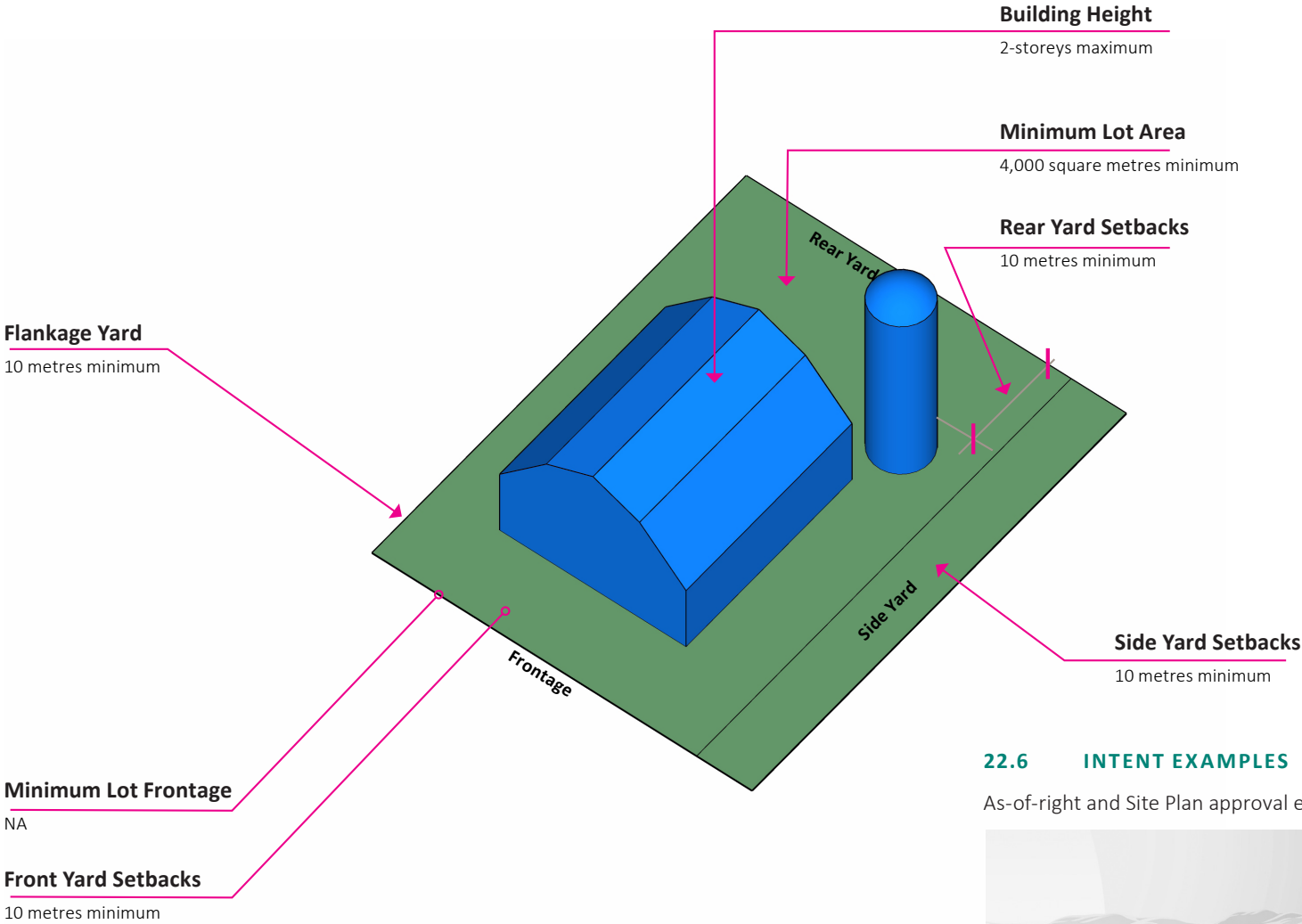
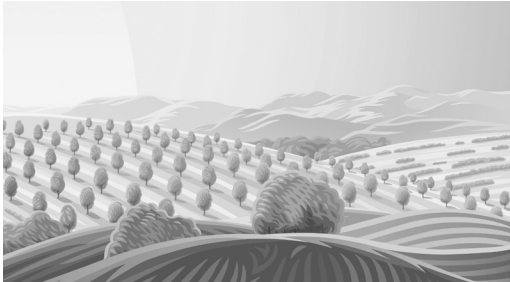


FIGURE 22.1 A ZONE Built Form Standards

22.6 INTENT EXAMPLES

As-of-right and Site Plan approval examples for the A Zone



PART 23 DEFINITIONS

For the purpose of this Bylaw, all words shall carry their customary meaning except for those defined hereafter:

Abut means where properties or lots share a common lot line or a common point along a lot line.

Accessory Building or Structure means a building or structure that is separated from the main building or structure on a lot, and is normally incidental, subordinate, and exclusively devoted to a permitted accessory use. Accessory Buildings or Structures shall be on the same lot as the main building, but do not include buildings or structures attached to the main building, or a building located completely underground.

Accessory Use means the use of land or a building or portion thereof, customarily incidental and subordinate to the principal use of the land or building and located on the same lot. e.g., coffee roasting is an accessory use to the permitted retail sale of coffee beans.

Accommodations means a building or part thereof in which rooms are regularly provided for compensation for use by the travelling public for gain or profit and is not used as a dwelling for the owner and/or members of the owner's family. Hotels, Inns, Motels and Cottages are examples of Accommodations; however, Accommodations shall not include a multi-unit dwelling, a bed & breakfast, a boarding, rooming or lodging house, an apartment building, or a residential care facility.

Adult Entertainment means any form of entertainment or services of which a principal feature or characteristic is the nudity or partial nudity of any person. For the purposes of this definition "partial nudity" shall mean less than completely

covered or opaquely covered: human genitals or human pubic region; human buttocks; or female breast below a point immediately above the top of the areola.

Agricultural Use means the utilizing of land, a building or structures to raise crops or animals or fowl and includes the harbouring or keeping of any one or more of the following livestock regardless of its stage of development: horse, pony, pig, cow, bull, goose, duck, hen, rooster, sheep, goat, or similar livestock.

Alter means any change in the structural component or facade of a building, or any increase or decrease in the volume of a building or structure.

Ambulance Facility means a building or part of a building used to house ambulance vehicles and may include one residential dwelling unit for exclusive use by the ambulance attendants while they are on duty.

Amenity Space means an area capable of being used for active or passive recreation and enjoyment of the occupants of a residential development. The Amenity Space may include landscaped areas, patios, private amenity areas, verandas, balconies, communal lounges, swimming pools, play areas and similar uses, but does not include any area occupied at grade by a building's service area, parking lots, aisles or access driveways.

Architect means a practicing architect who is a member of the The Nova Scotia Association of Architects (NSAA)

Artisan Food and Beverage Production Facility means a place where a licensed micro-brewery or a licensed micro-distillery produces craft beer, cider and spirits; or a place where specialized food products or soft drinks are produced.

Attached Building means a building complete in and of itself, which depends upon a division wall or walls that is (are) above established grade shared in common with an adjacent building or buildings for structural support and complete enclosure.

Autobody Repair Shop means a building used for the repair of body work, detailing and painting of automobile bodies.

Automobile Rental Establishment means a property or part of a property used for the rental of automobiles and the storage of automobiles for that purpose and does not include automobile sales or leasing establishments.

Automobile Service Station means a building or a clearly defined space on a lot used for the sale of lubricating oils and gasoline and may include the sale of automobile accessories, space for the repair of automobiles and a car wash facility.

Bakery means an establishment which produces and /or sells baked goods from an oven.

Basement (or Cellar) means that portion of the foundation system partially or wholly below grade level and which provides storage, utility, or living space.

Building means any structure, whether temporary or permanent, used or capable of use for the shelter, accommodation, and enclosure of persons, animals, material, or equipment.

Building Area means the greatest horizontal area of a building above grade within the outside surface of exterior walls and the centre line of firewalls.

Building Line means any line regulating the position of a building or structure on a lot.

Build-to Plane means the vertical plane that extends upward three storeys from a line drawn parallel to the street at the established front yard setback or extending to both the front and flanking yard setbacks if on a corner lot. <INSERT FIGURE>

Bulk Chemical Storage means the storage of chemicals, including organic compounds, herbicides, fertilizers, and pesticides for use on or off-site, but excluding materials in packages or containers intended for retail sale to the public and materials to support research and teaching activities.

Bulk Fuel Storage means the storage of petroleum or hydrogen fuels or solvents for use on or off-site, but excluding materials in packages or containers intended for retail sale to the public, fuels used for on-site space heating, and on-site fuelling of vehicles.

Bulk Mineral Storage means the storage of salt or other similar de-icing agents, sand, gravel, gypsum, or other similar materials for use on or off-site, but excluding materials in packages or containers intended for retail sale to the public or use by a public authority.

Business Premise means a building or an area within a building, separate or independent from any other portion of the building, occupied by a person for the purposes of

transacting business including the provision of institutional services and the provision of accommodations for the travelling public.

Cafe means an establishment primarily serving coffee or tea and may also serve light meals, sandwiches and/or desserts.

Carport means an open-ended automobile shelter by the side of a building.

Catering Establishment means a building where food is prepared for distribution and consumption outside the premises and does not include a Restaurant, Cafe, or Drive Through Facility.

Club (Including Commercial, Private or Service Clubs) means a building or part of a building used as a meeting place and activity centre for members of an organization.

Cluster or Clustering means a site-planning technique that concentrates buildings and structures in specific areas on a lot, site, or parcel to allow the remaining land to be used for recreation, open space, and /or preservation of features and/or structures with environmental, historical, cultural, or other significance. The techniques used to concentrate buildings may include, but shall not be limited to, reduction in lot areas, setback requirements, and/or bulk requirements, with the resultant open space being devoted by deed restrictions for one or more uses.

Cluster Residential Development means a land development project for more than two residential use buildings on the

same lot in which the site planning technique of clustering dwelling units is employed.

Commercial Brewery means a brewery or cidery engaged in the production of more than 15,000 hectolitres per year of beer or other related beverages where the primary business function is to sell packaged product to the Nova Scotia Liquor Commission and thence to the general public through retail liquor stores or for export.

Commercial Distillery means a distillery engaged in the production of more than 75,000 litres per year of liquor and spirits, other than wine or beer, where the product is blended or bottled for sale to the Nova Scotia Liquor Commission and thence to the general public through retail liquor stores or for export.

Commercial School means a school conducted for gain, and includes, but is not limited to, a secretarial school, language school, or driving school, but does not include a public school.

Commercial Motor Vehicle means any motor vehicle which is used for a business activity and which has as its main purpose financial gain and includes taxis, ambulance, trucks, tractors, tractor trailers, buses, delivery vehicles, and oil trucks, which has three or more axels.

Community Centre means a facility used for recreational, social, educational and/or cultural activities.

Community Garden means an area of land where volunteers develop, manage and operate a small-scale garden that

may contain vegetables, fruit, herbs, flowers, native and ornamental plants.

Convenience Store means a building or part of a building which is used as a store that serves the primary needs of the adjacent neighbourhood and includes the sale of a limited line of groceries, confectionery items, magazines, and a snack bar provided that any eating facility is within a wholly enclosed building.

Corner Vision Triangle means that part of a corner lot adjacent to the intersection of the exterior lot lines measured from such intersection a distance of 5 metres along each such lot line and joining such points with a straight line. The triangular shaped land between the intersecting lines and the straight line joining the points the required distance along the street lines shall be known as the “corner vision triangle”.

Council means the Town Council of the Town of Wolfville.

Craft Beverage Facility means a place where craft beer, cider, and spirits are produced and may include the following:

- Microbrewery means a craft brewery or cidery engaged in the production and packaging of less than 15,000 hectolitres per year of specialty or craft beer, ale, cider or other related beverages.
- Microdistillery means a craft distillery engaged in the production and packaging of less than 75,000 litres per year of liquor and spirits, other than beer or wine.
- Nanobrewery means a craft brewery or cidery engaged in the production and packaging of less than 2,000

hectolitres per year of specialty or craft beer, ale, cider or other related beverages.

Craft Products means products made by hand or small custom production processes including but not limited to potters, pewterers, goldsmiths, silversmiths, jewellers, toymakers, leatherworkers, upholsterers, woodworkers, furniture makers, musical instrument makers, clothing designers and makers, shoemakers, antique refinishers, glass or stained glass workers, sailmakers, small distilleries, and similar skilled craftspeople.

Cultural Facility means a building or part of a building that documents and interprets the social and religious structures, and the intellectual and artistic manifestations that characterize a society, including archives, libraries, museums, galleries, theatres, studios, botanical and zoological gardens of a natural, artistic, historic, educational, or cultural interest.

Daycare Facility means a place where four or more persons are cared for on a temporary daily basis without overnight accommodation but does not include a school.

Development means any erection, construction, alteration, replacement, or relocation of or addition to any structure and any change or alteration in the use of land, buildings or structures.

Development Officer means the person or persons appointed by Town Council to administer the Land Use Bylaw.

Development Permit means the permit issued by the Development Officer certifying that a proposed development complies with the provisions of the Land Use Bylaw.

Development Agreement means an agreement made between the Town and a Developer pursuant to the provisions of the Municipal Government Act.

Developer means any person, incorporated body, partnership or other legal entity which undertakes a development.

Distillery A place where alcoholic spirits are made by distilling fermented ingredients. The use may also include tasting, retail sales, or tours related to the products made on-site.

Drive Through Facility means a business which provides automobile line up space, and which enables transactions to be conducted with a client while the client remains within an automobile. This includes Drive Through Restaurants.

Dwelling means a building or a portion thereof, used or intended to be used for residential occupancy. A Dwelling shall not include Accommodations, a recreational vehicle or travel trailer.

Dwelling, Multi-unit (Large) means a building consisting of nine or more residential dwelling units within a single structure which share facilities such as amenity areas, parking and driveways.

Dwelling, Multi-unit (Small) means a building consisting of three to eight dwelling units within a single structure which share facilities such as amenity areas, parking and driveways.

Dwelling, Single Unit means a detached building used or intended to be used as a single dwelling unit.

Dwelling, Townhouse means a vertically divided building consisting of three or more dwelling units where each unit has its own dedicated exterior entrance and where each unit is designed to exist independently if subdivided.

Dwelling, Two Unit means a building containing two dwelling units.

Dwelling, Two Unit (Duplex) means a horizontally divided building consisting of two dwelling units.

Dwelling, Two Unit (Semi-detached) means a vertically divided building consisting of two dwelling units sharing a common wall where each unit has its own dedicated exterior entrance and where each unit is designed to exist independently if subdivided.

Dwelling Unit, Accessory - Internal means an independent dwelling unit built within the structure of an existing Dwelling Unit.

Dwelling Unit, Accessory - Detached means a detached building used or intended to be used as a single dwelling unit in an Accessory Building.

Educational Facilities means any building or part of a building designated for learning or instruction, including public, private and commercial schools, daycares and nursery schools, colleges, and universities. Educational uses may include classrooms, laboratories, or research facilities.

Emergency Services means any building or part of a building that houses an organization whose purpose is to provide police, ambulance, firefighting, or search and rescue services. These uses may also include, but are not limited to, the storage of emergency vehicles until they are dispatched, overnight accommodations for personnel directly related to the use, and associated training facilities.

Erect means to build, construct, reconstruct, alter or relocate, and without limiting the generality of the foregoing, shall include any preliminary physical operation such as excavating, grading, piling, cribbing, filling, or draining, and structurally altering any existing building or structure by an addition, deletion, enlargement, or extension.

Established Grade means, with reference to a building, the elevation of the finished grade of the ground where it meets the exterior of such building and when used with reference to a structure shall mean the average elevation of the finished grade of the ground immediately surrounding such structures, exclusive in both cases of any artificial embankment; and when used with reference to a street, road, or highway means the elevation of the street, road, or highway established by the Town or other designated authority.

Existing means existing as of the effective date of the By-law, unless otherwise noted.

EV Ready: A parking space supplied with conduit and electrical capacity to a designated electrical panel capable of supporting a minimum 120-volt (Level 1) charging circuit for future installation of an EV charging outlet.

Farm Animal means horses, cattle, sheep, goats, swine, fowl, mink or fox.

Farm Market means a building or part of a building in which farm produce; crafts and/or baked goods make up the major portion of items offered for sale.

Financial Institution means any chartered bank, trust company, credit union, or similar business.

Flood Plain means the area of land adjoining a river, stream, or ocean which has been or may be hereafter covered by flood water and shall include land subject to flooding by tidal action.

Flood Tolerant means remedial measures undertaken to reduce flood risk on buildings in the flood plain area.

Floor Area means the aggregate surface area of a building or defined space, as follows:

- **Commercial Floor Area** means the total usable floor area within a building used for commercial purposes, but excludes washrooms, furnace and utility rooms, and common areas between stores.
- **Dwelling Floor Area** means the aggregate floor area of all floors contained within the outside walls of a dwelling, excluding any private garage, porch veranda, sunroom; unfinished attic, basement or cellar, or other room not habitable at all seasons of the year.
- **Total Floor Area** means the aggregate area of all dwelling or commercial floor areas or other floor areas contained within the exterior wall of a building.

Grading means the alteration of land levels, including the addition or removal of topsoil or other material of any kind;

Green Roof means a roof consisting of vegetation and soil planted over a waterproofing membrane. (Canada Green Building Council).

Ground Floor means the lower most storey of a building which has more than one half of its height above the average finished grade level adjacent to the exterior walls of the building.

Hard Surface means any surface on a lot that is impervious to water (70% or greater run-off) and includes a building, asphalt paving, concrete paving, brick paving, and compacted granular surfaces.

Health Centre means a building used for the treatment of persons afflicted with or suffering from sickness, disease, or injury; and administrative offices for health care agencies and individuals.

Height means the vertical distance of a building between the streetline grade and one of the following:

- The highest point of the roof surface or the parapet, whichever is the greater, of a flat roof;
- The deckline of a mansard roof; or
- The mean level between eaves and ridges on a gabled, hip, gambrel, or other type of pitched roof.
- Exemptions from these Height Requirements are contained in Section XX.

Home based business means the accessory use of a dwelling for gainful employment involving the production, sale, or provision of goods and services on a small scale.

Home Occupation – Restricted means a room or rooms in a Dwelling Unit used for the operation of a business, where a maximum of one client may be on the premises simultaneously, without signage, and in which no employees, other than the resident, are employed.

Hospital means a building used for the treatment of persons afflicted with or suffering from sickness, disease, or injury.

Hostel means an establishment providing transient, overnight accommodations, typically characterized by low cost, shared use of a self-service kitchen, common areas, sleeping rooms, and bathroom facilities.

Hotel means a building or establishment which provides accommodation for the travelling public for financial remuneration and may include other facilities such as meeting rooms, restaurants and entertainment facilities.

Household means one or more persons living together as a single non-profit, independent housekeeping unit, sharing all areas of the Dwelling Unit.

Innovative Development means a development type that does not meet the zone requirements for as-of-right or site plan approval permissions but is still consistent with the overall intent of the municipal planning strategy and may be considered by development agreement.

Institutional Use means a non-profit, religious, or public use, such as a religious building, library, public or private school, hospital, or government owned or operated building, structure, or land used for public purpose.

Interpretation Centre means an establishment, building or structure, where knowledge of historical, cultural, or natural heritage, most often of local or regional significance, is disseminated or conveyed to visitors and residents.

Interpretive Display means an outdoor structure, exhibit or display where knowledge of historical, cultural or natural heritage, most often of local or regional significance, is conveyed to visitors and residents.

"Landscape Architect" means a person who is a member in good standing in the Atlantic Provinces Association of Landscape Architects (APALA);

Landscaping means any combination of living plants, such as trees, shrubs, vines, ground covers, flowers, or grass; natural features such as rock, stone, bark chips or shavings; and structural features, including but not limited to fountains, reflecting pools, outdoor art work, retaining walls under X in height, screening walls, fences, or benches; which are designed to enhance the visual amenity of a property or screen any objectionable features which may detrimentally affect adjacent land.

Laundromat means a business that provides home-type washing, drying and/or ironing machines for hire to be used by customers on the premises.

Level 1 Charging uses a standard 120v outlet for charging electric vehicles.

Level 2 charging is defined by SAE International's J1772 standard and includes:

- **Charge Method:** AC Level 2
- **Nominal Supply Voltage (V):** 208V to 240V AC, single phase
- **Maximum Current (Amps-continuous):** Equal to or less than 80A The amperage rating for EV circuits required by most Level 2 EVSE is 40A, although this may differ depending on the particular system design.

Light Industrial Use means the use of a building or part thereof for research and development facilities; small scale processing and fabrication operations; or small-scale light manufacturing operations.

Limited Commercial Use means a convenience store, laundromat, farm market or small-scale commercial use aimed at a local neighbourhood clientele.

Limited Institutional Use means the small scale use of land, or building by an incorporated body or society for promoting a particular purpose or for providing a service, including private schools, daycare facilities, medical clinics, places of worship, nursing homes, private non-profit clubs, and residential care facilities for five to ten persons.

Livestock Operation means a structure for the keeping and rearing of farm animals for agricultural uses.

Loading Space means a vacant area of land provided and maintained upon the same lot or lots upon which the principal

use is located which is suitable for the temporary parking of at least one commercial motor vehicle while merchandise or materials are being loaded or unloaded from such vehicles and which is not upon or partly upon any street or highway.

Lot means any parcel of land described in a deed or as shown on a registered plan of subdivision.

- **Corner Lot** means a lot situated at the intersection of and abutting on two or more streets.
- **Flag Lot** means a lot situated at the rear of another lot having normal street frontage and depending on a narrow strip of land which is part of the lot, for access to a street.
- **Interior Lot** means a lot situated between two lots and having access to one street.
- **Through Lot** means a lot bounded on two opposite sides by streets or highway provided, however, that if any lot qualifies as being both a corner lot and a through lot as herein before defined, such lot shall be deemed a corner lot for the purpose of the Bylaw.

Lot Area means the total horizontal area within the lot lines of a lot.

Lot Coverage means the percentage of the building area to the lot area.

Lot Depth means the horizontal distance between the front and rear lot lines. Where these lot lines are not parallel, the lot depth shall be the length of a line joining the midpoints of the front and rear lot lines.

Lot Frontage means the horizontal distance between the side lot lines. Where the side lot lines are not parallel, the Lot Frontage shall be measured perpendicularly to the line joining the middle of the front lot line with either the middle of the rear lot line or the apex of the triangle formed by the side lot lines and at a point therein equal in distance to the minimum applicable front yard. In the case of a corner lot with a corner vision triangle, the exterior lot lines (street lines) shall be deemed to extend to their hypothetical point of intersection for the purpose of calculating the frontage. <INSERT FIGURE>

Lot Line means the boundary line of a lot.

Front Lot Line means the line dividing the lot from the street; in the case of a corner lot either boundary line abutting the street may be deemed the front lot line providing it meets the frontage requirements of the Bylaw for the proposed use. In the case of a through lot, either boundary dividing the lot from the street shall be deemed to be the front lot line.

- (a) Side Lot Line means a lot line other than a front or rear lot line.
- (b) Rear Lot Line means the lot line furthest from or opposite to the front lot line.
- (c) Flankage Lot Line means a side lot line which abuts the street on a corner lot.

Lounge means a premise that is licensed under the Nova Scotia Liquor Control Act as a lounge but does not include a billiards club licensed as a lounge under the Nova Scotia Liquor Control Act.

Low Impact Development means the definition provided in the Town of Wolfville Stormwater Management Design Guidelines.

Main Building means that building on a lot wherein the principal use of that lot is carried out.

Main Wall means the exterior front, side, or rear wall of a building and all structural members essential to the support of a fully or partially enclosed space or roof.

Medical Clinic means a building used wholly for the medical, dental, surgical, or therapeutic treatment of human beings and includes pharmacies, medical equipment suppliers, health care offices and other professional and commercial uses related to medical care but does not include a hospital.

Mixed-use Neighbourhoods means neighbourhoods that contain a variety of land uses and services.

Neighbourhood Commercial Use means a small-scale commercial development inside an existing building designed to serve the immediate surrounding residential areas, featuring small storefronts, buildings located close to the street, and smaller scale than larger commercial properties in the Downtown area.

Non-Commercial School means a public or private academic, religious, or philanthropic school not conducted for monetary gain.

"Nova Scotia Land Surveyor" means a registered or licensed member, in good standing, of the Association of Nova Scotia Land Surveyors.

Nursing Home means a building wherein nursing care, room and board are provided to individuals incapacitated in some manner for medical reasons but does not include a hospital.

Obnoxious Use means a use which, from its nature or operation, creates a nuisance or is offensive by reason of noise or vibration or the emission of gas, fumes, dust, oil or objectionable odour or by reason of the unsightly storage of goods, wares, merchandise, salvage, refuse, waste, or other material.

Office (Business, Professional or Government) means a room or rooms where business may be transacted a service performed or consultation given, including but not limited to physicians, surgeons, dentists, lawyers, architects, engineers, accountants, real estate agents, insurance agents, massage therapists, photographers, optometrists, chiropractors, eye specialists and similar uses, and may include retail sales or dispensing of goods associated with and incidental to the main use.

"Order" means an enforceable order issued by the Town under the powers conferred to it by the Municipal Government Act and to an individual or an organization to take certain corrective action, or to refrain from an activity.

Outdoor Wood Furnace / Water Stove means any individual furnace designed to burn untreated wood and wood products and used for the purpose of heating liquid or air where the furnace is located outside the structure into which the hot water or air is produced or piped.

"Owner" includes as it refers to the owner of property:

- (a) A part owner, joint owner, tenant in common or joint tenant of the whole or any part of land or a building,
- (b) In the case of the absence or incapacity of the person having title to the land or building, a trustee, an executor, a guardian, an agent, a
- (c) Mortgagee in possession or a person having the care or control of the land or building,
- (d) A person who occupies shores, beaches or shoals, and
- (e) In the absence of proof to the contrary, the person assessed for the property;

Park means an open area devoted to passive recreational uses or conservation uses and may include ornamental gardens and lawns, botanical gardens, outdoor furniture, accessory structures, children's playgrounds, and on-site parking areas which support park uses.

Parking Lot or Area means an open area, other than a street, containing parking spaces for multiple motor vehicles, available for public use or as an accommodation for clients, customers or residents and which has access to a street or highway by means of driveways, aisles or manoeuvring areas where no parking or storage of motor vehicles is permitted.

Parking Space means an area use for the for the temporary parking or storage of motor vehicles of not less than 15.13 square metres and measuring 2.75 metres by 5.5 metres.

Pavilion means a building or temporary structure used as a shelter in a park or large garden that is usually open and ornamental and may include a tent.

Personal Office means a room or rooms in a Dwelling Unit used for the operation of a business, where a maximum of one client may be on the premises simultaneously, without signage, and in which no employees, other than the resident, are employed.

Personal Service Shop means a building or part of a building in which persons are employed in furnishing services and otherwise administering to the individual and personal needs of persons, and includes but is not limited to barber shops, hairdressing shops, shoe repair, tanning salons, aesthetician shops and electrolysis shops but excludes any manufacturing or fabrication of goods for sale.

Place of Assembly means a building or facility where people congregate for deliberation, entertainment, cultural, recreation or similar purposes, including but not limited to auditoriums, places of worship, clubs, halls, meeting halls, community facilities, open spaces, restaurants, galleries, and recreational fields, courts or facilities.

Place of Entertainment means a wholly enclosed building or facility where people assemble in order to engage in passive or active activities for the sake of being entertained. These uses may include but are not limited to cinemas/movie theatres, bowling alleys, arcades, billiards or pool halls, laser tag, etc. but exclude Adult Entertainment Uses and Lounges.

Place of Worship means a building used for religious worship, study, and instruction, including but not limited to churches, monasteries, mosques, synagogues, temples, etc. A place of

worship may also include an auditorium, hall, daycare facility or nursery operated by the place of worship.

"Professional Engineer" means a registered or licensed member in good standing of the Association of Professional Engineers of Nova Scotia;

Post Office means an institution which organizes and handles the delivery of mail, possibly with associated retail.

Public Authority means Her Majesty The Queen in the Right of Canada or the Province of Nova Scotia, the Town, or anybody authorized by any or all of the above.

Public Façade means that part of the exterior of a structure which can be seen from public property including but not limited to public streets.

Public Park means a park owned or operated by a Public Authority.

Recreational Facility means a facility, either publicly or privately owned, that is designed and equipped to support sport, leisure, and recreational activities, including but not limited to: Arenas, Community Centres, Curling Rinks, Gyms and Fitness Centres, Skate Parks, Spas, Sports Fields, Swimming Pools, Tennis Courts, and other similar uses.

Recreational Uses means the public or private use of land for parks, playgrounds, tennis courts, lawn bowling greens, indoor and outdoor skating rinks, athletic fields, golf courses, picnic areas, swimming pools, day camps, community centres, and other similar uses, together with necessary and accessory buildings and structures, but not including a track for the racing of animals or any form of motorized vehicles.

Repair Shop means any building or part thereof used for the repair of household articles including but not limited to radio, television, and appliance repair shops, plumbing repair shops, electrical repair shops, furniture repair shops, shoe repair shops and not including industrial or manufacturing, motor vehicle repair shops or auto body repair shops.

Residential Care Facility means a community-based group living arrangement, in a single dwelling unit, for no more than four persons exclusive of staff and/or receiving family, with physical and/or intellectual disabilities, in which accommodation, together with nursing and personal care is provided for the well-being of its residents through self-help, professional care, guidance and supervision unavailable in the residents' own family, an independent living situation, or facility. There shall be a resident manager at such a facility. These facilities in an Institutional zone may provide care for five or more persons. Where a single unit is located in a detached dwelling, and consists of four or fewer individuals, excluding staff, the use is a single unit dwelling. A Residential Care Facility is licensed, funded or approved by the Province of Nova Scotia.

Restaurant means a building or part thereof where food and drink are prepared and served to the public for consumption within the building or attached outdoor patio or for takeout.

Retail Store means a building or part thereof in which goods, wares, merchandise, substances, articles or things are offered or kept for sale to the public.

Rooming House see Lodging House.

Rooming Unit see Lodging Unit.

Secondary Containment means surrounding one or more primary storage containers to collect any hazardous material spillage in the event of loss of integrity or container failure.

Service Industries include a printing establishment, a laundry or cleaning establishment, a paint shop, plumbing shop, sheet metal shop and similar uses.

Short Term Rental means the rental of a dwelling unit or part thereof for overnight stay to the travelling public for a period of 30 days or less and includes a bed and breakfast or similar use.

Sidewalk Cafe means a group of tables and chairs, and other accessories belonging to an Owner, situated and maintained upon a public sidewalk for the use and consumption of food and beverages sold to the public, from or in, the Owner's adjoining indoor food and beverage establishment. Sidewalk cafes shall be subject to the Sidewalk Cafe Bylaw.

Sign means any letter, marks, symbols, or pictorials placed on a structure, and includes the structure on which the letter, marks, symbols, or pictorials are placed.

- **Canopy / Awning Sign** means any sign attached to or forming part of a canopy or awning.
- **Combined Sign** means a ground sign or a facial wall sign which refers to more than one business premise.
- **Fascia Wall Sign** means a sign attached to or erected against a wall of a building, with the face horizontally

parallel to the building wall and with no space between the back of the sign face and the building wall.

- **Ground Sign** means a sign erected on a free-standing frame, mast or pole and not attached to any building with a maximum of two sides used for copy area.
- **Illuminated Sign** means any sign which emanates light through transparent or translucent material from a light source within the sign and includes a neon light sign but does not include a sign placed on a lighted awning.
- **Portable Sign** means a ground sign no greater than 0.56 square metres (6ft²) in area which is not permanently affixed to or in the ground, including sandwich board signs.
- **Projecting Sign** means a sign, other than a facial wall sign, which is attached to and projects from a structure or building face.
- **Off Premises Sign** means a sign that is related to a business premise that is not located on the building or lot on which the sign is placed.
- **Reader Board Sign** means a sign designed with removable letters, marks, symbols, or pictorials or any combination of these to allow for periodic revisions of the sign message, but does not include:
 - Signs used by theatres, art galleries, arenas, churches or similar premises, for the purpose of identifying a program or event that will take place at the premise.

- Signs used by a farm market to denote availability of seasonal products.

Sign Area means the area of the smallest triangle, rectangle, circle or semi-circle which can wholly enclose the display area of one (1) side of the sign but not including any portion of the support structure. For canopies and awnings, sign area is considered to be the area of the smallest triangle, rectangle or circle or semi-circle which can wholly enclose the surface area of the elements which convey information.

Single Room Occupancy means a housing type wherein one or two people are housed in single rooms, wherein tenants typically share bathrooms and a kitchen in a single unit dwelling.

Sleeping Area means the aggregate area within a dwelling unit of all bedrooms measured from the inside walls of each bedroom and shall include the area of closets within bedrooms, but shall not include hallways, bathrooms or linen closets associated with the bedrooms.

Site Specific Rules or permissions that apply only to one particular property, rather than to all properties in the same zone.

Storey means that portion of a building which is situated between the top of any floor and the top of the floor next above it, and if there is no floor above it, that portion between the top of such floor and the ceiling above it. A storey shall not exceed 4.5m in vertical distance.

Street or Road means the whole and entire right-of-way of every highway or road vested in the Province of Nova Scotia or the Town of Wolfville.

Street Intersection means the area embraced with the prolongation or connection of the lateral boundary lines of two or more streets which join one another at an angle, whether or not street crosses the other.

Street Line means the boundary line of a street (including sidewalks).

Streetline Grade The elevation of the finished surface of the curb, sidewalk, or roadway at the streetline, as established by the Municipality. Where no grade has been established, the streetline grade shall be determined by the Engineer or Development Officer.

Structure means anything that is erected, built or constructed of parts joined together or any such erection fixed to or supported by the soil or by any other structure. A structure shall include buildings, walls, and signs as well as fences exceeding 2 metres in height.

Studio means a building or part thereof used for the study, or instruction of any fine or commercial art including photography, music, visual arts, and commercial design or the sale of craft products.

Swimming Pool means an artificial body of water outside a building, excluding ponds, having more than 10 square metres of surface area that is designed or intended to be used for swimming purposes and contains or is capable of containing a water depth of more than 60 centimetres.

Theatre means a building or part of a building used for showing motion pictures or live dramatic performances and shall include an open-air theatre.

Town means the Town of Wolfville.

Unique Having distinct characteristics, circumstances, or conditions that differentiate a property, use, or development from others, and that may justify special consideration or site-specific regulation within this Bylaw.

Use means the purpose for which any land, building or structure is utilized, and also means the purpose for which any land, building or structure is designed, arranged, or intended, or the purpose for which any land, building or structure is occupied or maintained.

Utility Equipment Building means a building containing any public or private utility system, works, plant, and equipment including by not limited to sewage pumping stations and telephone switching stations.

Veterinary Clinic means a building or part of a building used for the medical, surgical or therapeutic treatment of animals.

Warehouse means a building where wares or goods are stored but shall not include a retail store.

Waste Bin means a building used to store solid waste or resource materials for a period of time until they are collected for disposal.

Watercourse means every river, stream, lake, creek, pond, spring, lagoon, swamp, marsh, wetland, ravine, gulch, or other natural body of water within the jurisdiction of the Province.

Workshop means a building or part of a building where craft products are produced and where craft instruction may be offered.

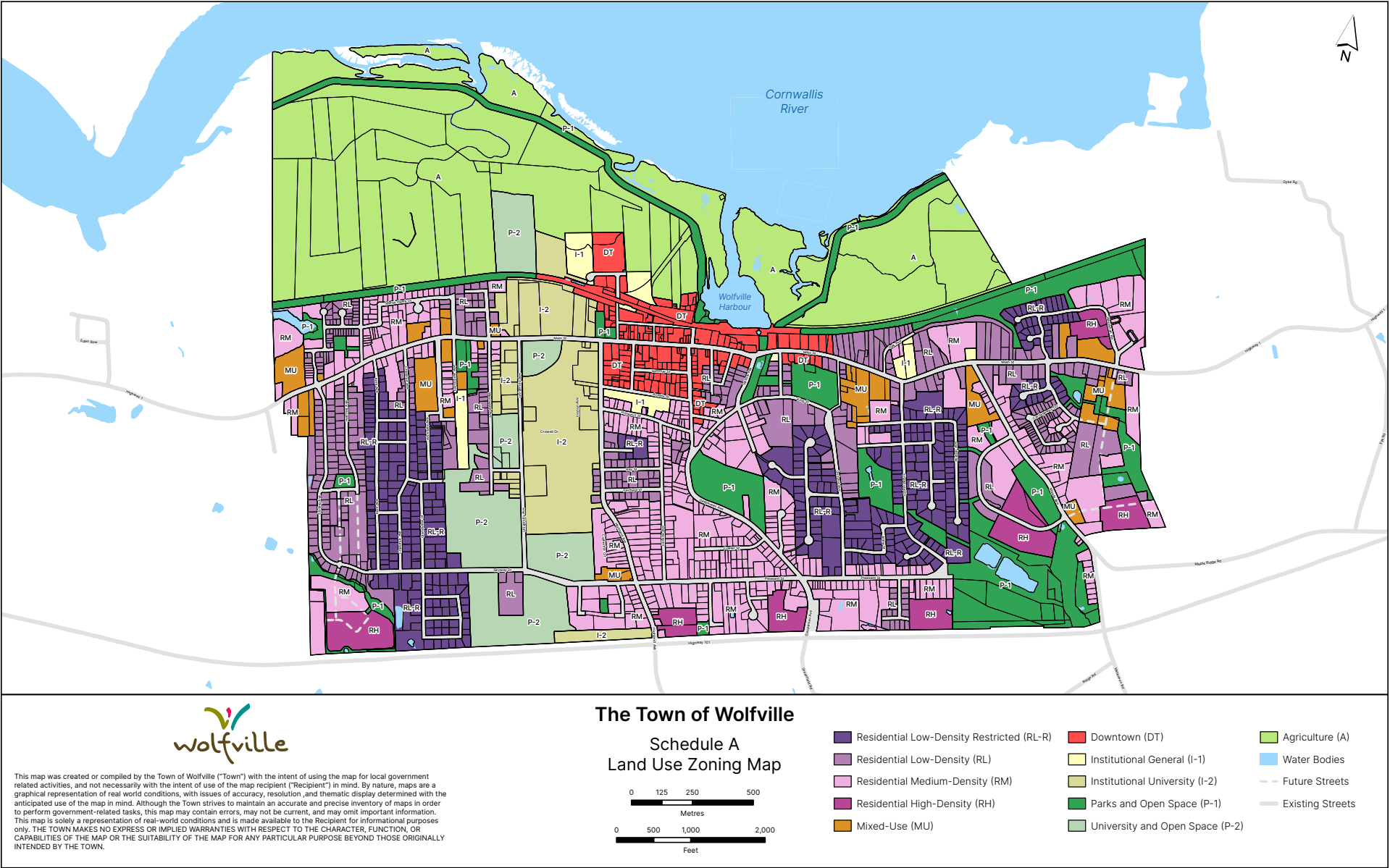
Yard means an open, uncovered space on a lot adjacent to a building or structure. In determining yard measurements, the minimum horizontal distance from the respective lot lines shall be used and for greater certainty:

- **Flankage Yard** means the side yard of a corner lot which side yard extends from the front yard to the rear yard between the flankage lot line and the nearest main wall of any structure. Minimum flankage yards means the smallest allowable distance a flankage yard can be measured.
- **Front Yard** means a yard extending across the full width of a lot between the front lot line and the nearest wall of any building or structure on the lot. Minimum front yard means the smallest allowable distance a front yard can be measured.
- **Rear Yard** means a yard extending across the full width of a lot between the rear lot line and the nearest wall of any main buildings or structure on the lot. Minimum rear yard means the smallest allowable distance a rear yard can be measured.
- **Side Yard** means a yard extending from the front yard to the rear yard of a lot between the side lot line and nearest wall of any building or structure on the lot. Minimum side yard means the smallest allowable distance a side yard can be measured.

Zone means a specific area of land shown on Schedule “A” of this Bylaw and more particularly referred to in Part 3 of this Bylaw.

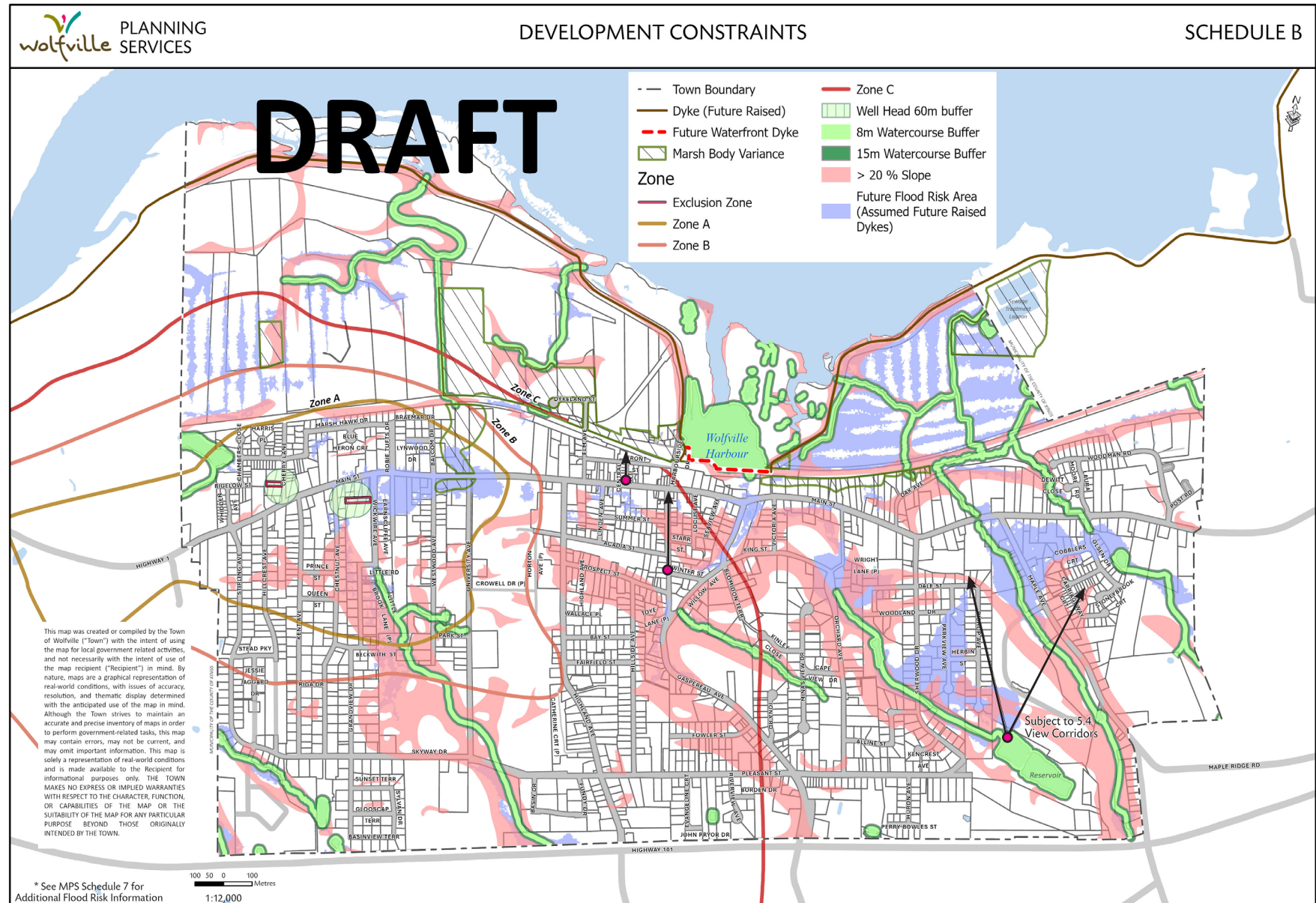
Schedule A:

LAND USE ZONING MAP



Schedule B:

DEVELOPMENT CONSTRAINTS OVERLAY



DOWNTOWN AND NEIGHBOURHOOD DESIGN AREAS



Schedule D:

COMPREHENSIVE DEVELOPMENT DISTRICT ZONE SUSTAINABILITY CHECKLIST

ABOUT COMPREHENSIVE DEVELOPMENT DISTRICTS (CDD)

The CDD designation highlights areas of Wolfville where future neighbourhood areas will be developed and is a way of regulating new residential land developments. Large vacant land holdings, primarily in the east and west ends of town shall remain in the CDD designation and zone. These areas provide opportunities for the creation of new neighbourhoods including a range of housing types and tenures and other compatible uses that provide for the day-to-day needs of residents. CDDs are intended to provide for flexibility and innovation in the design and sustainable development of these lands for a variety of residential purposes.

GUIDING PRINCIPLES

Four guiding principles set out to guide development in CDD zones in the Town of Wolfville. The principles originated from thorough assessment of the current environmental, social, and economic characteristics of the Town, while respecting future aspirations and goals of the community. The guiding principles frame the future goals of the Town and the checklist is a tool to help facilitate the process.

New development is essential to achieve a complete community to live, work and play. The purpose of the checklist and its criteria are meant to engage the developer in the planning process and establish an iterative process to build the best neighbourhoods possible that capture Wolfville's spirit, while developing neighbourhoods for our future.

THE FOUR GUIDING PRINCIPLES

Land Use + Design

The proposed development encompasses an array of land uses that reflect the needs of Wolfville residents and the neighbourhood members. A community centre ties surrounding areas and services together logically with daily needs within walking distance. Services proposed for the site reflect the projected density and needs of the development.

Connectivity

Neighbourhood design, provisions, and unit affordability account for individuals and households of various sizes and income levels. Where possible, infrastructure and building practices will accommodate accessible building practices. Mixed housing types are available throughout the neighbourhood and take into consideration the Town of Wolfville's unique demographics and community culture.

Environmental and Economic Sustainability

Sustainability based on environmental and financial indicators to ensure a prosperous and adaptable neighbourhood for current and future Wolfville generations.

Social Equity

Neighbourhood design, provisions, and unit affordability account for individuals and households of various sizes and income levels. Where possible, infrastructure and building practices will accommodate accessible building practices. Mixed housing types are available throughout the neighbourhood and take into consideration the Town of Wolfville's unique demographics and community culture.

INSTRUCTIONS

Developers and their consultants are important partners in maintaining and improving Wolfville's high quality of life and unique character that make our Town a destination in Atlantic Canada. By outlining sustainability criteria and engaging with developers in the earliest stages of site design and planning, enable the project to achieve its highest level of success in line with the Town's guiding principles. By outlining the criteria in such a way, the checklist allows developers to engage with the planning process and self-evaluate how their design and development reflects the future vision for the Town of Wolfville.

Self-Scoring

Applicants are to review and complete the checklist on their own. The applicant may ask any questions or seek direction if necessary during the application process. Any score received on the checklist does not determine whether a development will ultimately be approved or not, it is simply one component of the application process. The highest possible score is 260 points.

Each individual criterion should be ranked on a scale from 0-5 in the box provided preceding the component,

0 - indicating the component has not been addressed,

3 - it has been addressed to some extent, and

5 - the applicant feels they exceed what the checklist is asking.

* Where scoring is not relevant write response in the space provided.

Although it is not necessary, applicants are encouraged to submit any additional documents, reports, or certifications that may be relevant to the checklist criteria.

1) LAND USE + DESIGN

The proposed development encompasses an array of land uses that reflect the needs of Wolfville residents and the neighbourhood members. A community centre ties surrounding areas and services together logically with daily needs within walking distance. Services proposed for the site reflect the projected density and needs of the development.

Community Centre	Score	Description
Incorporate community social gathering places (village square, halls, youth and seniors facilities, community boards)?		

Mixed Land Uses	Score	Description
Promote diversification of the local economy via business type and size that is appropriate for the area?		
Please list the neighbourhood stores or amenity opportunities proposed on the site (i.e. school, daycare, library, community centre, health facilities, etc.).		

Community Plans	Score	Description
Describe the existing neighbourhood character (i.e. historic, single family, mixed use, etc.) and how the proposed development will enhance the adjacent neighbourhood		
Does the project enhance local identity and character consistent with government policy and plans (i.e. through architectural style, landscaping, colours, project name)?		
Contain elements of community pride and local character, such as public art?		

Leveraging Existing Infrastructure	Score	Description
Does the project contribute to heritage revitalization through the reuse, relocation, or rehabilitation of an existing structure, feature, or natural resource?		
Utilize pre-existing roads and services?		
Use locally sourced materials?		

2. CONNECTIVITY

Green and grey networks weave through the neighbourhood connecting the proposed site to surrounding neighbourhoods and the Town at large. Connections are clearly marked to implicitly guide pedestrians into and out of the site. Residents and visitors can travel easily through the neighbourhood despite their mode of transportation or accessibility.

Active Transportation	Score	Description
Incorporate community social gathering places (village square, halls, youth and seniors facilities, community boards)?		

Mixed Land Uses	Score	Description
Promote and improve trails and active transportation amenities?		
Provide additional support for alternative transportation use (walkways, cycling)?		

Green and Grey Networks	Score	Description
Link amenities such as school, trails, daily needs, public transit, and neighbourhoods through smart street and pedestrian design?		
If the property is adjacent to existing park space, open space, paths or trails, is a visual and pedestrian connection provided?		
Does the street layout and design encourage walking and cycling, while providing for personal and commercial vehicle use?		
Is there a interconnected road system with direct street connections, short block lengths, no cul-de-sacs?		

Alternative Forms of Transit	Score	Description
Accessible by multiple modes of transport, emphasizing public transit including future planned connections?		
Include a car share program?		
Strategic consideration for parking space requirements and placement within site (slanted/ shared spaces, rear parking)?		

Connected Green Network of Natural Assets	Score	Description
Create green spaces or strong connections to adjacent natural features, parks, and open space		

3. ENVIRONMENTAL + ECONOMIC SUSTAINABILITY

Environmental Sustainability refers to the provision and respect of the natural features that exist on the site, while considering innovative practices and solutions to harness new environmental opportunities. Developers shall consider existing natural assets on the site and exhibit time and consideration was paid to their best use and incorporation into the site.

Economic Sustainability refers to the financial viability of the proposed development for the CDD site. Developers must demonstrate consideration of residential density versus public infrastructure costs have been considered and the neighbourhood is fiscally feasible for future residents and the Town at large.

Environmental and Economic Sustainability are interlinked, as we live within a world of finite resources. Developers are encouraged to recognize the cost-effective ecosystem and municipal services already in place and consider how a project impacts services that make up local, regional, and national sustainability.

Environmental Consideration	Score	Description
Avoid contamination during construction?		
Provide on-site composting facilities?		
Provide an area for a community garden?		
Conduct an environmental assessment of the property prior to the commencement of design with a qualified professional?		

Working Landscapes	Score	Description
Cluster development on appropriate land to contain construction disturbance?		
Design attempts to maximize exposure to natural sun light (i.e. direction of rooms, placement of green space)		
Provide conservation measures for sensitive lands beyond those mandated by legislation?		

Low Impact Infrastructure	Score	Description
Reduce construction waste and enhance durability of materials used?		
Utilize environmentally sensitive or recycled materials?		
Plan to recycle water and waste water on-site?		

Stormwater Management	Score	Description
Use rocks and other materials in landscaping design that are not water dependent?		
Provide for zero stormwater run-off?		
Utilize natural systems for stormwater?		

Enhancing Natural Environment Features of High Ecological Value	Score	Description
Conserve, restore, or improve habitat (retention of trees, green space, and natural habitat)?		
Are there any significant existing environmental features that are maintained or enhanced on the site (trees, shrubs, sunlight, streams)?		
Account or attempt to mitigate light pollution within surrounding areas?		

Density	Score	Description
Anticipated price range of units (note price range for both commercial and residential units if applicable) - average price per square foot.		
Net increase to property tax base.		

Local Economy	Score	Description
Are there any commitments of direct employment created by this project during construction (number and duration of jobs)?		
Will the project provide direct employment (on the site) after the project is completed?		
Does this project have any features that will reduce the long-term costs to the community of operating and maintaining public services and infrastructure (i.e. reduce roads and pipes for servicing that will have to be maintained by the Town)?		
Are there any other financial components of economic sustainability advanced by this proposal?		

4. EQUITY

Neighbourhood design, provisions, and unit affordability account for individuals and households of various sizes and income levels. Where possible infrastructure and building practices will accommodate accessible building practices. Mixed housing types are available throughout the neighbourhood and take into consideration the Town of Wolfville's unique demographics and community culture.

Education Opportunities	Score	Description
Increase community opportunities for training, education and recreational activities?		
Does the project provide enhanced waste		

Mixed Housing Types	Score	Description
Does the project include rental housing units?		
Does the site include an array of housing forms relevant to the Town's needs? If yes, please list relevant forms, approximate numbers, and target population.		

Affordable Housing Supply	Score	Description
Does the project include rental housing units?		
Does the site include an array of housing forms relevant to the Town's needs? If yes, please list relevant forms, approximate numbers, and target population.		

Mixed Housing Types	Score	Description
Include provisioning for affordable housing units? If yes, please address the following points: - Percentage and number of units anticipated - Affordability time-frame guarantee (in years) - How will affordability be determined? - Anticipated price range of units (note different housing tenures)?		

Accessible Design and Built Form	Score	Description
Does the project incorporate features to enhance adaptability and accessibility within the proposed design standard for people with disabilities (i.e. wider door openings, reinforced walls in bathrooms for future installations, ground-oriented entrances)?		
Does the project and development account for the potential of repurposing units in the future (i.e. residential to commercial or home-occupancy)?		

Prioritize Active Shared Spaces	Score	Description
Does the development incorporate spaces for people of all different physical abilities?		
Are there public amenities (water fountains, washrooms, leisure infrastructure) provided in the development?		
Are residents, community stakeholders, and end-user groups involved in the planning and design process??		

Summary	Score	Description
Please calculate your final score and convey any final remarks, concerns, or additional information that you do not believe is addressed in the Checklist criteria, that you believe are an asset of pertinence to the proposed site design.		

Schedule E:

ENVIRONMENTAL STUDY REQUIREMENTS

In cases where an environmental study is required, the study shall include the following information:

INTRODUCTION AND PROJECT DESCRIPTION:

- Property owner and abutting owners
- Zoning
- Description of project
- Total land area of project
- Site plan, to scale, showing distances to watercourses, ponds, drainage channels, and existing and proposed slope of lands.

IMPACT DESCRIPTION:

Potential project affects upon:

- Susceptibility of flooding and potential floodwater flows
- Stormwater drainage and storage features on property and abutting properties
- Bearing capacity of soils
- Unstable slopes
- Other development constraints

MITIGATION MEASURES:

- A description of measures to overcome the constraints which are found to exist at the site.

DECLARATION:

- Time period study was conducted
- Date study was submitted
- Signature of property owner(s) and study author(s)
- Contact information for property owner(s) and study author(s)

ATTACHMENTS

- Attach any supporting documents

FLOODPLAIN

New development in areas within the floodplain as shown on Schedule "B", are required to sign a "*Floodplain Development Undertaking Form*" acknowledging recognition of risks and confirming that:

- (1) The finished floor elevation is no lower than 8m geodetic.
- (2) Walls and floors below 12m geodetic should be constructed to be flood tolerant.
- (3) Consideration be given to placement of mechanical equipment

Schedule F:

DOWNTOWN & NEIGHBOURHOOD DESIGN GUIDELINES



Disclaimer

These guidelines have been updated to reflect the practice of Design Review in the Town and align with the updated MPS & LUB.

These guidelines are draft and will undergo review by the Design Review Working Group, PAC, and Council.

These guidelines will be finalized before adoption of the new Planning Documents.

Town *of* Wolfville

DESIGN GUIDELINES

2025

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INTRODUCTION



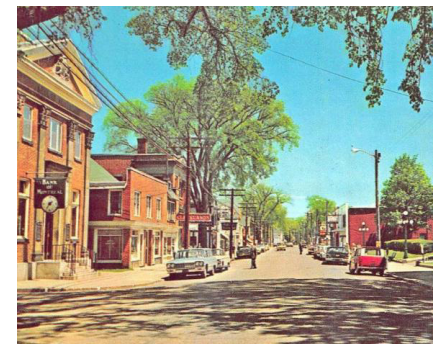
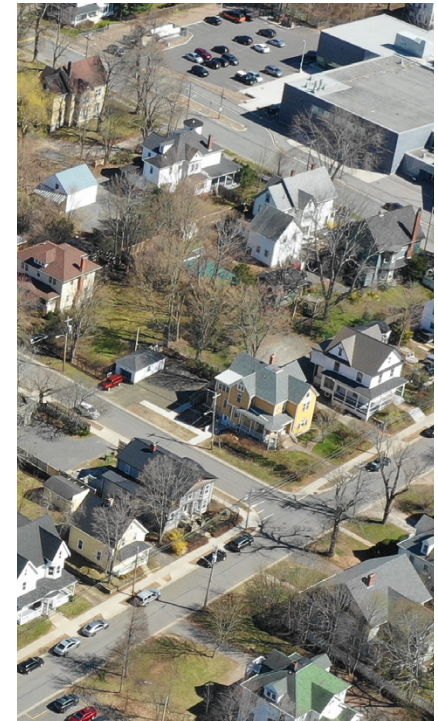
Chapter 1.0 Introduction

Wolfville takes great pride in its ability to shape its future and quality of life for residents and visitors to the Town. A sense of continuity in architectural style and thoughtful design is highly valued by residents and is immediately apparent to visitors.

The architectural history of the early Planter buildings remains from the early 1900s, whereas Mi'kmaq and Acadian heritage are not immediately evident in our built forms. Victorian buildings were added during the 19th Century, and the 1920s saw storefront architecture and several examples of the art deco and art nouveau styles and accommodated a surge of contemporary design from the post-war period to the present. The built form on the Acadia University Campus (past, present, and future) influences how people experience the Town and contributes to the overall experience. While there is respect for the Town's built heritage through a rich collection of Provincially and / or Municipally registered buildings (see Heritage Style Guide), there is nevertheless an eclectic nature

to the Town's built form born of its diverse historical and contemporary influences.

The 2025 Town of Wolfville Design Guidelines draw from the previous Residential Architectural Guidelines (1992), Downtown Architectural Guidelines (1992) and most importantly, take the experience from using the updated 2019 Design Guidelines to move stronger requirements into the Land Use By-Law while focusing on principles that will guide the review of development applications in both the Downtown and Neighbourhood Design areas.



1.1 DESIGN REVIEW WORKING GROUP

1.3.1 REVIEW PROCESS

The Design Review Working Group (as per the Town's Committees of Council Policy) advises Staff when reviewing applications in areas shown on the Design Review Areas Map. The Working Group also advises Staff on policy or regulatory change, from a design perspective.



DOWNTOWN DESIGN CHECKLIST

The following checklist shall be considered when an applicant in the Downtown area is being assessed with these guidelines.

- ✓ WELL DEFINED STREETWALL
- ✓ VISUAL RICHNESS
- ✓ PEOPLE-CENTRED DESIGN
- ✓ HERITAGE

NEIGHBOURHOOD DESIGN CHECKLIST

The following checklist shall be considered when an applicant in the Neighbourhood area is being assessed with these guidelines.

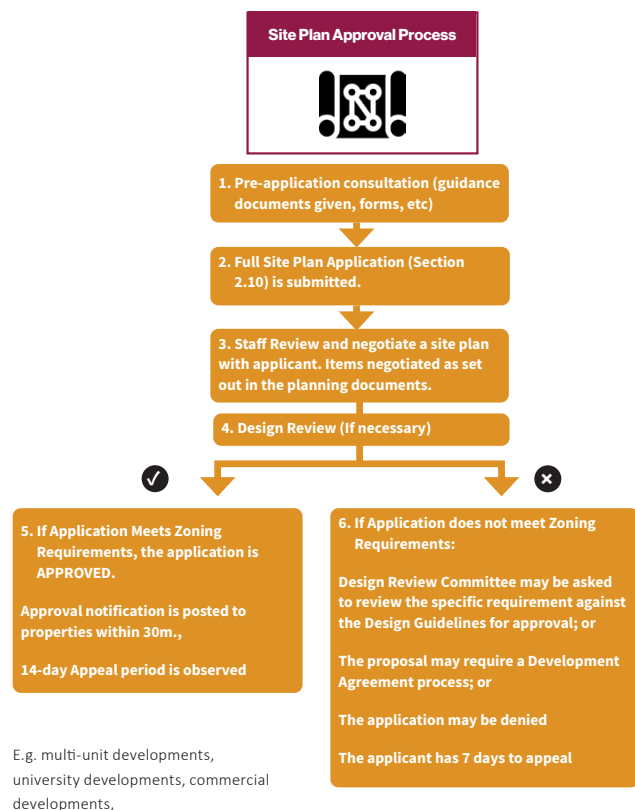
- ✓ EMPHASIZE NATURAL SETTING
- ✓ INFILL APPROPRIATELY
- ✓ ACCESS, UTILITY, PARKING
- ✓ NEIGHBOURHOOD HERITAGE



1.2 APPLICATION PROCESS

1.2.1 SITE PLAN

For Site Plan applications, Staff use the Working Group to support the negotiation of desired outcomes.



1.2.2 DEVELOPMENT AGREEMENTS

For Development Agreements, Staff package the Working Group feedback into the Planning Advisory Committee reporting, which informs recommendations to Council.



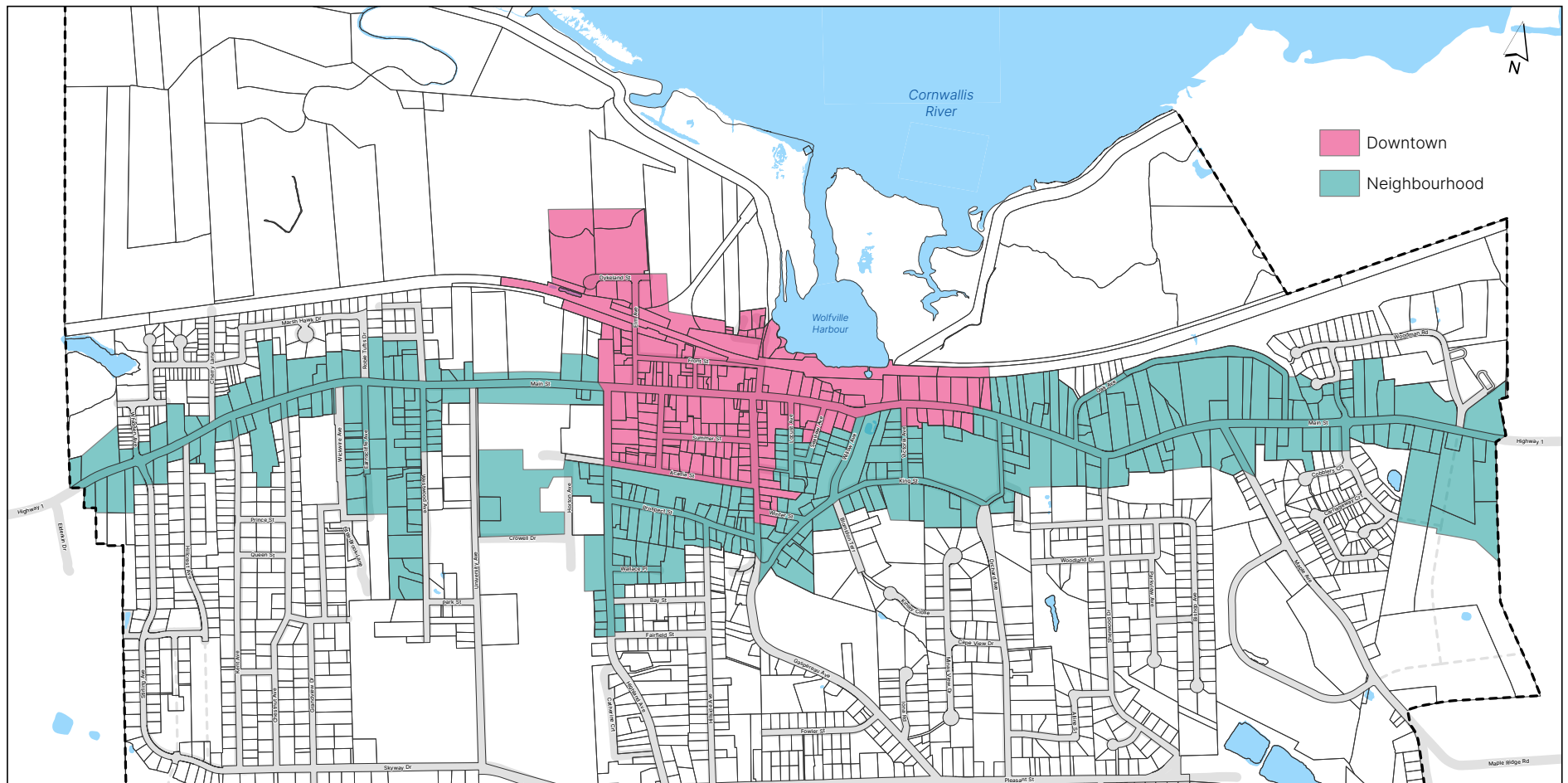
See the Municipal Planning Strategy and Land Use By-Law for more information on application processes.

An aerial photograph of a town, likely Wolfville, showing a dense residential and commercial area. A railway line runs diagonally across the lower left portion of the image. The town is characterized by numerous houses, some with gabled roofs, and larger commercial buildings. There are several parking lots and streets visible. The overall tone of the image is sepia or aged. The text "DESIGN REVIEW AREAS" is overlaid in large, white, sans-serif capital letters across the upper middle part of the image.

DESIGN REVIEW AREAS

Chapter 2.0 Design Review Areas

These design guidelines apply to two principle planning areas; the Downtown and the Neighbourhood areas as identified in the Generalized Future Land Use Plan.





DOWNTOWN DESIGN REVIEW

Chapter 3.0 Downtown Design Review

These Design Guidelines are grounded in principles of design that help support staff as they review applications. It is recognized that there are various ways a project may respond to these principles through good, thoughtful design in the Downtown context.

3.1 DOWNTOWN DESIGN PRINCIPLES



WELL-DEFINED STREETWALL

New buildings should align with existing buildings to create a streetwall that defines the edge of the public realm.

Streetwalls should be sympathetic to the scale of the traditional Main Street architecture where there is generally a three storey (or less) streetwall. Depending on the location, buildings taller than three storeys should look to integrate a step back or other tools above the third story and along the street to maintain the pedestrian scale of the downtown.



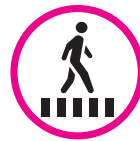
VISUAL RICHNESS

Blank walls facing the public realm are discouraged.

Buildings facades should include colour, articulation, and fine-grain detailing that builds on vernacular examples in the vicinity of the site.

Commercial store fronts should be designed with a width of 7-8m to encourage regular access into commercial businesses along the Street.

Create space outside of the Right of Way for flower boxes, decorative plants, and / or trees.



PEOPLE-CENTRED DESIGN

Building design can improve the quality of the environment for people while they take in the Town at a walking pace.

Building facades should be principally occupied by windows that allow connection between private commercial space and the public realm (e.g. non-opaque glazing).

Mechanical and other utility equipment shall be located away from the public realm.

Upper storey uses that form part of the streetwall should contribute to the life of the street. Recessed patios, operable windows, and the associated architectural detail of these features can enhance the connection between the public realm and the private uses (residential or commercial).



HERITAGE

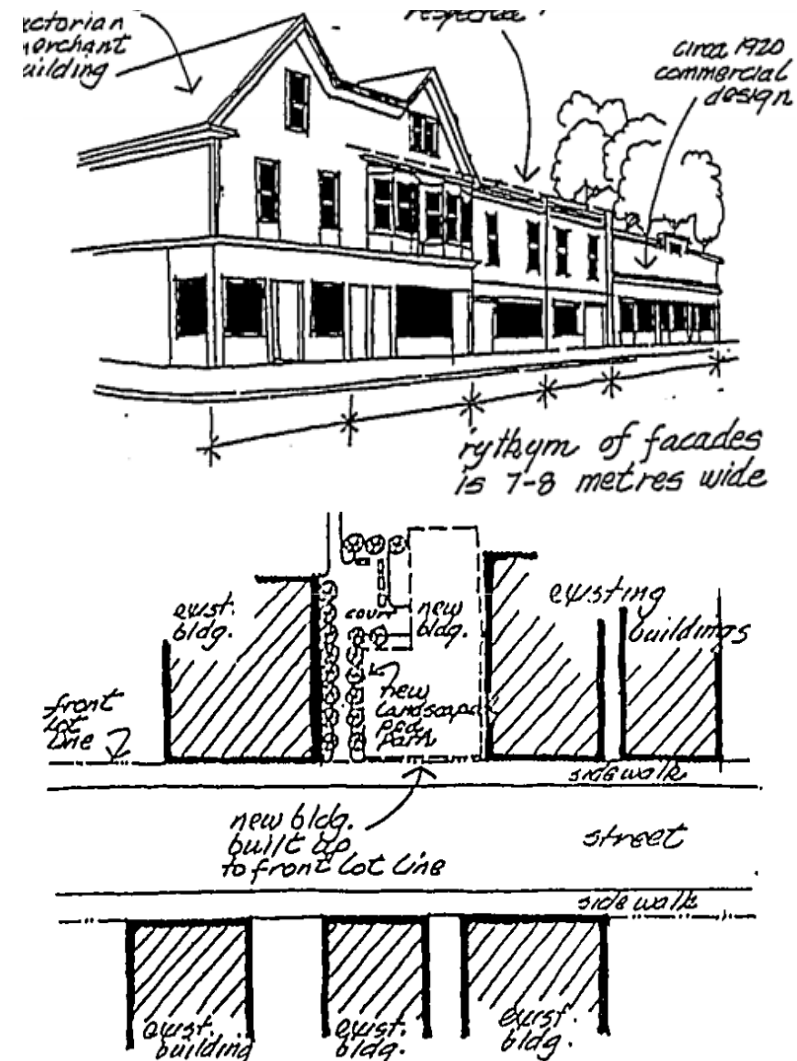
Draw on design motifs from the street. Take note of decorative brackets, corner boards, horizontal articulations lines, and entry pediments as examples.



3.2 DOWNTOWN DESIGN AREA DETAIL

3.2.1 WELL DEFINED STREETWALL

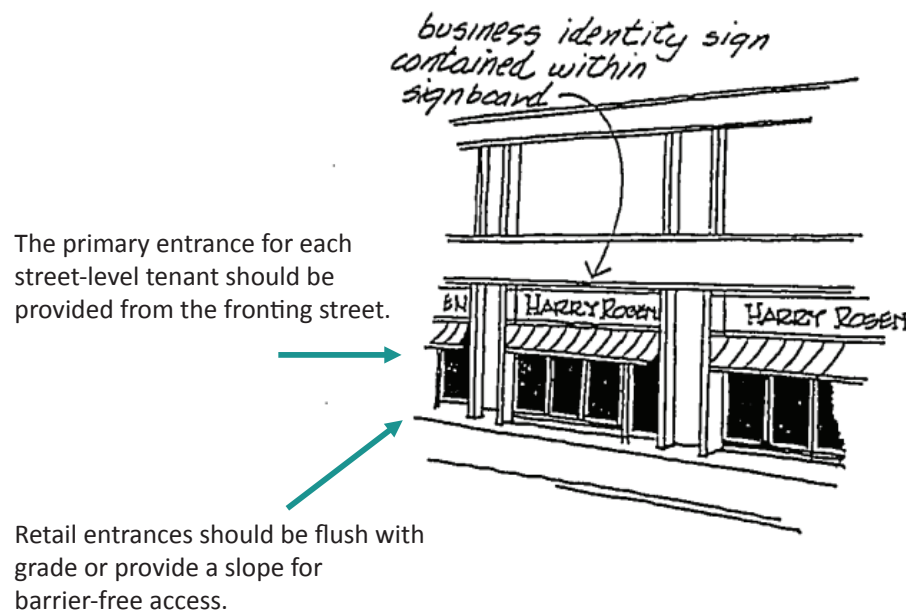
- New buildings should reflect the local spatial arrangement and separation distances of the existing street wall.
- New development should reflect the existing streetwall height, through overall building height or building setbacks.
- New buildings and major additions should be constructed to the front lot line.
- Orient and position buildings to define the streetscape on public streets and internal driveways to assist in creating active pedestrian environments.
- Large side yards in the streetscape should be avoided as they can create undesirable gaps within the streetscape.
- Sites with a change of grade should not be limited as long as the street-facing façade and massing adheres to these guideline.
- The floor-to-floor height of the ground floor should be 4.5 metres to support retail usage at grade.
- Common height lines of adjacent structures should be respected so that new construction can blend within the existing streetscape.
- If new development exceeds the height of adjacent buildings, setbacks will be used for an appropriate transition of scale.
- The primary entrance for each street- level space should be provided from the fronting street.





3.2.2 VISUAL RICHNESS

- Building façades should be designed with a variety of architectural elements including wall planes and roof lines, human scale proportions, large windows and porches/entryways. Avoid minimizing retail views and frontage with large columns and building extrusions.
- New construction should respect the traditions of first floor base lines, large storefront windows, recessed entries, and vertical window orientation on the second and third floors.
- The tradition of approximately 75% of the wall surface being occupied by windows should be respected for the principle façades at the ground floor level.
- For active commercial and retail uses at grade include transparent glazing 0.5 metres above grade and at least 2.5 metres in height.
- In corner conditions, where active uses are required, commercial or office uses should wrap the corner.
- Ensure retail entrances are located at or near grade. Avoid split level, raised or sunken retail entrances except where necessary due to sloping conditions.
- Provide architectural expression and design elements such as cornice lines, window bays, entrances, canopies and fenestration in pattern, scale and proportion that relate to neighbouring buildings and engage the pedestrian realm.
- Common height lines of adjacent structures should be respected so that new construction can blend within the existing streetscape.
- Long monolithic façades should be avoided with a maximum horizontal width of 7-8 metres wide.
- If the façade exceeds 7-8 meters include a series of bays defined by vertical elements such as changes in materials, building projections, columns or other vertical architectural elements.

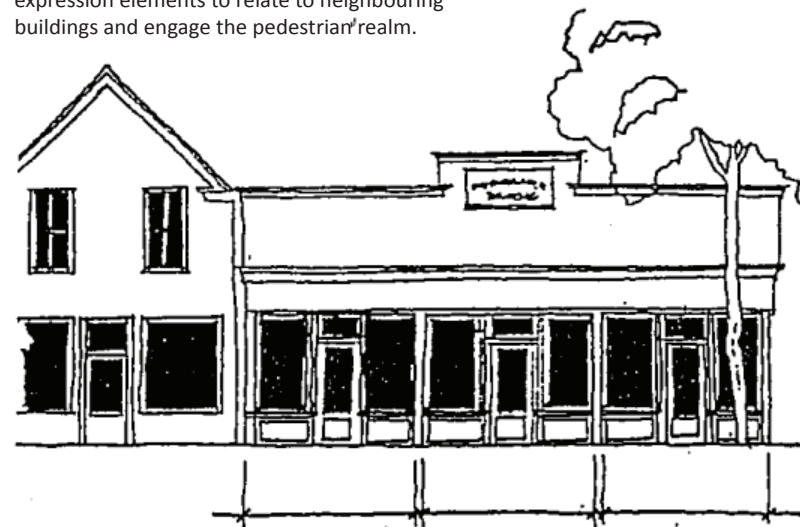




3.2.3 PEOPLE CENTRED DESIGN

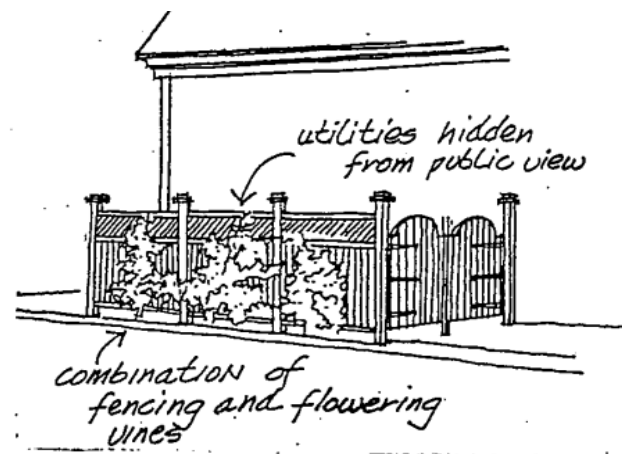
- Along commercial / retail street frontages, avoid projecting balconies on the front façade of the building.
- Wherever possible, locate surface parking areas behind buildings and screened from the view of the public realm.
- Parking areas should be organized into small bays, rather than large surface lots.
- Rear lane access to parking amenities is preferred with the number of vehicular entrances held to a minimum. Vehicular access shall be from an alley or mid-block connection on a connecting street and curb cuts and vehicular entries should be limited to the minimum width required.
- Inset or partially inset balcony arrangements which are integrated into the façade of the buildings are preferred.
- Minimize visual impact through the use of clear glazing on all public facing facades.
- Avoid large blank walls or garages that dominate the frontage. This principle enhances personal safety by ensuring continuous visibility between residents and the public realm.
- Prioritize the pedestrian experience, establish clear building orientation, and prevent entrances from being obscured by garages or landscaping, thereby enhancing the legibility and safety of the street.
- Integrate permanent, recessed awnings, canopies, or colonnades that project over the public sidewalk for pedestrian protection from sun and rain. The design and height must accommodate pedestrian flow and street trees.
- Window placement, size, material and style should help define architectural style and integrity.

Include vertical window orientation and architectural expression elements to relate to neighbouring buildings and engage the pedestrian realm.



Long monolithic facades should be broken up by a series of vertical architectural elements and projections.

tradition of articulated facades



utilities hidden from public view

combination of fencing and flowering vines



3.2.4 HERITAGE

WINDOWS AND DOORS

- The original windows and doors should be repaired and enhanced if possible. If they must be replaced, the new windows and doors should replicate the existing configuration and materials used in the original construction.
- The proportion of glazing areas in heritage infill should respect the pattern of windows of its neighbouring buildings

CORNICES

- Cornices and roof lines should be repaired using similar materials to the original structure.
- Cornices of neighbouring buildings should be mirrored or referenced in new construction.

MATERIALS

- Repairs and renovations to existing heritage buildings should use the same or similar materials to the original structure.
- Complimentary accent materials include: steel, copper, glass and painted or unpainted wood.
- Materials that should be avoided are: stucco, vinyl siding, exterior insulation and highly reflective or tinted glass.
- Infill buildings should respect the neighbouring historic material uses by using similar or complimentary materials in construction.
- Complimentary accent materials include: steel, copper, glass and painted or unpainted wood.
- Historic materials in the Core Commercial Area include: brick, wood detailing, wood siding, and stone.
- Materials that should be avoided are: stucco, vinyl siding, exterior insulation and highly reflective or tinted glass.



A photograph of a street scene with a building facade, string lights, and a sign for 'ARCHANT Wine TAVERN'. The image is overlaid with a teal color filter. The text 'NEIGHBOURHOOD DESIGN REVIEW' is centered in white, uppercase letters.

NEIGHBOURHOOD DESIGN REVIEW

Chapter 4.0 Neighbourhood Design Review

These Design Guidelines are grounded in principles of design that help support staff as they review applications. It is recognized that there are various ways a project may respond to these principles through good, thoughtful design in the Neighbourhood context.

4.1 NEIGHBOURHOOD DESIGN PRINCIPLES



EMPHASIZE NATURAL SETTING

Buildings should be oriented and positioned in a way to maximize the advantage of existing and new tree canopy.

Position and design buildings to respect the existing terrain, minimizing the need for extensive grading or large retaining walls.

Use materials and colours that harmonize with the local environment.

Public views to the Harbour should be maintained and enhanced from neighbourhoods where those views are present today.

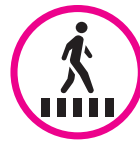


INFILL APPROPRIATELY

New accessory and main buildings shall be located in side and rear yards. These buildings should not be permitted in front or flankage yards.

Encourage infill that offers a diversity of housing types (e.g., laneway housing, duplexes, or small-scale multiplexes) that are compatible in scale, providing options for different household sizes while maintaining the low-density feel.

Require new construction to use durable, attractive, and contextually appropriate materials and architectural details to ensure longevity and seamless integration with the existing neighbourhood aesthetic and vernacular architecture.



ACCESS, UTILITY, PARKING

Require off-street parking to be located in the rear or side yard (e.g., via laneways or tucked-under garages) to prevent garage doors and driveways from dominating the streetscape.

Screen driveways from neighbouring properties, and screen mechanical and utility equipment from public view.



NEIGHBOURHOOD HERITAGE

Encourage the adaptive reuse of existing heritage structures, allowing them to gain new functional lives while preserving their architectural integrity, rather than defaulting to demolition.

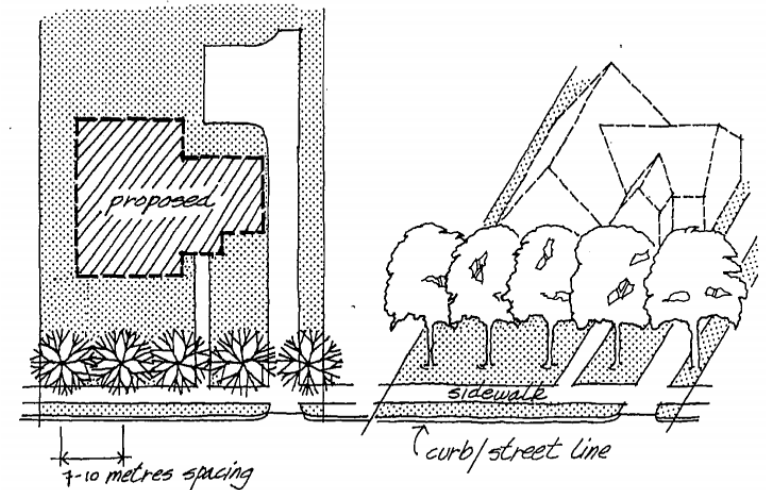
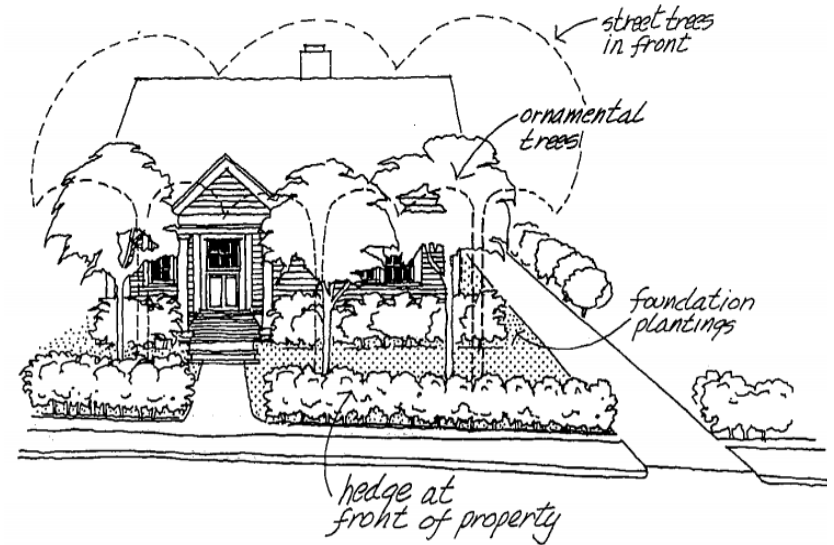
For repairs, alterations, or new infill, owners should use durable, natural, and historically compatible exterior materials (e.g., wood siding, masonry, appropriate roofing) and should be discouraged large, uninterrupted areas of vinyl or pre-finished metal that conflict with the established neighbourhood character.

4.2 NEIGHBOURHOOD DESIGN AREA



4.2.1 EMPHASIZE NATURAL SETTING

- Prioritize the retention and protection of all healthy, mature trees on your property, especially those in the front yard. Implement construction zone fencing to prevent root damage during construction.
- Minimize the total area covered by impervious surfaces (driveways, patios, large roof extensions). Aim to maximize the soft-landscaped green space available for rainwater absorption.
- Ensure your front yard remains primarily green space, limiting the encroachment of parking and maximizing the area of grass, garden beds, and trees.
- Front yards should be landscaped using native or regionally adapted plants in your landscaping to minimize water consumption, reduce the need for fertilizers, and support local ecology.
- Consider street-trees and foundation plantings around structures.
- Use trees, hedges, or shrubbery to define street edge (see image to right)
- Design your project to minimize the need for massive excavation or infill. Use construction techniques, such as step-down foundations, that allow the building to follow the natural contours of the land.





4.2.2 INFILL APPROPRIATELY

- When building new or significantly expanding, align front setback (distance from the street) to be consistent with the established pattern of your direct neighbours to maintain a unified street face.
- Design infill buildings to be good neighbours by minimizing shade, shadow and overlook onto adjacent properties.
- Design buildings for durability and sustainability.
- Ensure the garage does not dominate the pedestrian experience.
- Consider windows in the attic of the garage or shed, where a loft or second suite can be located.
- Select high-quality, long-lasting exterior materials (e.g., wood siding, brick, natural stone) that complement the texture and finish of older homes in the neighbourhood.
- Ensure the size, shape, and vertical orientation of your windows relate proportionally to those on nearby heritage homes. Avoid large, uninterrupted sheets of glass on the primary facade.
- Design the height of your main floor and the height of your eaves/roofline to either match or transition respectfully to the corresponding elements on adjacent buildings.
- Maintain a scale of development that is compatible with neighbouring context.





4.2.3 ACCESS, UTILITY, PARKING

- Ensure the main path from the public sidewalk to your front door is clear, direct, and universally accessible. Make this route feel more important than the route to the garage.
- Where possible, utilize a shared driveway with a neighbour or restrict your driveway to a single-vehicle width to minimize pavement and the number of breaks in the curb/sidewalk.
- Long-term parking should be located in the side or rear of the lot.
- Driveway widths should be minimized to preserve the street-edge.
- Break-up or hide parking areas with vegetation to enhance walkability.
- Locate all service elements (waste bins, utility meters, HVAC units) in the side or rear yard and use high-quality, opaque screening (fencing or landscaping) to shield them from public view.
- Minimize exterior stairs, and when necessary locate them in the rear yards.
- Incorporate direct barrier-free and convenient pedestrian circulation routes within surface parking lots.
- Accessible parking spaces should be available in close proximity to barrier-free access ways.
- Barrier-free building entrances should be provided within new construction.
- Architectural treatments may include the use of prominent sills, mouldings, over boxes, soldier courses, and/or shutters.





4.2.4 NEIGHBOURHOOD HERITAGE

- Before making major changes, determine if property has any heritage designation or recognized value.
- When maintaining or renovating a heritage feature (e.g., original windows, porch railings, siding), always choose repair and conservation using traditional methods and materials over full replacement.
- Protect and retain the character-defining elements of your historic home (e.g., trim, siding pattern, chimneys) as these are the features that give the property its historic value.
- Protect and maintain any historic landscape features such as stone walls, large specimen trees, or original plantings that contribute to the setting of the heritage home.
- The preference is for use of traditional materials such as wood, brick or stucco.



HERITAGE ARCHITECTURE STYLE GUIDE

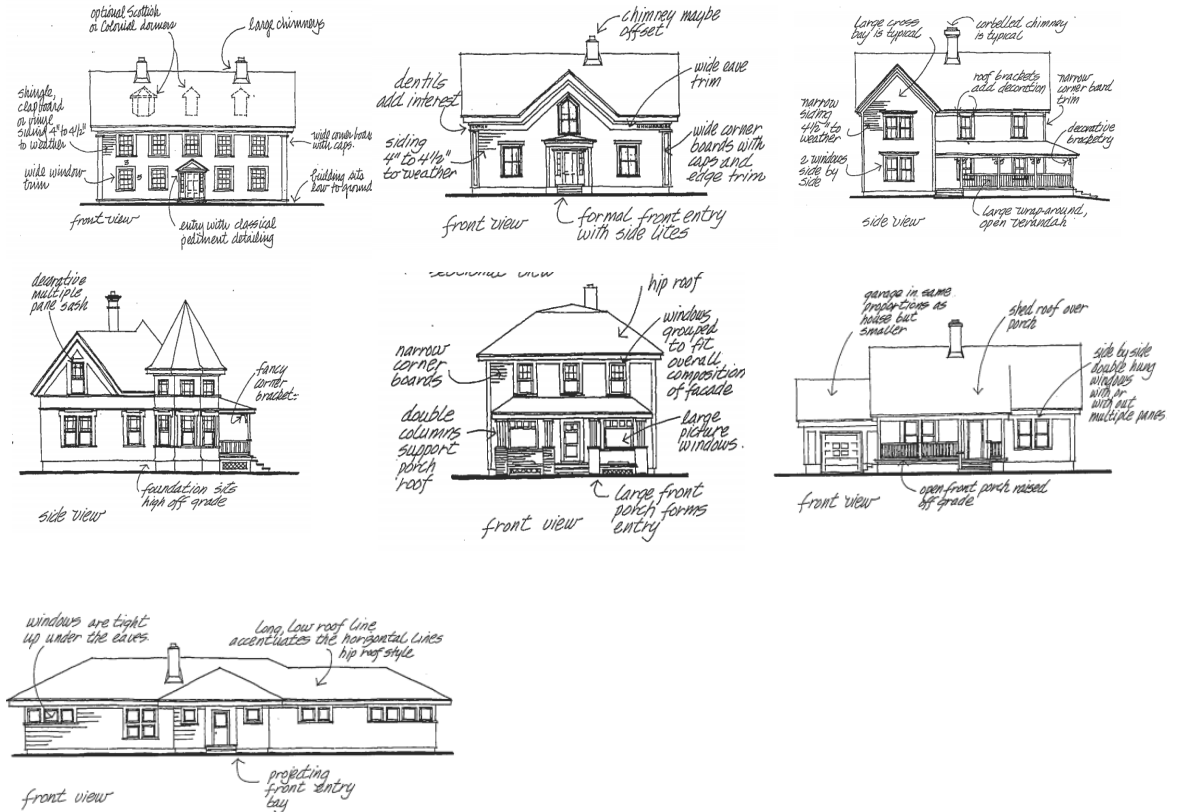
Chapter 5.0 Heritage Architecture Style Guide

4.1 HISTORY OF HERITAGE

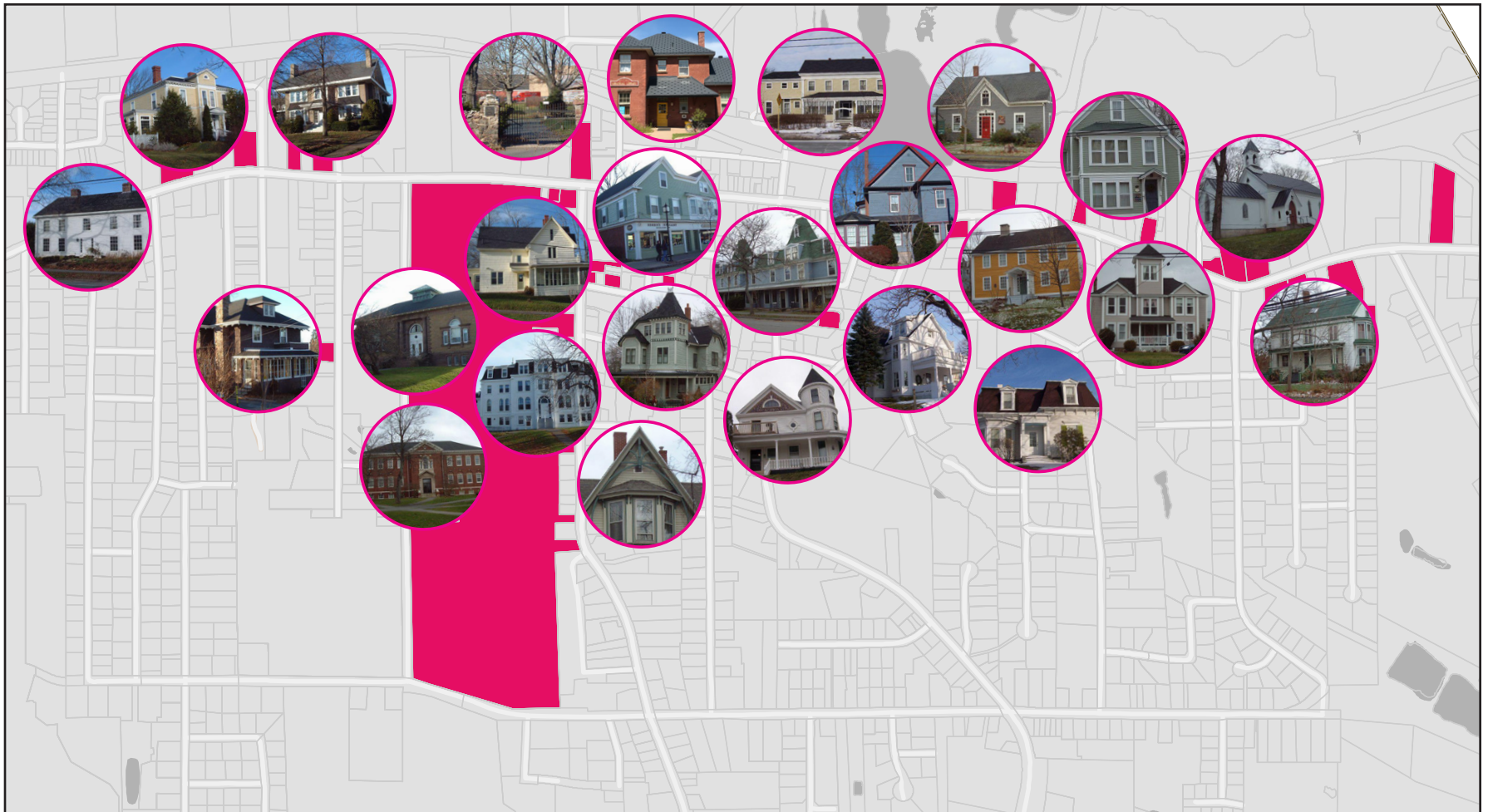
The Town of Wolfville's heritage buildings and cultural built forms are recognized as a contributor to the unique character of the Town.

The work completed for the 1992 Design Guidelines represent a wealth of community interest and knowledge of heritage forms and recognizes the diverse architectural change that occurred in Wolfville, sometimes down to the decade.

This style guide is included to educate individuals on the historic built forms frequently found in Wolfville.



5.1.1 EXISTING REGISTERED HERITAGE PROPERTIES



5.1.2 GEORGIAN (1750-1850'S)



Proportion/Scale Large, simple rectangular proportions. Pure forms without additions or variations in shape. One and one half to two and one half storeys. Exception: Many on East Main Street have large additions.

Materials Heavy timber construction, finished in shingles or wood siding. Exception: One such house on Main Street has vinyl siding.

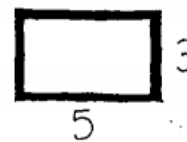
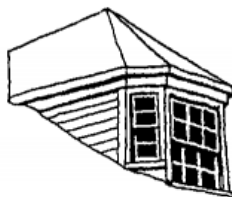
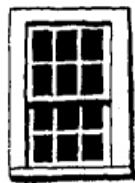
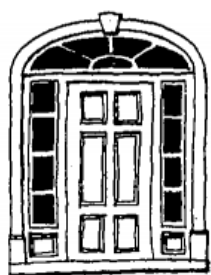
Roof Pitches Broad roof with moderate pitch. Exception: Some roofs are truncated and some are hipped.

Windows Six over six double-hung windows. Exception: The Georgian house on Main Street has had the original windows replaced by contemporary windows. Exception: The Georgian house on Westwood Avenue has Scottish dormers.

Porches/Verandahs One of the few examples of housing styles in Wolfville that did not originally have a front porch or verandah.

Decorative Elements Classically detailed wide corner boards and cornices, with transoms and sidelights at the front doors. The Georgian house on Westwood Avenue and Alumni Hall on Main Street have shutters.

Colours Painted traditional white. Exception: One such house on Main Street has white and grey vinyl siding. Georgian houses on east Main are painted an array of colours such as peach and grey.

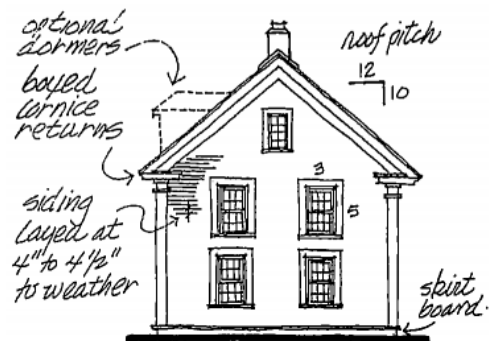


5.1.3 GEORGIAN (1750-1850'S)

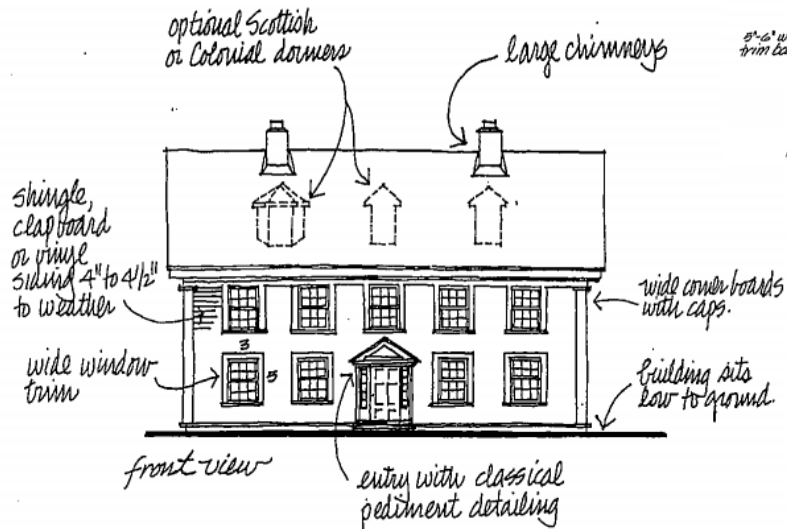


height of Georgian house is normally 2 1/2 storeys but maybe 1 1/2 storeys.

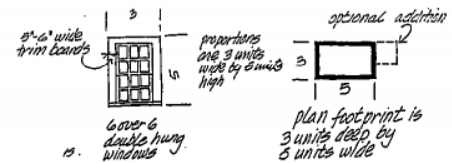
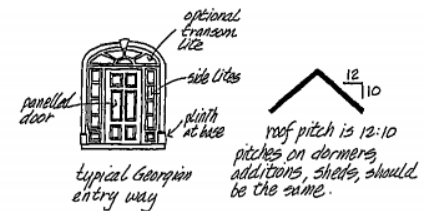
elevation view



end view



front view



5.1.4 VICTORIAN GOTHIC (1855 – 1875)



Proportion/Scale Simple rectangular proportions. One and one half to two and one half storeys.

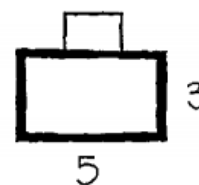
Materials Heavy timber construction, finished in shingles or wood siding.

Roof Pitches Main roof is broad with a moderate pitch with steep pitches on the front gables.

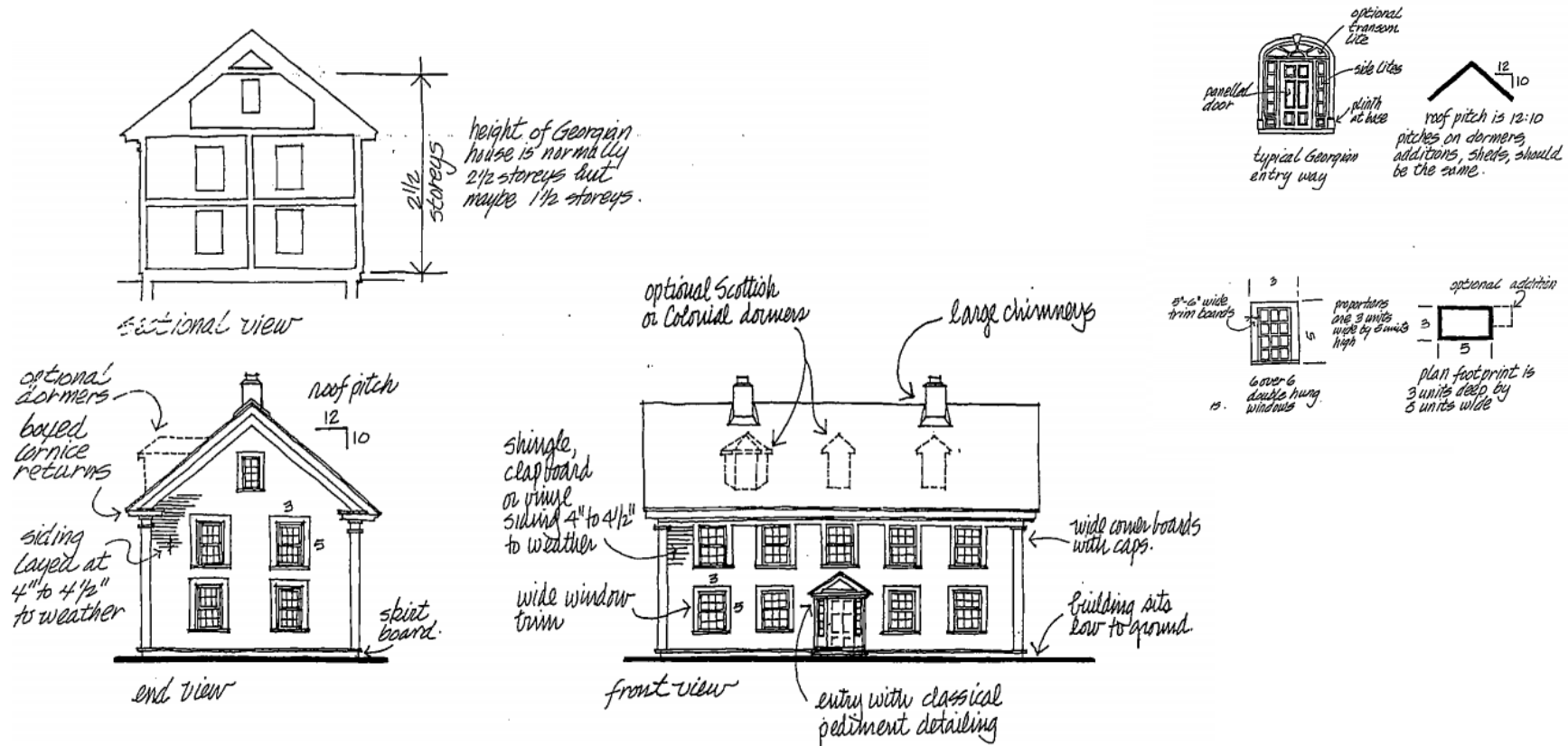
Windows Six over six double hung windows. The narrow pointed windows in the front gables is a distinguishing Gothic feature.

Porches/Verandahs Covered front porches. Some are enclosed.

Decorative Elements Brackets under the eaves, and wide corner boards Classically detailed. Some have shutters on the windows.



5.1.5 VICTORIAN GOTHIC (1855 – 1875)



5.1.6 CLASSICAL REVIVAL (1880 – 1900)



Proportion/Scale Two and one half storeys.

Materials Wood siding. Exception: Vinyl siding.

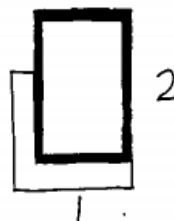
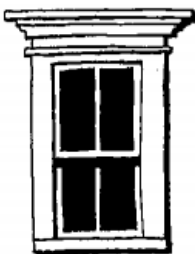
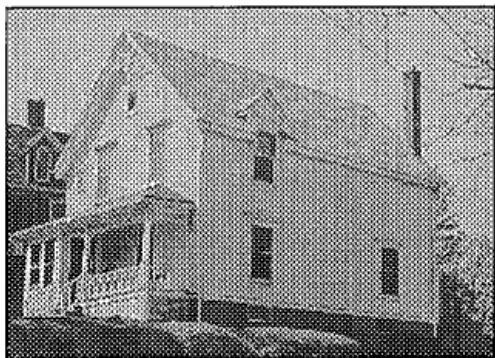
Roof Pitches High pitched with full height attics.

Windows Large one over one or two over two double hung windows.

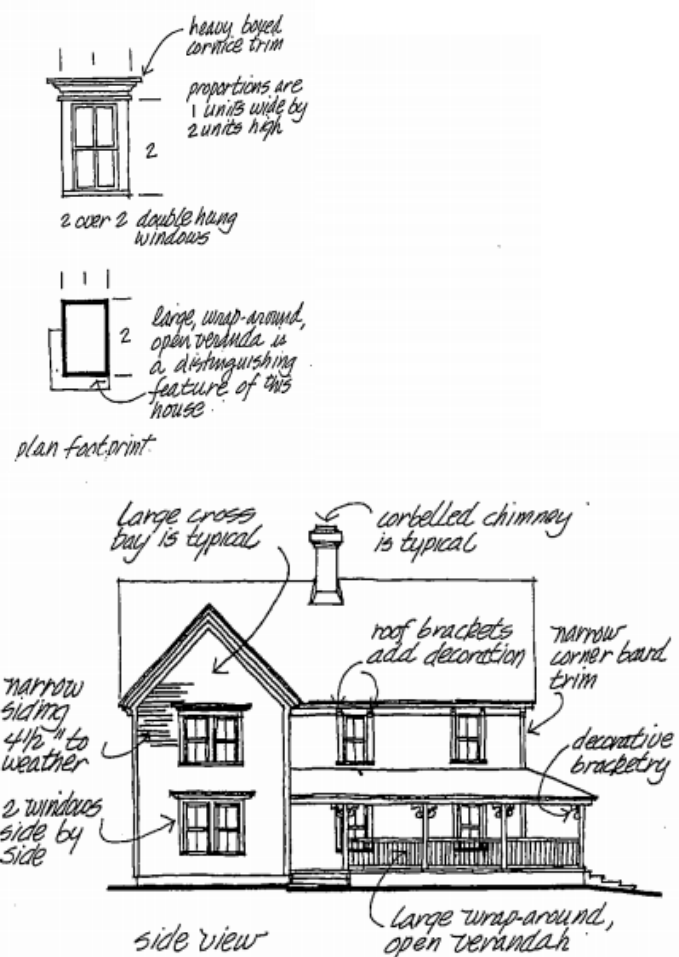
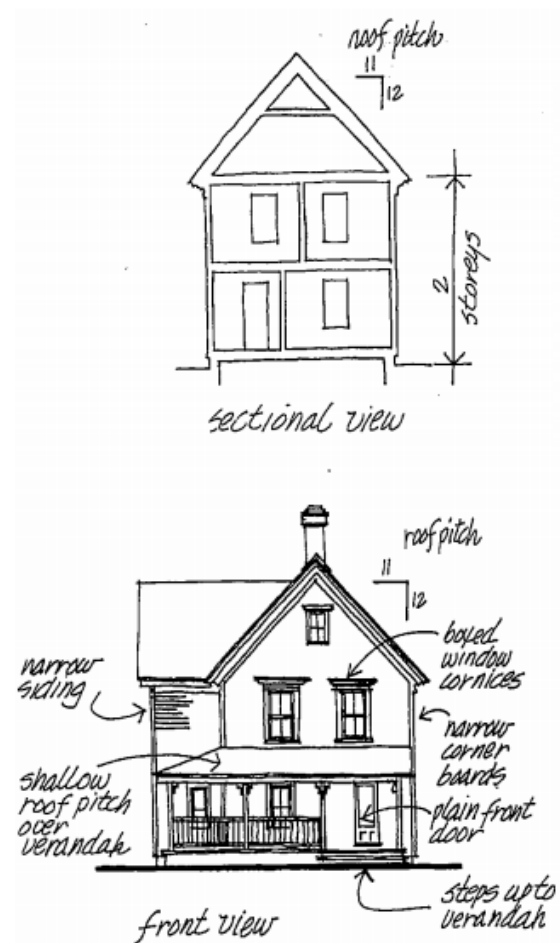
Porches/Verandahs Large open covered porches, across the front or wrap around two sides of the house. Exception: Some porches have been removed.

Decorative Elements Wide column like corner boards that are topped with capitals. Pediments over the windows.

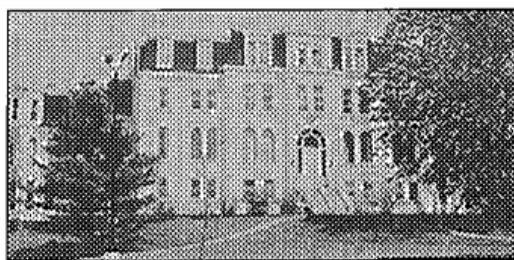
Colours Often white, but some are blue, green, yellow, pink or brown.



5.1.7 CLASSIC REVIVAL (1880 – 1900)



5.1.8 MANSARD (1870 – 1885)



Proportion/Scale Wide and rectangular with a symmetrical shape. Two to three storeys.

Materials Wood construction with wood siding.

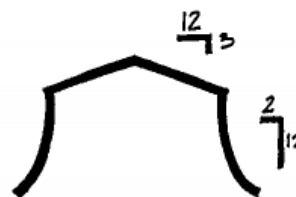
Roof Pitches A double roof pitch, with the lower pitch concave and very steep, and the upper pitch very shallow or moderate. This roof shape defines the style of Mansard.

Windows One over one or two over two double hung windows.

Porches/Verandahs Many covered verandahs, often wrapping around the house.

Decorative Elements Heavy cornice details, with intricate Victorian detailing on the verandahs. Horizontal banding through the upper portions on the main and second floor.

Colours Peach, white or grey.



5.1.9 QUEEN ANNE REVIVAL (1890 – 1900)



Proportion/Scale Several elements of different sizes that are arranged in an asymmetrical composition. Massing is stretched vertically. Two to three storeys.

Materials Wood construction with wood siding. Exception: Vinyl siding.

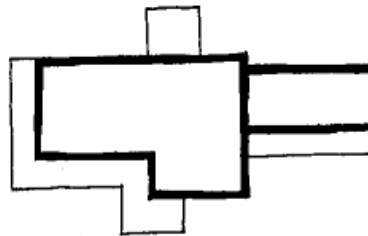
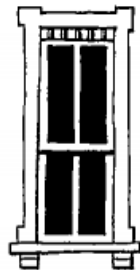
Roof Pitches Many converging roofs of different sizes and steep pitch.

Windows One over one or two over two double hung windows. There is some stained glass.

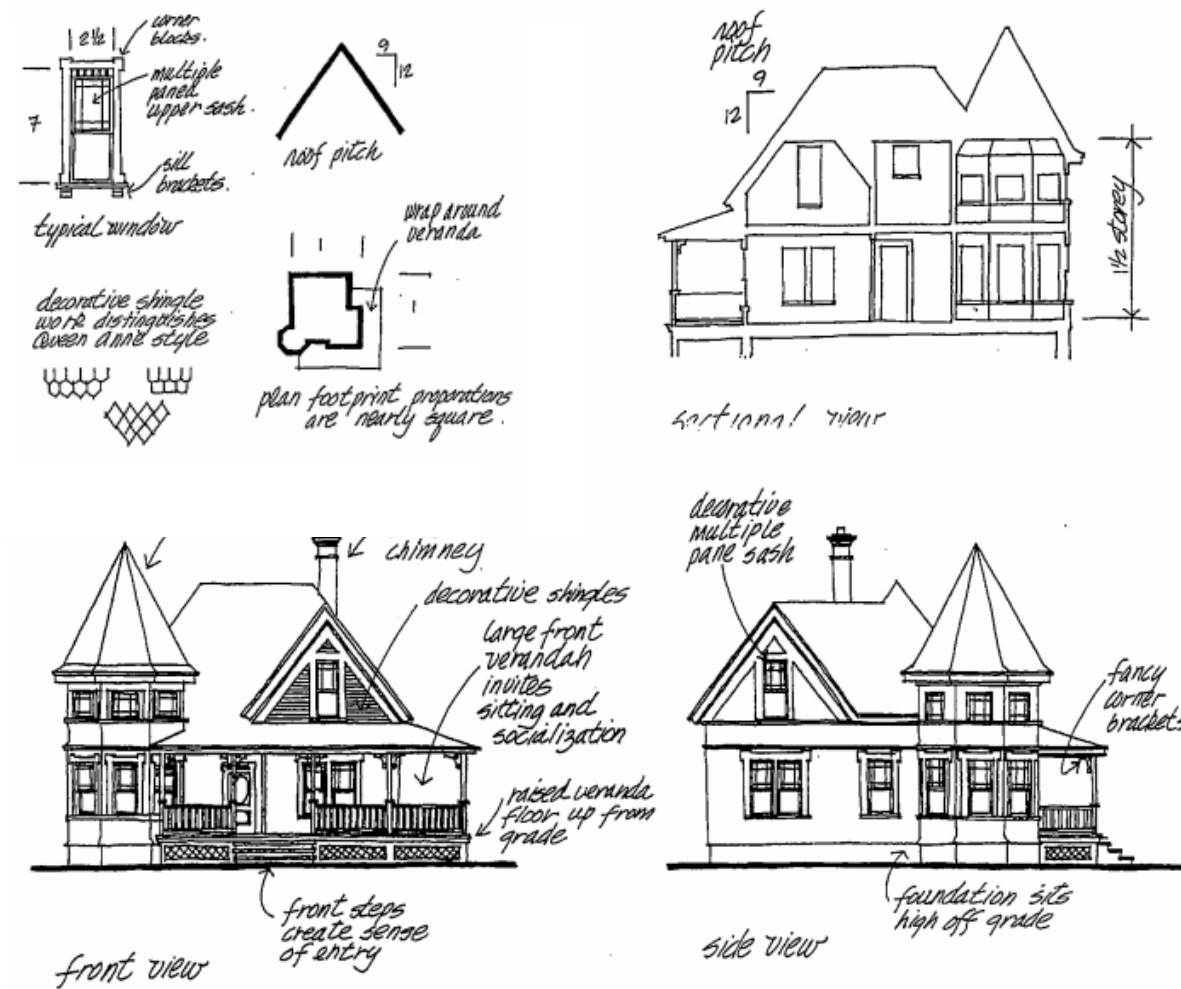
Porches/Verandahs Many covered verandahs, often wrapping around the house.

Decorative Elements Gingerbread trim and corner towers are common. Patterned shingle work.

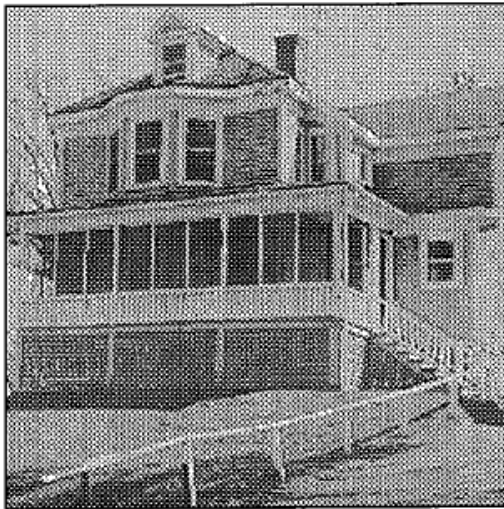
Colours Multiple colours with a dark colour for the body of the house. Exception: Some are painted white.



5.1.10 QUEEN ANNE REVIVAL (1890 – 1900)



5.1.11 MODIFIED QUEEN ANNE (1900-1930'S)



Proportion/Scale Large and rectangular with often exaggerated overhangs. Formal composition with a central main door. Two to two-and-one-half storeys.

Materials Wooden shingles, sometimes in alternating row widths. Exception: Some are veneered in stucco or vinyl sided.

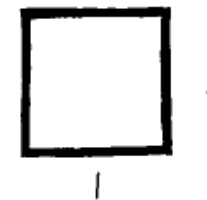
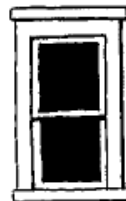
Roof Pitches Moderate, often four sided roofs.

Windows Windows are large and wide, often with divided lights in the upper portions of the windows, and a singular large pane in the bottom. Also a variety of double hung windows.

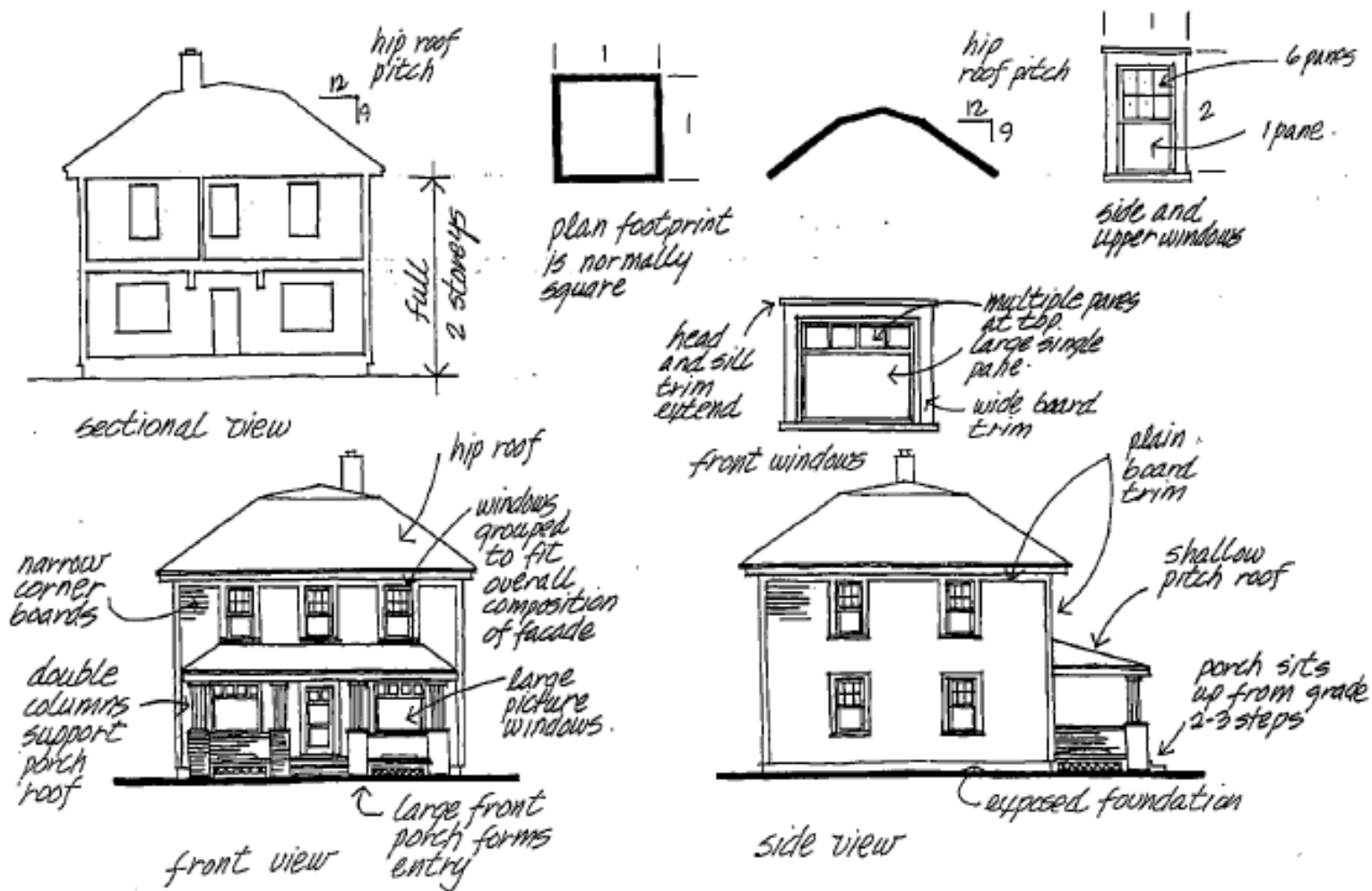
Porches/Verandahs Wide open porches that often stretch across the entire front of the house.

Decorative Elements Large eave brackets on the later houses of this period. As a reaction against decoration of the Queen Anne period, the Queen Anne Modified has little or no decoration.

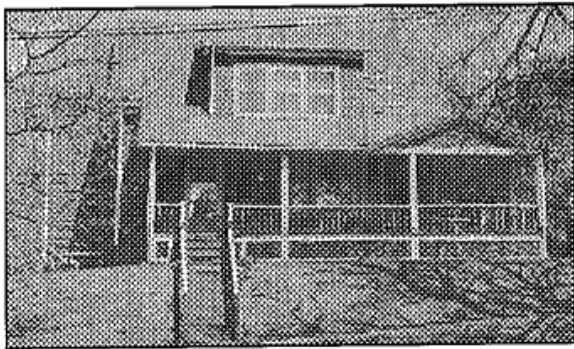
Colours Often white, brown, grey or sandy colours.



5.1.12 MODIFIED QUEEN ANNE (1900-1930'S)



5.1.13 POST WAR (1945 – 1955)



Proportion/Scale Modest in scale. Range from one to two storeys.

Materials Wood siding with a wide profile. Exception: Vinyl siding.

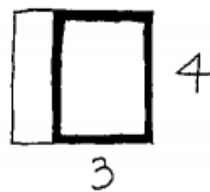
Roof Pitches 12:12 pitches or shallower. Some are hipped.

Windows Wide double hung, with a range of numbers of lights. Some picture windows.

Porches/Verandahs Many have small enclosed porches on the front.

Decorative Elements Often no corner boards, and no decoration other than shutters.

Colours White, brown, blue, yellow or green.



5.1.14 MODIFIED CAPE COD (1950-1970)



Proportion/Scale One and one half storeys.

Materials Wood siding with a wide profile. Exception: Vinyl siding.

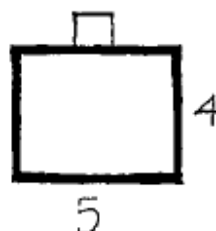
Roof Pitches 12:12 pitches or shallower.

Windows Wide double hung, with a range of numbers of lights. Some picture windows.

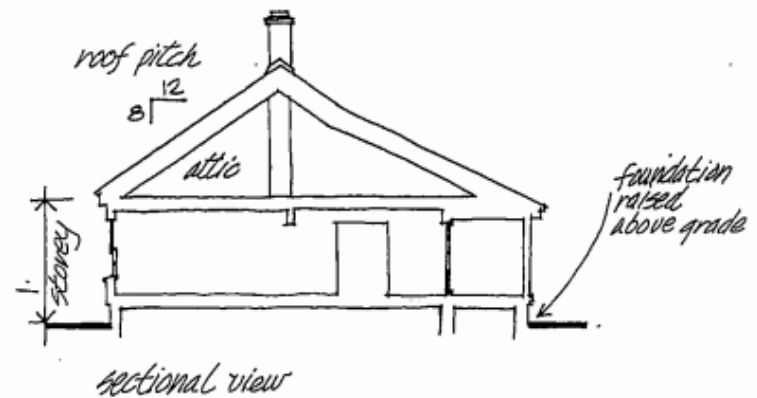
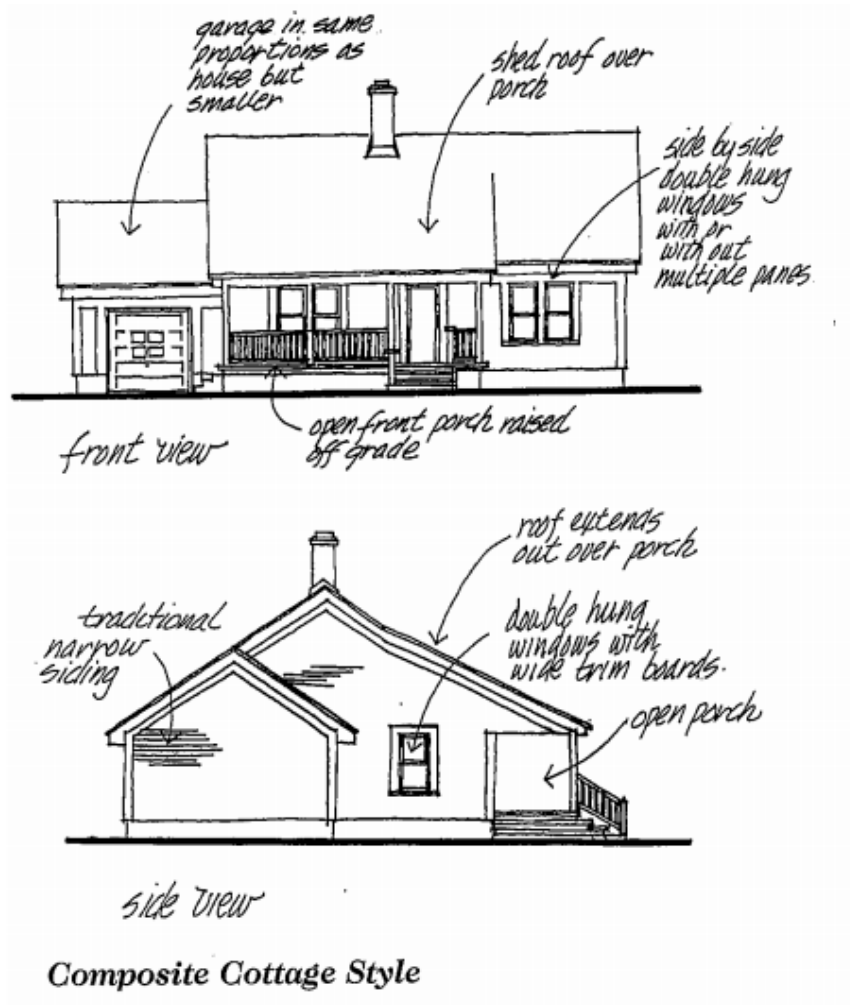
Porches /Verandahs Many have small enclosed porches in the centre of the front.

Decorative Elements Often no corner boards, and have no decoration other than shutters.

Colours White with black shutters.



5.1.15 MODIFIED CAPE COD (1950-1970)



5.1.16 RANCH STYLE BUNGALOW (1950-1975)



Proportion/Scale Long and low, often "L" shaped. One storey.

Materials Wide profile wood siding. Exception: Vinyl siding.

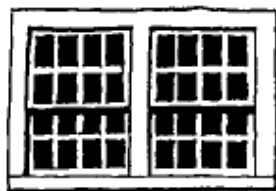
Roof Pitches Very shallow to moderate, often four sided, with broad overhangs.

Windows Wide, one over one double hung windows. The later ones have horizontal sliding windows.

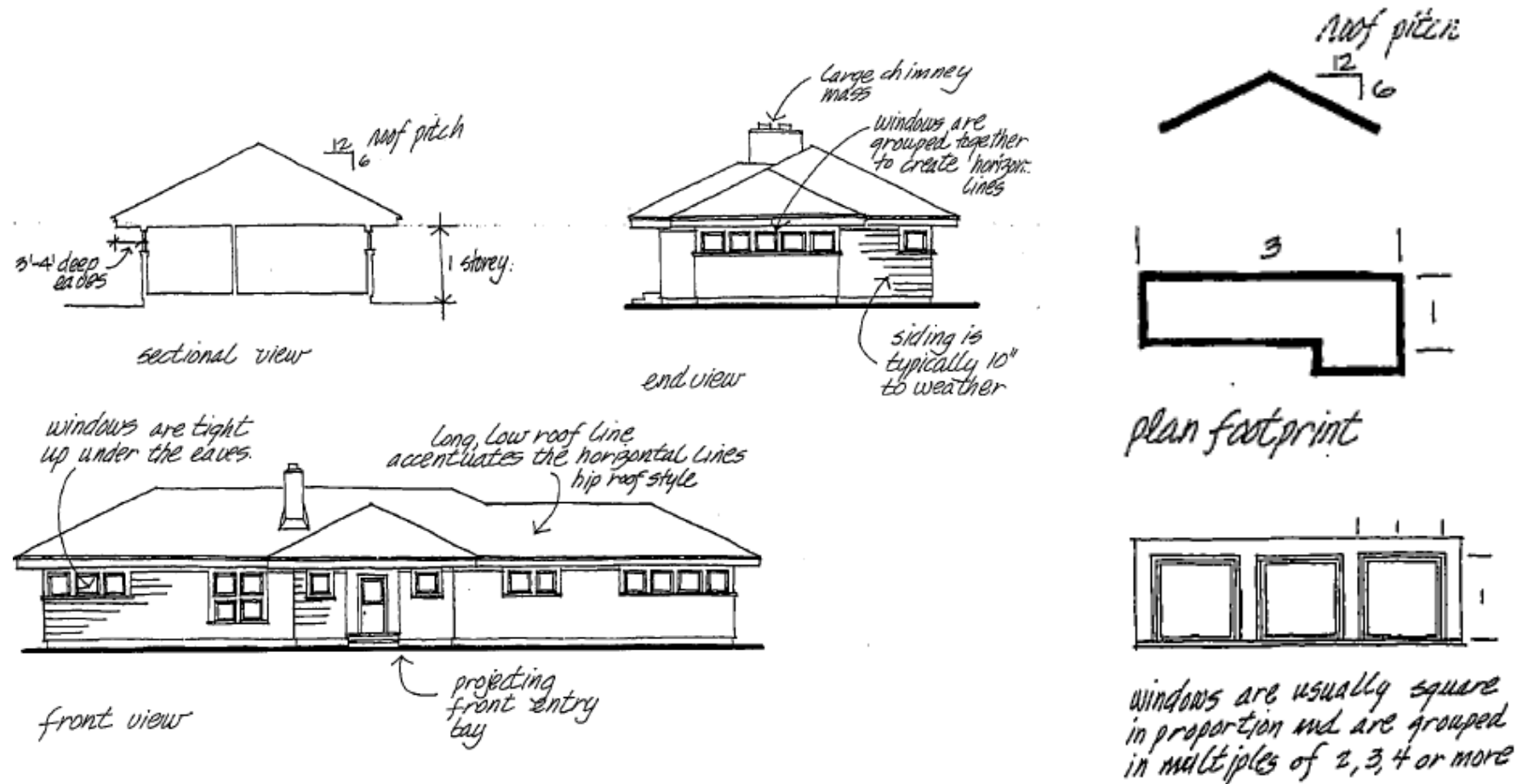
Porches/Verandahs Earlier ones have large covered porches. Many have recessed doorways. Exception: Porch with Classical portico.

Decorative Elements Often no corner boards, and no decoration other than shutters.

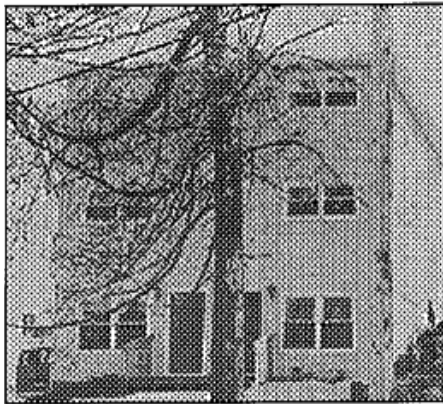
Colours White, blue or pastels



5.1.17 RANCH STYLE BUNGALOW (1950-1975)



5.1.18 MODERN (1970-PRESENT)



Proportion/Scale Long and low, sometimes "L" shaped. Range from one to two and one half storeys. Exception A multiple unit apartment building of three floors.

Materials Vinyl siding on the houses and brick facing on the apartment building.

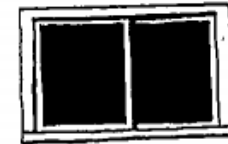
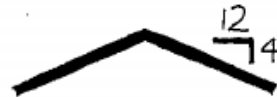
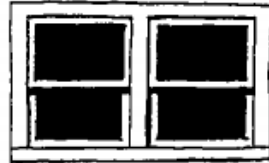
Roof Pitches Moderate to flat with little or no overhangs.

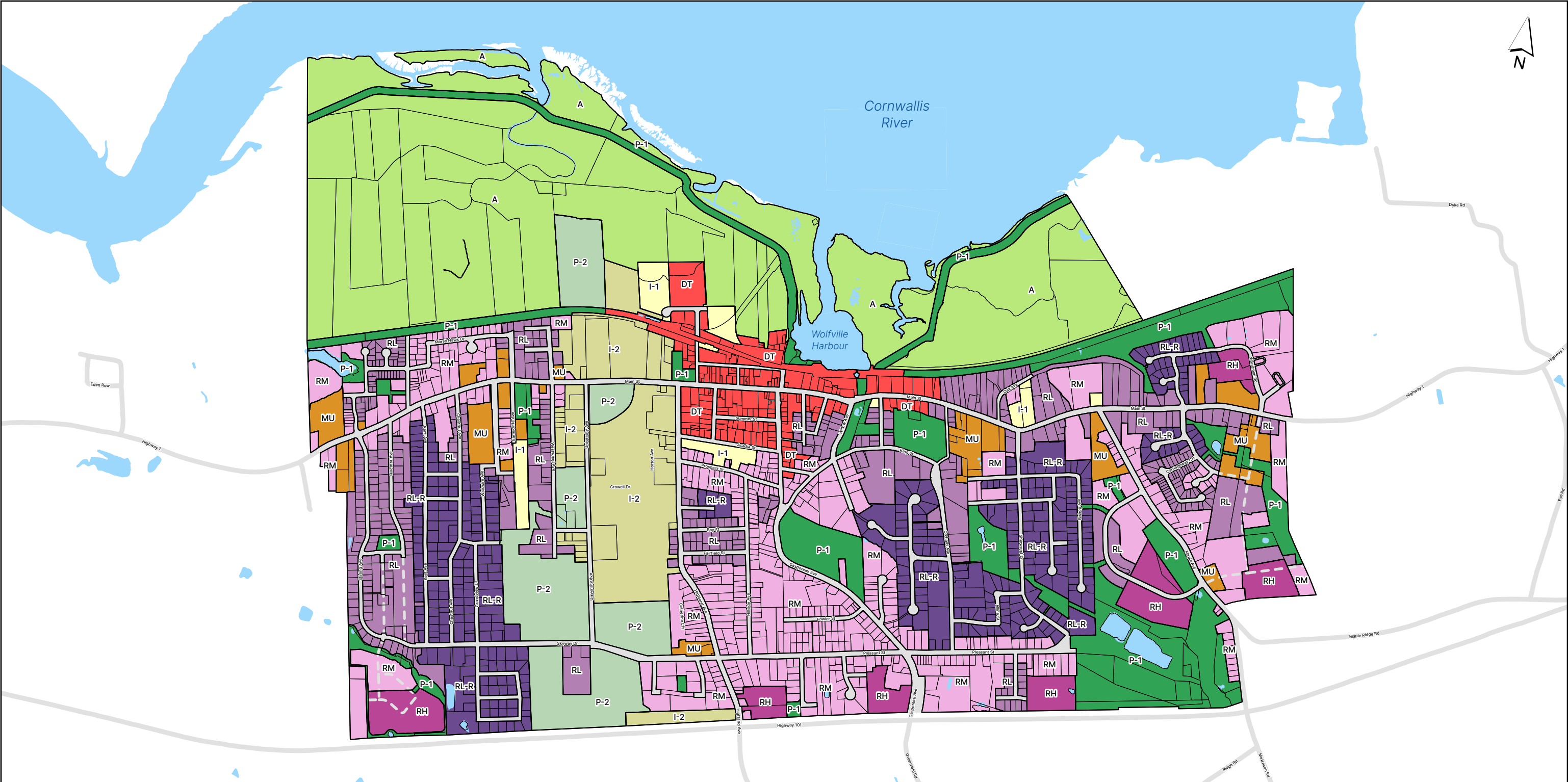
Windows Varying sizes of horizontal sliding windows.

Porches/Verandahs Recessed doorway on the houses, and a covered porch on the apartment building.

Decorative Elements None.

Colours White, blue or pastels.

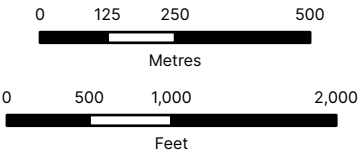




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The Town of Wolfville

Schedule A
Land Use Zoning Map



- | | | |
|---|---------------------------------|------------------|
| Residential Low-Density Restricted (RL-R) | Downtown (DT) | Agriculture (A) |
| Residential Low-Density (RL) | Institutional General (I-1) | Water Bodies |
| Residential Medium-Density (RM) | Institutional University (I-2) | Future Streets |
| Residential High-Density (RH) | Parks and Open Space (P-1) | Existing Streets |
| Mixed-Use (MU) | University and Open Space (P-2) | |